



September 13, 2024

Megan Rupard
EPD Solutions
3333 Michelson Drive, Suite 500
Irvine, California 92612

RE: Archaeological Resources Records Search Results for the Del Amo/Crenshaw Boulevard Project, Torrance, Los Angeles County, California

Dear Ms. Rupard:

An archaeological resources records search has been completed for the Del Amo/Crenshaw Boulevard Project. The proposed 8.27-acre project is located at 2421 West 205th Street (APNs 7352-018-067 and -068) in the city of Torrance, California. The subject property is situated within the former rancho of San Pedro (Dominguez), Township 4 South, Range 14 West on the U.S. Geological Survey (USGS) *Torrance, California* (7.5-minute) topographic quadrangle map. As part of the environmental review process, BFS Environmental Services, a Perennial Company (BFS), conducted a cultural resources records search at the South Central Coastal Information Center (SCCIC) at California State University, Fullerton.

Archaeological Records Search Results

The cultural resources records search was completed by BFS at the SCCIC on September 5, 2024, and encompassed the project area and a one-mile radius surrounding the project. Based upon the records search results, eight resources have been previously recorded within one mile of the project area, all of which are historic resources. The resources include the Torrance Health Center, the Fern-Greenwood School, the Gill Bridge, the Dow Chemical district, two railroads, a commercial warehouse, and a historic building. No resources are recorded within the project area.

The records search identified 22 previous studies within the one-mile search radius. One previous study (McKenna 2009) was mapped on the subject property. This study was a historic context overview of the city of Torrance and does not directly address the subject property.

The full results of the completed archaeological records search are attached to this letter report (Attachment A). Please contact us should you have any questions or require additional study for this project.

Regards,

A handwritten signature in black ink, appearing to read "Tracy A. Stropes". The signature is fluid and cursive, with the first name "Tracy" being the most prominent.

Tracy Stropes, M.A., RPA

Attachment:

Attachment A – Archaeological Records Search Results

ATTACHMENT A

Archaeological Records Search Results

BFSA Environmental Services, a Perennial Company

CALIFORNIA HISTORICAL RESOURCES INFORMATION SYSTEMS RECORDS SEARCH

Company: BFSA Environmental Services, a Perennial Company
Processed By: Emily T. Soong
Date Processed: September 13, 2024
Project Identification: Del Amo/Crenshaw Boulevard
Information Center: South Central Coastal Information Center
Search Radius: 1 Mile Buffer

Historical Resources:

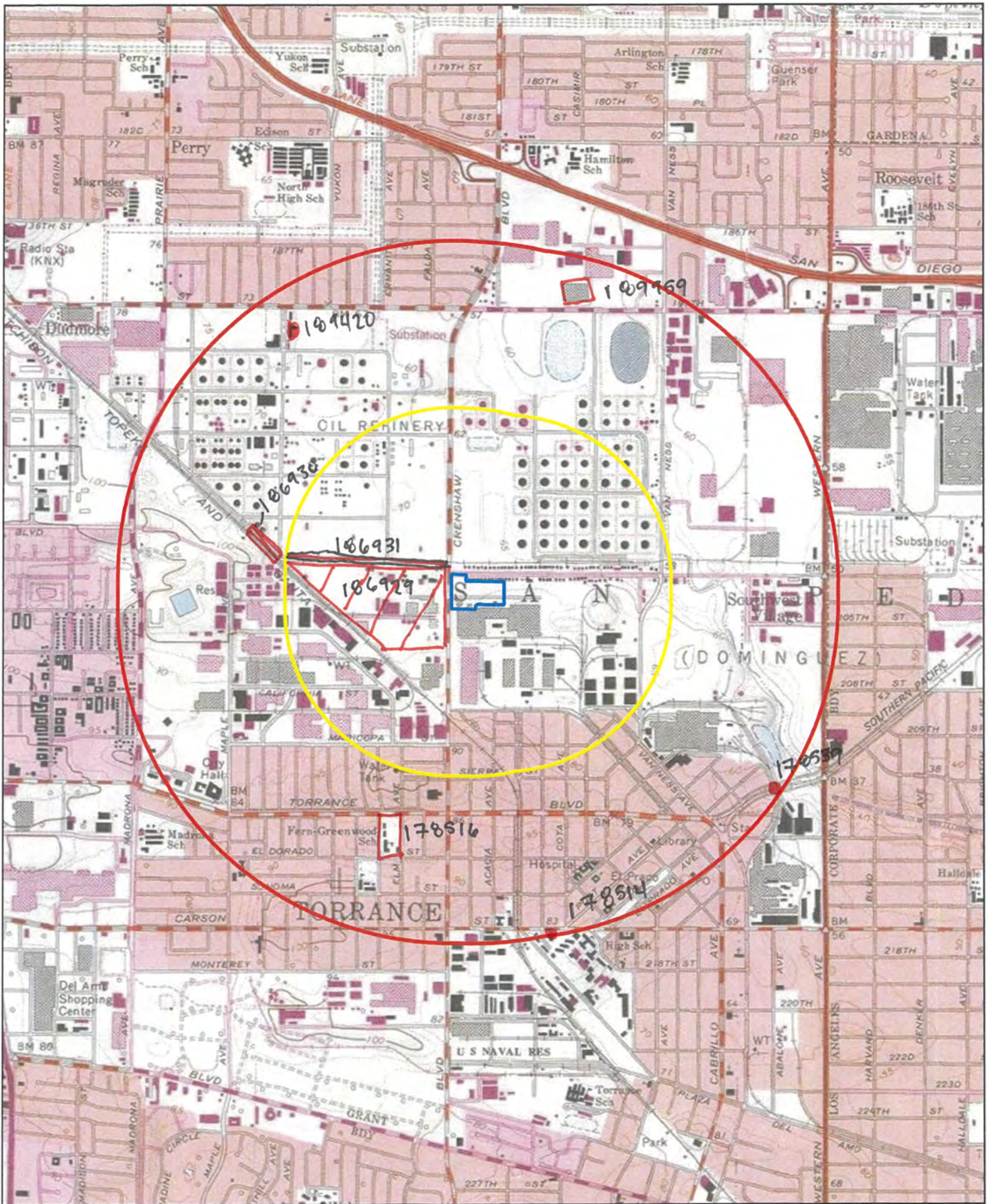
Trinomial and Primary site maps have been reviewed. All sites within the project boundaries and the specified radius of the project area have been plotted. Copies of the site record forms have been reviewed for all recorded sites.

There are eight resources located within a mile radius of the current project area, none of which are located within the subject property.

Previous Survey Report Boundaries:

Project boundary maps have been reviewed. National Archaeological Database (NADB) citations for reports within the project boundaries and within the specified radius of the project area have been reviewed.

There are 22 reports within a mile radius of the current project area, one of which overlaps with the subject property (LA-10333).



- Project
- Half mile radius
- 1 mile radius

Del Amo/Crenshaw Boulevard Project

USGS Torrance Quadrangle
(7.5-minute series)



Resources
1:24,000
1 of 1

ETS BFSA 8/14/2024

Resource List

Del Amo Crenshaw Blvd

Primary No.	Trinomial	Other IDs	Type	Age	Attribute codes	Recorded by	Reports
P-19-178514		OHP Property Number - 029193; Resource Name - Torrance Health Center		Historic		1994 (Christy Johnson McAvoy, Historic Resources Group)	LA-10829
P-19-178516		OHP Property Number - 029195; Resource Name - Fern Ave School	Building	Historic	HP15	1991 (M. Bihn & M. Lappen, City of Torrance Planning Department)	
P-19-178539		OHP Property Number - 029218; Resource Name - Gill Bridge; Other - Torrance Bridge; Other - Dominguez Land compnay 's "El Prado" Bridge; Other - Irving Gill-Torrance Blvd Bridge; Other - Pacific Electric Railroad Bridge	Structure	Historic	HP19	1979 (R. Hatheway & J. Chase, City of Torrance); 1988 (T. Alario, City of Torrance); 1989 (M. Bihn & K. Payne, City of Torrance Planning Dept./Redevelop.)	
P-19-186929		Resource Name - Dow Chemical Plant	District	Historic	HP08; HP11	2003 (M. Strauss, EDAW); 2020 (M. Wilson, AECOM)	LA-10103, LA-10878
P-19-186930		Resource Name - BNSF Harbor Subdivision	Structure	Historic	HP39	2003 (M. Strauss, EDAW)	LA-09910, LA-10878
P-19-186931		Resource Name - Alcoa Spur	Structure	Historic	HP39	2003 (M. Strauss, EDAW); 2020 (M. Wilson, AECOM)	LA-10103, LA-10878
P-19-189420		Resource Name - Mobil Torrance Refinery Admn Bldg; Other - ExxonMobil Torrance Refinery Admin. Building	Building	Historic	HP08	2011	LA-11051
P-19-189959		Resource Name - Self-Storage; Other - AT&T Mobility LLC LA0665-01 LTE/Storage & Extra/USID 48065	Building	Historic	HP06	2011 (K.A. Crawford, Crawford Historical Services)	LA-11659

PRIMARY RECORD

Primary #

HRI #

Trinomial

NRHP Status Code 252

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Other Listings

Review Code

Reviewer

Date

P1. Resource Identifier: Torrance Health CenterP2. Location: a. County Los Angeles and (Address and/or UTM Coordinates. Attach Location Map as required.)b. Address 2300 Carson StreetCity Torrance

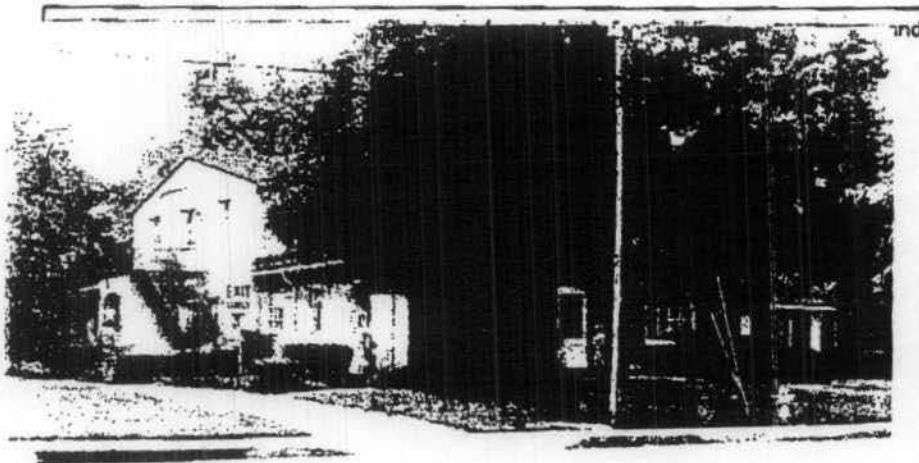
Zip

c. UTM: USGS Quad (7.5'/15') Date Zone mE/ mN

d. Other Locational Data (e.g., parcel #, legal description, directions to resource, additional UTMs, etc., when appropriate):

P3. Description (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries): Designed in the Spanish Colonial Revival style, this one and two-story health center has a distinctly residential appearance. The whitewashed, masonry structure is covered by gabled roofs of red tile. Exposed rafters are curved and notched. In plan, the building forms a T-shape with a one-story wing set perpendicular to Carson Street bisected slightly off-center by a two-story wing. The main lobby of the building is located in the corner of the T and is sheltered by a loggia along the front of the two-story wing. Stout, square piers support the tiled roof of the loggia. The facade of the one-story wing facing Carson Street features a secondary entrance flanked by windows. Two round-headed arches which spring from raised brick impost moldings, frame the entrance and window to the west. Another smaller round-headed arched opening to the east contains a window. Casement windows are used throughout the building which appears to be intact and in good condition.

P4. Resources Present: ☐ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District



P6. Date Constructed/Age:

☐ Prehistoric ☐ Historic ☐ Both

1933 F

P7. Owner and Address:

County of Los Angeles

P8. Recorded by (Name, affiliation,

and address): Christy JohnsonMcAvoy, Historic ResourcesGroup, 1728 N. Whitley Avenue,Hollywood, CA 90028P9. Date Recorded: 9/30/94P10. Type of Survey: ☐ Intensive☐ Reconnaissance ☐ Other

Describe: Survey of earthquake
damaged property for purposes
of Sec. 106 Review

P11. Report Citation (Provide

full citation or enter

"none."): 1994 NorthridgeEarthquake Project Review

Attachments: ☐ NONE ☐ Map Sheet ☒ Continuation Sheet ☒ Building, Structure, and Object Record ☐ Linear Resource Record

☐ Archaeological Record ☐ District Record ☐ Milling Station Record ☐ Rock Art Record ☐ Artifact Record ☐ Photograph Record

☐ Other (List):

DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

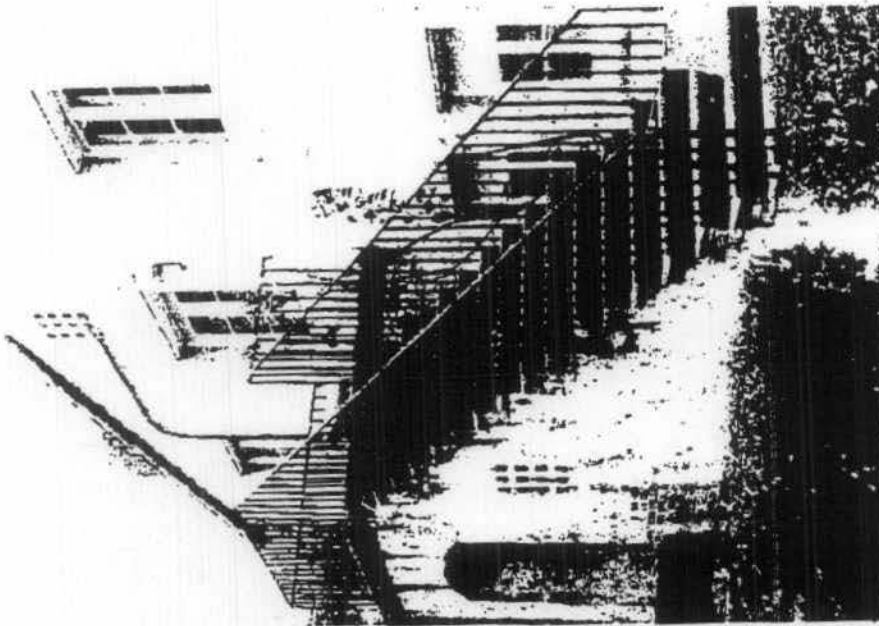
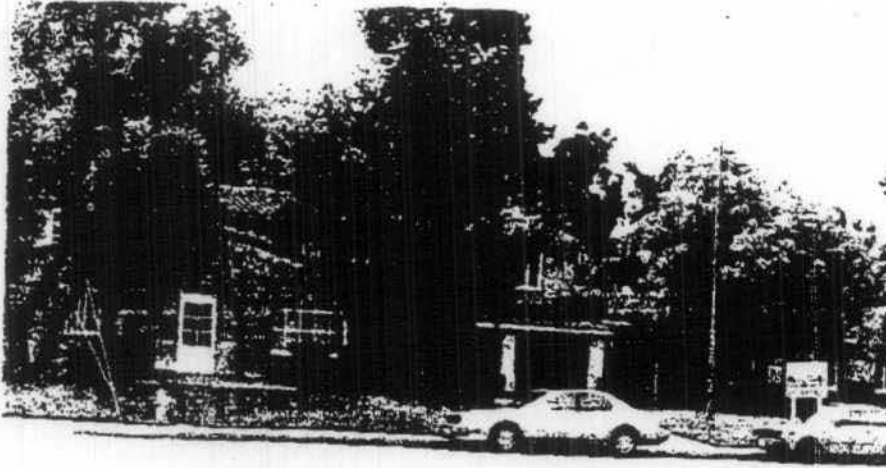
19-178514

Primary # _____
HRI #/Trinomial _____

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☒ Continuation ☐ Update

Resource Identifier: Torrance Health Center



BUILDING, STRUCTURE, AND OBJECT RECORD

Page 3 of 3

B1. Resource Identifier: Torrance Health Center

B2. Historic Name: Torrance Health Center

B3. Common Name: Torrance Health Center

B4. Address: 2300 Carson Street

City: Torrance

County: Los Angeles

Zip: _____

B5. Zoning: _____ B6. Threats: Earthquake Damage

B7. Architectural Style: Spanish Colonial Revival

B8. Alterations and Date(s): _____

B9. Moved? ☒ No ☐ Yes ☐ Unknown Date: _____ Original Location: _____

B10. Related Features: _____

B11. Architect: Unknown

Builder: Unknown

B12. Historic Attributes (List attributes and codes): HP41--Community Health Center

B13. Significance: Theme Public Health

Area Los Angeles County

Period of Significance _____

Property Type Health Center

Applicable Criteria A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.) The Torrance Health Center is significant under National Register Criterion A as one of a group of nine health centers erected in the County of Los Angeles during the late 1920s and early 1930s. The centers represented an organizational structure developed by then Los Angeles County Health Officer, J.L. Pomeroy M.D., which emphasized decentralization and coordination with cities and school districts. The county was divided into service districts with health centers providing for the activities of the health and welfare departments as well as clinics for medical care of the indigent and prevention of communicable diseases. The Torrance Health Center was the last to be constructed by the County in 1933 at a cost of \$38,100. The centers were all designed in the Spanish Colonial Revival style and blended seamlessly into the surrounding residential neighborhoods. Although the name of the architect is unknown, it was most probably designed by a Los Angeles County in-house architect. The structure may have been designed by J.W. Smart and Karl Much, the architects of record for similar health centers in Santa Monica and Alhambra. The structure appears to be substantially intact and retains its architectural integrity.

B14. References: "Unified Health and Welfare Centers of Los Angeles County, California" Architectural Record, June 1935, p. 445-449; DPR 523, prepared by Roger Hathaway & John Chase, 1979.

B15. Evaluator: Christy Johnson McAvoy

Date of Evaluation: 9/30/94

(This space reserved for official comments.)



"FINAL COPY"
19-178516

NPS Form 10-900
(Rev. 8-86)

SEP 13 1991

OMB No. 1024-0018

United States Department of the Interior
National Park Service

NATIONAL REGISTER OF HISTORIC PLACES
REGISTRATION FORM

1. Name of Property

historic name: Fern Avenue School

other name/site number: Fern-Greenwood School

2. Location

street & number: 1314 Fern Avenue

not for publication: N/A

city/town: Torrance

vicinity: N/A

state: CA

county: Los Angeles

code: 037

zip code: 90503

3. Classification

Ownership of Property: Torrance Unified School District, 2335 Plaza del Amo,
Torrance, California, 90501 (public-local)

Category of Property: building

Number of Resources within Property:

Contributing	Noncontributing	
1	0	buildings
0	0	sites
0	0	structures
0	0	objects
1	0	Total

Number of contributing resources previously listed in the National Register: 0

Name of related multiple property listing: N/A

4. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act of 1986, as amended, I hereby certify that this x nomination request for determination of eligibility meets the documentation standards for registering properties in the National Register of Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60. In my opinion, the property x meets does not meet the National Register Criteria.

 See continuation sheet.

Shade R. Craig
Signature of certifying official

January 14, 1992
Date

California Office of Historic Preservation

State or Federal agency and bureau

In my opinion, the property meets does not meet the National Register criteria.

 See continuation sheet.

Signature of commenting or other official

Date

State or Federal agency and bureau

5. National Park Service Certification

I, hereby certify that this property is:

 entered in the National Register

 See continuation sheet.

 determined eligible for the
National Register

 See continuation sheet.

 determined not eligible for the
National Register

 removed from the National Register

 other (explain):

Signature of Keeper

Date
of Action

6. Function or Use

Historic: Educational

Sub: School

Current : Educational

Sub: 4 School

7. Description

Architectural Classification: Spanish Colonial Revival

Other Description: _____

Materials: foundation concrete

roof terra cotta

walls OTHER: plaster

other

Describe present and historic physical appearance.

The Fern Avenue School's nominated area comprises only the main building which is located at the southwest corner of the site. Completed in 1933, it consists of nine classrooms, administration and a kindergarten. Henry L. Gogerty is the architect on record. This structure is the oldest elementary school still in use and the only example of Spanish Colonial Revival architecture for a school building in the City of Torrance.

Historic Physical Appearance:

The strength of the building's appearance lies in its simple shapes, clean lines, and unity of form. The main building is a combination of one and two-story structures, "L" shaped in plan with low ridge roofs of clay "Spanish" tile. The building is divided into three sections: the east wing includes the kindergarten, the kindergarten work area, and the play area; the south part contains the administration offices, nurse's office, and supply room; the first floor of the north part comprises the girls and boys bathrooms, janitor work room, and five classrooms; and the second floor of the north part takes in the teachers' work area, janitor, storage room, and four classrooms. The arcade on the east elevation faces north and functions as an entry way between the kindergarten and the main play area, but is included within the roofline. The building also has a basement area for storage and central heating uses.

All exterior walls are constructed with six layers of brick (14" thick) with a plaster surface. The walls show a decorative base that rises approximately two

X See continuation sheet

8. Statement of Significance

Certifying official has considered the significance of this property in relation to other properties: locally.

Applicable National Register Criteria: A and C

Criteria Considerations (Exceptions) : N/A

Areas of Significance: Architecture
Education
Social History

Period(s) of Significance: 1932 - 1941

Significant Dates: 1932

Significant Person(s): N/A

Cultural Affiliation: N/A

Architect/Builder: Gogerty, Henry L., architect

State significance of property, and justify criteria, criteria considerations, and areas and periods of significance noted above.

The Fern Avenue School is one of the most notable buildings remaining in Torrance which embodies the best architectural and historical characteristics from its time period. The School is both historically and architecturally significant under criteria "A" and "C." The main building is significant in three areas: architecture, social history and education. First, it is the oldest elementary school still in existence as well as the only example of Spanish Colonial Revival architecture applied to a school building in Torrance. The main entrance of the Fern School, in particular, is significant for the excellence of its craftsmanship and detailing. Second, the school became the focal point of a debate for more local control (The City Charter movement) between local residents and the Los Angeles City School District. This debate emerged as Torrance's residential population grew from the original center of town to the west and residents demanded more local control over services. Third, the school's early student population included minority groups who were segregated from Torrance's white residents; specifically, Japanese and Hispanic families who lived to the north and west of the center of town. Two other structures in Torrance, Torrance High School (Italianate Style) and the Pacific Electric Bridge (Irving Gill) have been listed in the National Register of Historic Places.

The period of significance is 1932 to 1941. Although the main building of the Fern School continues to be important to the community into the present, the period of significance has been ended at 1941, fifty years ago. As the oldest existing elementary school in the City of Torrance, the Fern School's main building has been recognized as a significant historic site by the Torrance Historic Resources Survey (May, 1979) and the Torrance Historical Society.

 X See continuation sheet.

9. Major Bibliographical References

X See continuation sheet.

Previous documentation on file (NPS):

- preliminary determination of individual listing (36 CFR 67) has been requested.
- previously listed in the National Register
- previously determined eligible by the National Register
- designated a National Historic Landmark
- recorded by Historic American Buildings Survey #
- recorded by Historic American Engineering Record #

Primary Location of Additional Data:

- State historic preservation office
- Other state agency
- Federal agency
- X Local government
- University
- X Other -- Specify Repository: Torrance Unified School District
Los Angeles Unified School District

10. Geographical Data

Acreeage of Property: .40 acre

UTM References: Zone Easting Northing Zone Easting Northing

A	<u>11</u>	<u>376840</u>	<u>3744450</u>	B	<u> </u>	<u> </u>	<u> </u>
C	<u> </u>	<u> </u>	<u> </u>	D	<u> </u>	<u> </u>	<u> </u>

 See continuation sheet.

Verbal Boundary Description: Refer to enclosed scale map

X See continuation sheet.

Boundary Justification: The boundaries are drawn to encompass the historic resource and its immediate setting.

11. Form Prepared By

Name/Title: Michael G. Bihn, Senior Principal Planner
Michael Lappen, Planning Associate

Organization: City of Torrance Planning Department Date: March 20, 1991

Street & Number: 3031 Torrance Blvd.

Telephone: (213) 618-5990

City or Town: Torrance

State: CA

ZIP: 90503

United States Department of the Interior
National Park ServiceNATIONAL REGISTER OF HISTORIC PLACES
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(Fern Avenue School) Page 2

feet above grade. The east elevation plans show a four-bay arcade, with round-headed arch, on the east elevation of the structure. The kindergarten play yard is elevated above street level and has cast stone posts and a decorative wrought iron railing. All exterior stairs and the arcade are flanked by decorative wrought iron railings. The entries have double doors made of 2 1/2" pine with six glass window panes and a ten glass window panes over the entry. The kindergarten entry consists of a single door made of 2 1/2" pine.

On the west elevation, the decorative main entry serves as the focal point for the structure. It is graced with cast stone columns and Corinthian capitals support the cornice and frame the doorway. Above the doorway, there is a window opening with decorative stone and metalwork. The architect has also incorporated four small stylized friezes, depicting ocean themes, above the doorway. The entrance includes cast stone steps leading to the two, 2 1/4" thick hardwood doors (6'4"x 8'6") with plate glass panels.

The fenestration is rich in variety. A mixture of windows sizes and materials were integrated into the design of the exterior facade. Each classroom has an average of five, double hung windows (3'8"x 8') with eight glass panes, wood sash and frame. The Kindergarten and other work rooms have double hung windows (2'10"x 8) with eight glass panes, wood sash and frame, for a total of three on the east and nine on the south elevations. The Administrative offices contain double swing windows (3'5"x 5') with eight glass panes, metal sash and frame. There are three such windows on the west and five on the south elevations. The facade also shows other steel windows of various sizes, including six double hung, wood sash and frame windows (3'2"x 5'6") for the bathrooms and two single swing windows (1'11"x 3') in the teachers' lounge area. One window, with six glass panes, wood sash and frame that tips inward, is located on the second floor hallway. Four windows covered with decorative wrought iron grills on the west facade. All classroom windows are supported by cast stone sills.

The roof design comprises several levels and visually ties together the separate parts of the structure with the consistency of roof slope. The highest level is over the entrance and steps downward to the second and first story levels on the north and south sides of the structure. It supports two wrought iron finials on the ridge of the highest level which frames the main entrance. Also, a square, cast stone chimney rises east of the roof ridge over the northern part of the second floor.

The interior is unified by a single hallway that is highlighted by natural light from the classrooms. The interior walls and ceilings are constructed of reinforced concrete. Each classroom has two doorways, with 1 3/4" wood panel doors, facing the hallway. Interior windows (2'4" x 3'), with six glass planes, are placed over each doorway and spaced between the doorways. The windows are designed to filter more natural light into the hallway area. The interior

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(8-86)

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staircases exhibit wood railings. The classrooms show hardwood floors but, it is unknown whether hardwood or linoleum tile floors originally covered the hallways. The classrooms were originally designed with a cloakroom and storage areas located in the rear. Room 3 was also used as an auditorium with a stage located at the north end of the room. However, it lacked a wardrobe and storage area. The entry area, near the administration offices, has a glass display case and hanging wrought iron lamps. The administration office area includes a hardwood counter, hardwood teacher mailboxes, and a centralized intercom system in a wood cabinet.

Alterations and Present Physical Appearance:

Over the years, both the Los Angeles City and the Torrance Unified School Districts have endeavored to retain the original architectural integrity of the main building. Some changes have occurred, but the building's facade has not been significantly altered and retains its architectural integrity. In 1946 an addition was made to the north; in 1959 the school spent \$250,000 to rehabilitate and seismically upgrade the building to earthquake standards. In the 1970s the original double-hung classroom windows were replaced with aluminum windows. However, one original double-hung window remains located at the south stairway. The classroom window dimensions, and wood trim, remain as originally designed. In the 1980s the School District replaced the original exterior doors, which had deteriorated, with new ones of the same style, dimensions and materials.

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Educational and Social History:

The Fern School was constructed during a period of rapid growth in the Torrance area. The City of Torrance, incorporated in 1921, experienced a period of rapid growth in the late 1920s, spurred by its location near the Los Angeles Harbor, the result of an oil boom, and the further expansion of heavy industry (U.S. Steel's purchase of Columbia Steel). The resident population increased from 825 in 1919 to 8,000 by 1932. Likewise, the school population had increased from 150 to 1800 students and from seven to 70 teachers during the same period. By 1928, the local principals were actively recruiting new teachers and Torrance Elementary School, constructed in 1924, was using the school auditorium for classroom space and considering half-day sessions.

Prior to construction of the main building, the Fern School site was used to house the expanding elementary school population. In September 1928, the Los Angeles City School District located a temporary wood structure from El Segundo to the new site at Fern Avenue, what was then considered to be the western edge of town. The district transferred 119 students and three teachers to the new site. Called the "Fern Street Annex to Torrance Elementary School," the school initially functioned as an extension to Torrance Elementary School for first to third graders. The administration and Parent-Teachers Association remaining based at Torrance Elementary School. By 1930, however, the Fern Avenue School was on firm footing with 150 students from all elementary grades, four teachers and its own principal, Guy Holliday. The "Elementary School" PTA continued to represent both schools.

Community leaders often complained that the School District neglected Torrance. They felt that, as a "workingman's" town, the City was relegated to "second class status" and received only a 50% return on its tax contribution to the School District. Still, Torrance residents supported several district wide Bonds to purchase properties and finance construction of new school facilities for the entire School District, including Torrance. By August 1931, Torrance boosters, desiring greater local autonomy, proposed to create a city charter which would also transfer the schools into local hands. Political leaders proposing the City Charter and educational leaders supporting the School District debated their respective causes in the public forum. As a result, the School District postponed any new building project for the Fern School until a vote decided the fate of the City Charter. In April, 1932, voters rejected the City Charter and the Los Angeles City School District retained control of the Torrance schools.

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(8-86)

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In May, 1932, the School Board authorized \$84,000 to fund the new school's construction. Henry L. Gogerty designed the new school building and the firm of George Mitty, from Los Angeles, was contracted to do the actual grading and construction work. The Torrance Herald, the local newspaper, reported that a few local firms were subcontracted to assist the building activity. Grading on the site began in June, 1932 with work taking approximately 125 days to complete. Students were moved into the new building in December 1932, with four teachers and Guy Holliday continuing as principal. Within a few months after completion, the Fern School withstood its first test when it survived, sustaining no damage, the Long Beach Earthquake of March, 1933.

In February 1933, the School District changed Torrance Elementary School and Fern School's districts. Torrance Elementary retained the older center of town while Fern School served the area north and west of older center part of town to the City's western boundary with Redondo Beach. The redrawn area brought another 149 students, originally from Torrance Elementary School (except for those 6th grade students who wished to graduate from Torrance Elementary School), to Fern Avenue, for a total of 295 students and six teachers. The next year, enrollment increased to 320, with ten teachers, and remained under 350 students until the Second World War.

Fern School's new district drew students from various ethnic groups, including families of newly arrived white refinery workers, Japanese farmers and Hispanic laborers. In 1912, the original town's master plan designated areas outside the town as "foreign quarters" or "areas for non-Caucasians." Minorities were excluded from joining most civic and community organizations. As a result of separation, Hispanic families resided in the neighborhood a mile north of the center of town called "The Pueblo" or "The Mexican Village." These residents worked for local industry and maintained their own church, market and community organizations. Likewise, the first and second generation Japanese families, who lived and worked in the strawberry fields a mile west of the center of the City, supported their own newspaper, markets, school and social organizations. Also the city was expanding further westward from the old center to accommodate new housing for white refinery workers. Newspaper reports from the 1930s and interviews with former Fern School students indicate that the different ethnic groups attended the same classes and played together without confrontation. Fern School provided students from different ethnic groups a rare chance to integrate with each other.

School principals under Los Angeles administration included Guy Holliday in 1930, Oda Vans in 1935, Lillian Billups in 1939, Mary Charlotte Weddell in 1941, Carolyn Blackmun in 1946. Albert Posner served as the first principal under the Torrance Unified School District. He was followed by Ron Henderson, Charles Crecelius, Malcolm Richland, Joe T. Brandon, Ellen Booz, Donald Credell, Daniel Kelly, Cecil Paschall, Lucille LeMieux, and Robert Scharf.

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Architectural Significance:

Architecturally, the main building is distinct from the other Torrance schools due to its use of the Spanish Colonial Revival style, which was popular among professional Californian architects in the 1920s. Torrance schools show a variety of architectural styles and the two earlier schools in Torrance proper, Torrance High School (1917, listed in the National Register of Historic Places) and Torrance Elementary School (1924, altered and presently integrated into Torrance High School) were designed in the Italianate style. Other regional schools included the Waltheria School (1933, the Mission Style building was demolished) established by the Redondo Beach School District in 1925 and the Perry School (the wood building was demolished) which began in 1905 by Los Angeles County. The variety of architectural styles at this time also may indicate that the Los Angeles City School District did not subscribe to any specific architectural style in order to "elevate or instruct" students, as was the case during the 1930s with instructive frieze work seen on "Art Deco" buildings.

Compared to other commercial and institutional buildings in Torrance, the Fern School is one of the two non-residential, Spanish Colonial buildings to have retained its architectural integrity; the other being the Nativity Church (1921). The City began with an impressive architectural tradition since the firm of Olmsted and Olmsted created the original town plan and Irving Gill (noted for his stripped down design with elements of the Mission style) designed the first commercial and residential structures. In Torrance, the Spanish Colonial style became a popular style for several residences, institutional and commercial buildings during the 1920s. The following non-residential buildings, which exhibit elements of the Spanish Colonial style, have been listed in the City's "Torrance Historic Resources Survey:" Torrance Women's Club (1925, altered), Torrance Memorial Hospital (1924, altered), Pacific Telephone and Telegraph Building (1928, unaltered), and the First Baptist Church (1948, altered). Unlike the Fern School, all these buildings are also located near the older center part of the City. In the 1930s, the design for Torrance's institutional buildings was done primarily in the Federal Style (The City Hall, 1936, Torrance Civic Auditorium, 1937 and Public Library, 1937).

The efficient design has also enabled the building to continue as an elementary school during the periods of rapid population growth and decline without altering its architectural integrity. Immediately following its completion the Long Beach Earthquake in 1933 presented the new school building with its most

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severe challenge to its structural integrity. The Torrance Herald reported that the building survived without any damage, compared to Torrance Elementary School which was forced to temporarily close. In 1947, administrators for the new Torrance Unified School District called the fifteen year old Fern School "one of the most modern plants in the City of Torrance." Even in the mid-1980s, the strength of the Fern School's design was further reinforced when the School District, facing declining student enrollment, decided to close the newer Greenwood School and rehabilitate the older main building. When compared to post World War II school architecture, many teachers and former students note that the Fern School provides a comfortable learning atmosphere. Its architecture give the building a permanence and stability in the neighborhood (now considered to be located in the aging center of Torrance) as older traditional single-family, worker housing is slowly being replaced by upscale homes, multiple-family residences.

Henry L. Gogerty is the architect on record. During the 1920s, he was known for his use of the Spanish Colonial Revival style with Churrigueresque ornamentation, notably the "Hollywood Playhouse" (Hollywood Palace) and the "Bane Building" in Hollywood, California.

Community Commitment:

Since 1985, the City of Torrance, the Torrance Unified School District and the Torrance Historical Society have been committed to the preservation of the Fern Avenue School's main building. City and School officials and staffs have worked closely during the application process. In February, 1990, the Board of Education began the nomination process by expressing its commitment to the preservation of the historically and architecturally significant school. The City's Planning Department staff undertook research to obtain the appropriate historical documentation for the preparation of the nomination form. This nomination form was completed with the close cooperation of Fern School's administration, teachers and parent organization. Both City and School District officials have also expressed their desires to see the school included into the National Register.

NPS Form 10-900-a
(8-86)

OMB Approval No. 1024-0018

United State Department of the Interior
National Park Service

NATIONAL REGISTER OF HISTORIC PLACES
CONTINUATION SHEET

Section number 9

Bibliography

(Fern Avenue School) Page 2

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History of Torrance, A Teacher's Resource Guide. Torrance, California: Torrance Unified School District September 1964. Document on file with the Torrance Historical Society, Torrance, California.

Registry of Teachers. Los Angeles City School District, 1909-1946. On file with the Torrance Historical Society, Torrance, California.

Planning Documentation:

Hatheway, Roger, and John Chase. "Torrance Historical Resources Survey" Torrance, California. May 1979. Document on file with the City of Torrance Planning Department, Torrance, California.

NPS Form 10-900-a
(8-86)

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United State Department of the Interior
National Park Service

NATIONAL REGISTER OF HISTORIC PLACES
CONTINUATION SHEET

Section number Attachment # 1 (Fern Avenue School) Page 1

There are 23 photographs with this registration form. All are of the Fern Avenue School in Torrance, California.

With the exception of photograph #1, the historic photo, all photos were taken by Michael Lappen and Steve Crecy on December 11, 1990.

Photograph #1

1. Fern Avenue School
2. Torrance, California
3. Haig and Haig
4. 1932
5. Torrance Historical Society
6. View from the corner of Fern Avenue and El Dorado Street, looking north/east at west elevation.

Photograph #2

- 1-2. Same as above
3. Michael Lappen
4. December 11, 1990
5. City of Torrance Planning Department
6. View from the corner of Fern Avenue and El Dorado Street, looking north/east at west elevation.

Photograph #3

- 1-5. Same as above
6. View from Fern Avenue, looking south/east at west elevation.

Photograph #4

- 1-5. Same as above
6. View of east elevation, looking south/west.

Photograph #5

- 1-5. Same as above
6. View of east elevation with arcade connecting main building with north annex, looking west.

NPS Form 10-900-a
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OMB Approval No. 1024-0018

United State Department of the Interior
National Park Service

NATIONAL REGISTER OF HISTORIC PLACES
CONTINUATION SHEET

Section number Attachment # 1 (Fern Avenue School) Page 2

Photograph #6

- 1-2. Same as above
- 3. Steve Crecy
- 4-5. Same as above
- 6. View of the south elevation, looking north.

Photograph #7

- 1-5. Same as above
- 6. View of west elevation main entrance, with ornamentation.

Photograph #8

- 1-5. Same as above
- 6. View of west elevation main entrance, detail of doorway and ornamentation

Photograph #9

- 1-2. Same as above
- 3. Michael Lappen
- 4-5. Same as above
- 6. north entry doorway, detail

Photograph #10

- 1-5. Same as above
- 6. Kindergarten Play Area with cast stone columns, detail looking north.

Photograph #11

- 1-2. Same as above
- 3. Steve Crecy
- 4-5. Same as above
- 6. Detail of gate at the Kindergarten Play Area, looking north.

Photograph #12

- 1-2. Same as above
- 3. Michael Lappen
- 4-5. Same as above
- 6. West elevation window detail, with wrought iron grating.

NPS Form 10-900-a
(8-86)

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United State Department of the Interior
National Park Service

NATIONAL REGISTER OF HISTORIC PLACES
CONTINUATION SHEET

Section number Attachment # 1 (Fern Avenue School) Page 3

Photograph #13

- 1-5. Same as above
- 6. West elevation window detail.

Photograph #14

- 1-5. Same as above
- 6. First floor hallway, showing classroom windows and doors, looking north.

Photograph #15

- 1-5. Same as above
- 6. Original fire extinguisher case with wood detail, second floor hallway.

Photograph #16

- 1-5. Same as above
- 6. The northern staircase, showing detail of original bannister and wood work.

Photograph #17

- 1-5. Same as above
- 6. The northern staircase, showing original double hung window with original wood frame and trimming.

Photograph #18

- 1-5. Same as above
- 6. Interior view of a typical classroom, showing detail of original storage area, looking south.

Photograph #19

- 1-5. Same as above
- 6. Interior view of a typical classroom showing wood floor and new windows, looking west.

Photograph #20

- 1-5. Same as above
- 6. View of typical classroom with original wood floor, looking east

NPS Form 10-900-a
(8-86)

OMB Approval No. 1024-0018

United State Department of the Interior
National Park Service

NATIONAL REGISTER OF HISTORIC PLACES
CONTINUATION SHEET

Section number Attachment # 1 (Fern Avenue School) Page 4

Photograph #21

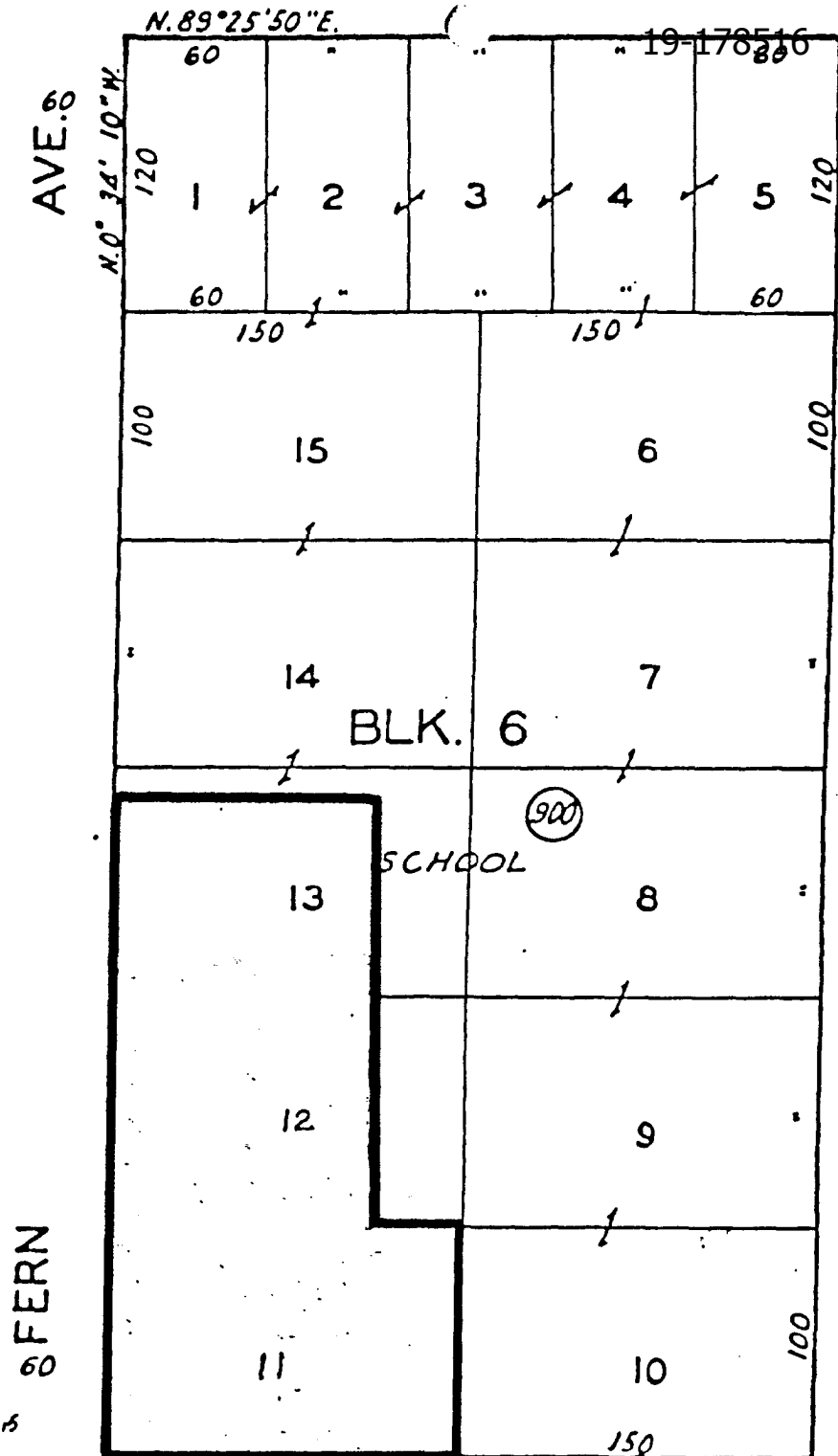
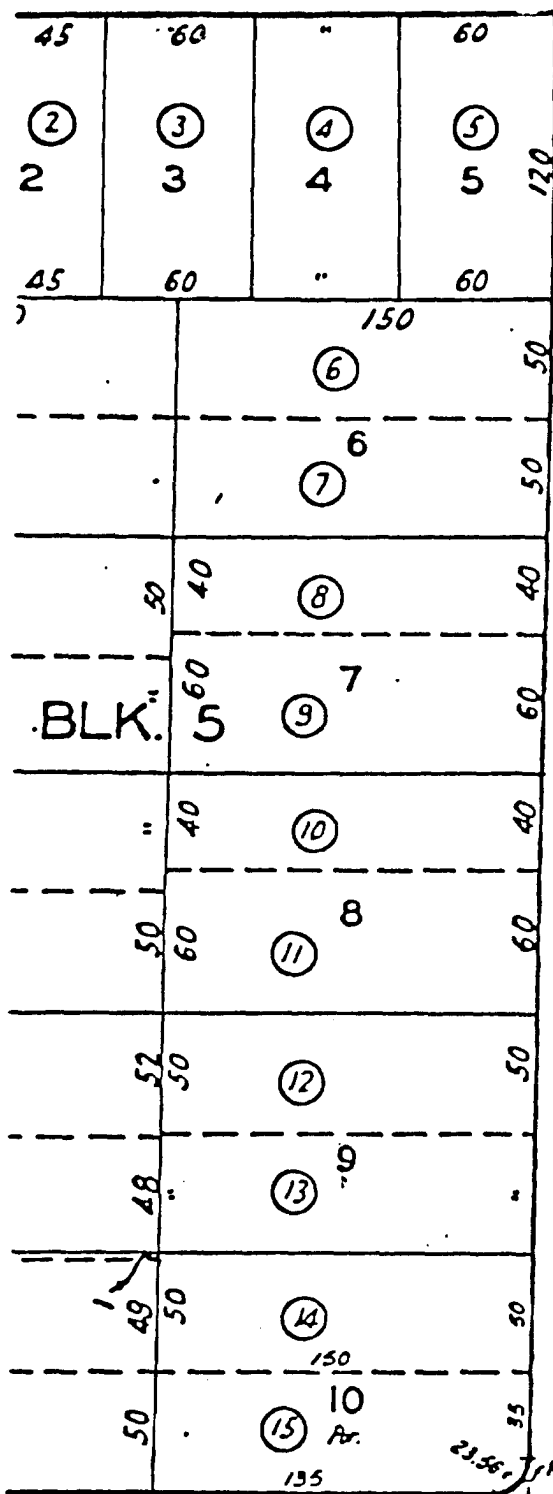
- 1-5. Same as above
- 6. View of original wood counter in the administration office.

Photograph #22

- 1-5. Same as above
- 6. View of the original intercom system in the administration office.

Photograph #23

- 1-5. Same as above
- 6. View of an original classroom bench (dated late 19th century) from the Perry Elementary School (demolished), located in the main hallway.



FERN AVENUE SCHOOL

Torrance
California

1" : 80'



FERN AVENUE

ELM AVENUE

North Annex

Temporary

Structures

Cafeteria

Main Building

Kindergarten
Play Area

EL DORADO STREET

OFFICE OF HISTORIC PRESERVATION

DEPARTMENT OF PARKS AND RECREATION

POST OFFICE BOX 942896

SACRAMENTO, CALIFORNIA 942896-0001

(916) 445-8006



• September 17, 1991

• See Attached List

•
DeRoy J. Jackson, City Manager
City of Torrance
3031 Torrance Blvd.
Torrance, CA 90503
•

We are pleased to inform you that the above property will be considered by the State Historical Resources Commission for nomination to the National Register of Historic Places. The National Register is the Federal Government's official list of historic buildings and other cultural resources worthy of preservation. Listing in the National Register provides recognition and assists in preserving our Nation's heritage. Enclosed is a copy of the criteria under which properties are evaluated.

Listing in the National Register provides the following benefits to historic properties:

—Consideration in the planning for federally assisted projects. Section 106 of the National Historic Preservation Act of 1966 provides that the Advisory Council on Historic Preservation be given an opportunity to comment on projects affecting such properties.

—Eligibility for Federal tax benefits. If a property is individually listed in the National Register, or determined a contributing structure within a National Register district, certain tax provisions may apply. The Tax Treatment Extension Act of 1980 includes provisions regarding charitable contributions for conservation purposes of partial interests in historically important land areas or structures, including archeological sites and private residences. The Tax Reform Act of 1986 contains provisions intended to encourage preservation of depreciable historic structures by allowing favorable Federal tax provisions for rehabilitation. Beginning January 1, 1987, the Tax Reform Act of 1986 replaces the rehabilitation tax incentives available under prior law with a 20% investment tax credit for rehabilitations of certain historic commercial, industrial, and residential rental buildings.

—Consideration of historic values in the decision to issue a surface coal mining permit where coal is located, in accord with the Surface Mining and Control Act of 1977.

—Qualification for Federal grants for historic preservation when funds are available.

Owners of private properties nominated to the National Register of Historic Places must be given an opportunity to concur in or object to listing in accord with the National Historic Preservation Act Amendments of 1980 and Federal regulations 36 CFR Part 60. Any owner or partial owner of private property who chooses to object to listing is required to submit to the State Historic Preservation Officer a notarized statement certifying that the party is the sole or partial owner of the private property, as appropriate, and objects to the listing. For a single privately owned property with one owner, the property will not be listed if the owner objects. In nominations with multiple ownership of a single property, the property will not be listed if a majority of the owners objects. Each owner or partial owner of private property has one vote regardless of what part of the property that party owns. If the property cannot be listed because the owner or a majority of owners objects prior to the submission of a nomination

19-178516

by the State, the State Historic Preservation Officer shall submit the nomination to the Keeper of the National Register for a determination of the eligibility of the property for inclusion in the National Register. If the property is then determined eligible for listing, although not formally listed, Federal agencies will be required to allow the Advisory Council on Historic Preservation an opportunity to comment before the agency may fund, license, or assist a project which will affect the property. If you choose to object to the listing of your property, the notarized objection must be submitted to Mrs. Kathryn Gualtieri, State Historic Preservation Officer, Office of Historic Preservation, Post Office Box 942896, Sacramento, CA 94296-0001, fifteen (15) days before the State Historical Resources Commission considers this application; it will be considered by the Commission on November 1, 1991

at 9:00 a.m., in Room 302, County Administration Building
San Diego, California

If you wish to comment on whether the property should be nominated to the National Register, please send your comments to the above address. Comments must be received before the State Historical Resources Commission considers this application. A copy of the application is on file in our office and will be made available to you upon request.

Sincerely,



Kathryn Gualtieri
State Historic Preservation Officer

Enclosure

OFFICE OF HISTORIC PRESERVATION

DEPARTMENT OF PARKS AND RECREATION

POST OFFICE BOX 942896

SACRAMENTO, CALIFORNIA 94296-0001

(916) 445-8006



• September 17, 1991

• See Attached List

•
Michael G. Bihn, Sr. Principal Planner
Michael Lappen, Planning Associate
City of Torrance Planning Department
3031 Torrance Blvd.
Torrance, CA 90503
•

The National Register of Historic Places Program is administered in California by the State Office of Historic Preservation. The property indicated above has been submitted for the Register.

- ☒ The State Historical Resources Commission will consider this property and make a judgment as to whether or not it meets the criteria on November 1, 1991 at 9:00 am in Room 302, County Administration Building, San Diego, California
- ☐ The State Historical Resources Commission recommended the nomination of the property to the National Register. It will be presented to the State Historic Preservation Officer for formal nomination. The final decision will be made by the Keeper of the National Register, National Park Service, U.S. Department of the Interior, P.O. Box 37127, Washington, D.C. 20013-7127.
- ☐ The property was placed on the National Register _____
- ☐ The property was determined eligible for placement on the National Register _____

Placement on the National Register affords a property the honor of inclusion in the nation's official list of cultural resources worthy of preservation and provides a degree of protection from adverse effects resulting from federally funded or licensed projects. Registration provides a number of incentives for preservation of historic properties, including special building codes to facilitate the restoration of historic structures, and certain tax advantages. There are no restrictions placed upon a private property owner with regard to normal use, maintenance, or sale of a property listed in the National Register; however, proposals to demolish registered properties may require a standard review in compliance with local ordinances or the California Environmental Quality Act.

Sincerely,

Kathryn Gualtieri
State Historic Preservation Officer

Fern Avenue School
Torrance, Los Angeles County
Staff Evaluation

The Fern Avenue School is a large, one- and two-story stucco clad building with red tile roof. It was constructed in 1932 in the Spanish Colonial Revival style. The entryway is particularly attractive, with churrigueresque elements framing a second story door and balconet. In 1946 a sympathetic annex was added to the main building, linked to it by an arcaded hyphen. The addition does not detract from the architectural presence of the property overall. The only other notable alteration is the replacement of some of the wooden windows with aluminum ones.

The building appears to be significant under criteria A and C. It is Torrance's oldest extant elementary school and was the focal point of a dispute between the City of Torrance and Los Angeles County over local control of the schools. Throughout the 1930s and beyond, Fern Avenue School served Torrance's many Japanese and Hispanic families, providing an important institution of acculturation. Architecturally, the school is one of Torrance's major examples of Spanish Colonial Revival styling and one of the few such non-residential buildings that retains integrity. Staff recommends listing at the local level of significance.

Maryln Bourne Lortie
October 8, 1991

OFFICE OF HISTORIC PRESERVATION
DEPARTMENT OF PARKS AND RECREATION
P.O. BOX 942896
SACRAMENTO 94296-0001
(916) 445-8006
FAX: (916) 322-6377



August 28, 1991

Michael G. Bihn, Senior Principal Planner
Michael Lappen, Planning Associate
City of Torrance Planning Department
3031 Torrance Blvd.
Torrance, CA 90503

Dear Mr. Bihn and Mr. Lappen:

I have reviewed your National Register nomination for the Fern Avenue School in Sacramento. If you can return a revised nomination to our office by October 1, we will schedule the nomination for the November 1 meeting of the State Historical Resources Commission. The meeting will take place in San Diego.

Please refer to the following when preparing revisions:

1. Resource Count and Geographic Data

Since a majority of the buildings on the site are non-contributing, it would be better to draw a boundary of convenience around the nominated building and its immediate setting. Please provide a scale map (with north arrow) that delineates the smaller boundary. The boundary description (Item 10) should state: "Refer to enclosed scale map." The boundary justification should state: "The boundaries are drawn to encompass the historic resource and its immediate setting." Please revise acreage accordingly and also the resource count on page 1.

2. Description

Please see Bulletin 16 (pages 58-59) for instructions on completing the "Materials" section.

3. Significance

Since you are claiming both historic and architectural significance for the building, the applicable criteria are A and C.

The period of significance cannot begin before the construction of the building. It is likely that the appropriate period of significance is 1932 to 1941 (fifty years ago). The period of significance can be justified in the text with a statement such as: "Although the Fern Avenue School continues to be important to the community into the present, the period of significance

Bihn & Lappen
August 28, 1991
Page Two

has been ended at 1941, fifty years ago." (Any significance beyond that date must be justified as exceptional.)

Significant date should be 1932.

In discussing the architectural significance of the school, a broader context should be established. Please compare the Fern Avenue School to other large non-residential buildings in Torrance. What other examples of Spanish Colonial Revival architecture exist within that category? What is the relative architectural importance of the school? Also, is the school the oldest elementary school building still in existence in Torrance? What other historic school buildings are still extant in Torrance?

Your arguments for historic significance are a little cloudy and could use some clarification. Your text indicates that the school was a "focal point as Torrance's residential population grew to the west." Please provide enough additional information about the development of the city to specifically demonstrate the nature of the school as "focal point" and why this was important to the history of the community. Also, the text indicates that the school served a minority population and provided students a "shared experience in American society." You will need to go further to establish that the school had a special contribution beyond what would be expected in any school serving a minority population. Perhaps more information about the social composition of the city and its power relationships would be helpful.

Please note that you need develop significance for only one of the areas -- architecture, education, social history -- in order to justify listing. You may choose to expand on one or more of these themes in your revision.

If you have any questions, please do not hesitate to phone me at (916) 322-9601.

Sincerely,

Maryln Bourne Lortie
State Historian II

OFFICE OF HISTORIC PRESERVATION

DEPARTMENT OF PARKS AND RECREATION

POST OFFICE BOX 942896

SACRAMENTO, CALIFORNIA 942896-0001

(916) 445-8006



• August 29, 1991

• See Attached

• City Manager
City of Torrance
City Hall, 3031 Torrance Blvd.
Torrance, CA 90503

•

The above property will be considered by the State Historical Resources Commission for nomination to the National Register of Historic Places. The National Register is the Federal Government's official list of historic buildings and other cultural resources, worthy of preservation. Listing in the National Register provides recognition and assists in preserving our Nation's heritage. Enclosed is a copy of the criteria under which properties are evaluated.

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—Qualification for Federal Grants for historic preservation when funds are available.

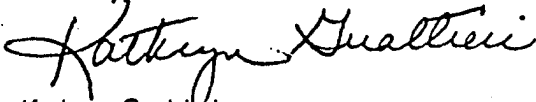
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County Administration Building, San Diego, California

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Sincerely,



Kathryn Gualtieri
State Historic Preservation Officer

Enclosure

OFFICE OF HISTORIC PRESERVATION

DEPARTMENT OF PARKS AND RECREATION

POST OFFICE BOX 942896

SACRAMENTO, CALIFORNIA 942896-0001

(916) 445-8006



• August 29, 1991

19-178516

Los Angeles County Historic
Landmarks/Records Commission
500 West Temple St., Rm 383
Los Angeles, CA 90012

• See Attached

The above property will be considered by the State Historical Resources Commission for nomination to the National Register of Historic Places. The National Register is the Federal Government's official list of historic buildings and other cultural resources, worthy of preservation. Listing in the National Register provides recognition and assists in preserving our Nation's heritage. Enclosed is a copy of the criteria under which properties are evaluated.

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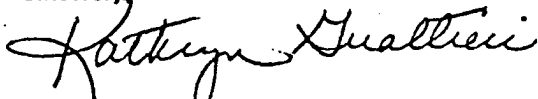
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November 1, 1991 at 9:00 a.m., in Room 302

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Sincerely,



Kathryn Gualtieri
State Historic Preservation Officer

Enclosure

OFFICE OF HISTORIC PRESERVATION

DEPARTMENT OF PARKS AND RECREATION

POST OFFICE BOX 942896

SACRAMENTO, CALIFORNIA 942896-0001

(916) 445-8006



• August 29, 1991

•

Chair, Board of Supervisors
Los Angeles County
500 W Temple Street, Rm. 358
Los Angeles, CA 90012

•

• See Attached

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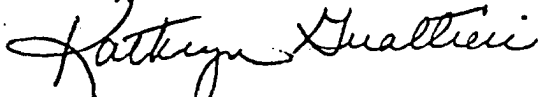
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Sincerely,



Kathryn Gualtieri
State Historic Preservation Officer

Enclosure

OFFICE OF HISTORIC PRESERVATION

DEPARTMENT OF PARKS AND RECREATION

P.O. BOX 942896

SACRAMENTO 94296-0001

(916) 445-8006

FAX: (916) 322-6377



01/15/92

Mr. Jerry Rogers, Keeper
National Register of Historic Places
National Park Service
U.S. Department of the Interior
P.O. Box 37127
Washington, D.C. 20013-7127

Dear Mr. Rogers:

Subject: Nomination to the National Register of Historic Places
FERN AVENUE SCHOOL

We are submitting the above stated property for nomination to the National Register of Historic Places. The property is located in or near: TORRANCE
in the county of: LOS ANGELES
in: California.

Sincerely,

Mary L. Core
for Steade R. Craigo, AIA, Acting
State Historic Preservation Officer

Enclosures



IN REPLY REFER TO

United States Department of the Interior

NATIONAL PARK SERVICE

P.O. BOX 37127

WASHINGTON, D.C. 20013-7127

MAR 17 1992

OHP

Notified
3-28-92
19-178516



The Director of the National Park Service is pleased to inform you that the following properties have been entered in the National Register of Historic Places. For further information call 202/343-9542.

FEB 28 1992

WEEKLY LIST OF ACTIONS TAKEN ON PROPERTIES: 2/17/92 THROUGH 2/21/92

KEY: State, County, Property Name, Address/Boundary, City, Vicinity, Reference Number NHL Status, Action, Date, Multiple Name

ALABAMA, ELMORE COUNTY, East Wetumpka Commercial Historic District, Roughly, Company St. from Spring St. to E. Bridge St. and E. Bridge and Commerce Sts. from Main to Hill Sts., Wetumpka, 92000055, NOMINATION, 2/20/92

CALIFORNIA, LOS ANGELES COUNTY, Fern Avenue School, 1314 Fern Ave., Torrance, 92000067, NOMINATION, 2/20/92

COLORADO, GRAND COUNTY, Timber Creek Road Camp Storage Building, Timber Creek Rd., Estes Park vicinity, 87001135, REMOVAL, 2/11/92 (Rocky Mountain National Park MRA)

FLORIDA, MONROE COUNTY, AFRICAN QUEEN, 99701 Overseas Hwy., Key Largo, 91001771, NOMINATION, 2/18/92

ILLINOIS, COOK COUNTY, Humboldt Park, Roughly bounded by N. Sacramento and Augusta Blvds., and N. Kedzie, North and N. California Aves. and W. Division St., Chicago, 92000074, NOMINATION, 2/20/92 (Chicago Park District MPS)

ILLINOIS, TAZEWELL COUNTY, Farm Creek Section, S side of Farm Cr., East Peoria vicinity, 91002039, NOMINATION, 2/06/92

INDIANA, CLAY COUNTY, Coal Company Store, S Harmony Rd., Harmony, 86001121, REMOVAL, 2/07/92

KENTUCKY, PULASKI COUNTY, Parker House, 206 N. Vine St., Somerset, 84001970, ADDITIONAL DOCUMENTATION, 2/20/92 (Pulaski County MRA)

MARYLAND, ANNE ARUNDEL COUNTY, Douglass Summer House, 3200 Wayman Ave., Highland Beach, 92000069, NOMINATION, 2/20/92

MARYLAND, CHARLES COUNTY, Acquinsicke, Billingsley Rd. W of Jct. with MD 228, Pomfret vicinity, 92000070, NOMINATION, 2/20/92

NEW JERSEY, MERCER COUNTY, Herring, Donald Grant, Estate, 52, 72 and 75 Arretton Rd., Princeton Township, Rocky Hill vicinity, 91001927, NOMINATION, 1/17/92

NEW YORK, MONROE COUNTY, Baptist Temple Building, 14 Franklin St., Rochester, 85003630, ADDITIONAL DOCUMENTATION, 2/20/92 (Inner Loop MRA)

NEW YORK, WYOMING COUNTY, Cates, Seth M. House, 15 Perry Ave., Warsaw, 92000031, NOMINATION, 2/21/92

OKLAHOMA, OKLAHOMA COUNTY, Cover Cemetery, Cover Rd. between Douglas and Post Rds., Edmond, 91001895, NOMINATION, 12/27/91

OREGON, DESCHUTES COUNTY, Slover, B. A. and Ruth House, 1 NW Rocklyn Rd., Bend, 92000061, NOMINATION, 2/20/92

OREGON, JACKSON COUNTY, Pedigree, S. and Sarah J. House, 407 Scenic Ave., Ashland, 92000063, NOMINATION, 2/20/92

OREGON, JACKSON COUNTY, Pell, Emil and Alice Applegate House, 52 Granite St., Ashland, 92000062, NOMINATION, 2/20/92

VIRGINIA, HIGHLAND COUNTY, Cifax Rural Historic District, Jct. of VA 644 and VA 643 and surrounding valley area, Cifax, 92000052, NOMINATION, 2/20/92

WISCONSIN, BROWN COUNTY, Lawton, C. A. Company, 233 N. Broadway, De Pere, 91001985, NOMINATION, 1/30/92

WISCONSIN, WAUKESHA COUNTY, First Congregational Church, 100 E. Broadway, Waukesha, 91001991, NOMINATION, 1/30/92

WISCONSIN, WOOD COUNTY, Wahle--Laird House, 208 S. Cherry Ave., Marshfield, 91001988, NOMINATION, 1/30/92

OFFICE OF HISTORIC PRESERVATION
DEPARTMENT OF PARKS AND RECREATION
P.O. BOX 942896
SACRAMENTO 94296-0001
(916) 445-8006
FAX: (916) 322-6377
(916) 653-6624
FAX (916) 653-9824



March 23, 1992

Los Angeles County Historic
Landmarks/Records Commission
500 West Temple Street, Room 383
Los Angeles, CA 90012

RE: Fern Avenue School
1314 Fern Avenue, Torrance
National Register of Historic Places

The National Register of Historic Places Program is administered in California by the State Office of Historic Preservation. The property indicated above was placed on the National Register February 20, 1992.

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There are no restrictions placed upon a private property owner with regard to normal use, maintenance, or sale of a property listed in the National Register. However, proposals to demolish registered properties may require a standard review in compliance with local ordinances or the California Environmental Quality Act. In addition, registered properties damaged due to a natural disaster may be subject to the provisions of Section 5028 of the Public Resources Code regarding demolition or significant alterations, if imminent threat to life safety does not exist.

If you have questions or require further information, please contact Marci Breisacher, (916) 653-8954.

Sincerely,

Original Signed by

Steade R. Craigo, A.I.A., Acting
State Historic Preservation Officer

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March 23, 1992

Mr. Don Lee, President
Torrance Historical Society
1345 Post Avenue
Torrance, CA 90501

RE: Fern Avenue School
1314 Fern Avenue, Torrance
National Register of Historic Places

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March 23, 1992

Torrance Unified School
2335 Plaza Del Amo
Torrance, CA 90501

RE: Fern Avenue School
1314 Fern Avenue, Torrance
National Register of Historic Places

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March 23, 1992

Ms. Katy Geissert, Mayor
City of Torrance
3031 Torrance Boulevard
Torrance, CA 90509-2970

RE: Fern Avenue School
1314 Fern Avenue, Torrance
National Register of Historic Places

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(916) 653-6624
FAX (916) 653-9824

March 23, 1992

Mr. Edward J. Richardson
Torrance Unified School District
2335 Plaza Del Amo
Torrance, CA 90509

RE: Fern Avenue School
1314 Fern Avenue, Torrance
National Register of Historic Places

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March 23, 1992

Los Angeles County Historic
Landmarks/Records Commission
500 West Temple Street, Room 383
Los Angeles, CA 90012

RE: Fern Avenue School
1314 Fern Avenue, Torrance
National Register of Historic Places

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March 23, 1992

Chair, Board of Supervisors
Los Angeles County
500 West Temple Street, Room 383
Los Angeles, CA 90012

RE: Fern Avenue School
1314 Fern Avenue, Torrance
National Register of Historic Places

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March 23, 1992

City Manager
City of Torrance
City Hall, 3031 Torrance Boulevard
Los Angeles, CA 90503

RE: Fern Avenue School
1314 Fern Avenue, Torrance
National Register of Historic Places

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March 23, 1992

Deroy J. Jackson, City Manager
City of Torrance
City Hall, 3031 Torrance Boulevard
Los Angeles, CA 90503

RE: Fern Avenue School
1314 Fern Avenue, Torrance
National Register of Historic Places

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(916) 653-6624
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March 23, 1992

Michael G. Bihn, Sr. Principal Planner
Michael Lappen, Planning Associate
City of Torrance Planning Department
3031 Torrance Boulevard
Los Angeles, CA 90503

RE: Fern Avenue School
1314 Fern Avenue, Torrance
National Register of Historic Places

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State Historic Preservation Officer

HISTORIC RESOURCES INVENTORY

(State use only)

Ser	Site	Mo.	Yr.
UTM	11/3744720/	Q	NR 3 SHL
Lat	378700	Lo.	Era Sig
Adm	T2 T3	Cat	HABS HAER Fed

IDENTIFICATION

1. Common name: Gill Bridge 190501-122
2. Historic name, if known: Torrance Bridge
3. Street or rural address Located near corner of Torrance Blvd. & Western Ave.
City: Torrance ZIP: 90501 County: Los Angeles
4. Present owner, if known: City of Torrance Address: 3031 Torrance Blvd.
City: Torrance ZIP: 90503 Ownership is: Public ☒ Private ☐
5. Present Use: Bridge Original Use: Same
Other past uses: None

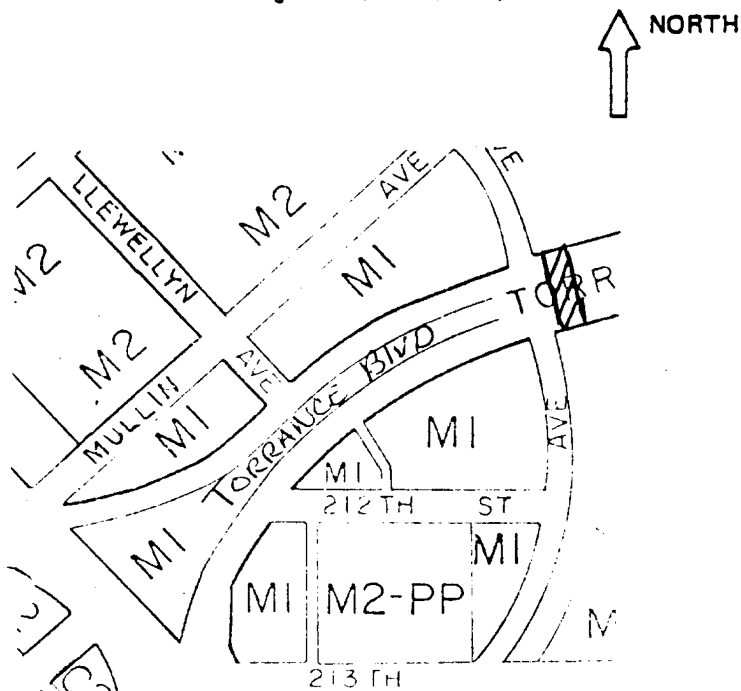
DESCRIPTION

6. Briefly describe the present physical appearance of the site or structure and describe any major alterations from its original condition:

This structure consists of a concrete railroad bridge. The design is striking through the architect's use of interrelated and balanced arches. The bridge was designed to accommodate three separate transportation functions: pedestrian, railway, and auto. These functions were realized in a two level design framework. The structure continues to perform these functions in an unaltered condition.

The bridge is in a highly visible location on Torrance Boulevard. The surrounding landscape and greenery adds greatly to the visual impact of this structure.

7. Locational sketch map (draw and label site and surrounding streets, roads, and prominent landmarks):



8. Approximate property size:

Lot size (in feet) Frontage 50
Depth 150;
or approx. acreage _____

9. Condition: (check one)

a. Excellent ☐ b. Good ☐ c. Fair ☒
d. Deteriorated ☐ e. No longer in existence ☐

10. Is the feature a. Altered? ☐ b. Unaltered? ☒

11. Surroundings: (Check more than one if necessary)

a. Open land ☐ b. Scattered buildings ☐
c. Densely built-up ☐ d. Residential ☒
e. Commercial ☒ f. Industrial ☒
g. Other ☐

12. Threats to site:

a. None known ☒ b. Private development ☐
c. Zoning ☐ d. Public Works project ☐
e. Vandalism ☐ f. Other ☐

13. Date(s) of enclosed photograph(s): 12-78

NOTE: The following (Items 14-19) are for structures only.

14. Primary exterior building material: a. Stone ☐ b. Brick ☐ c. Stucco ☐ d. Adobe ☐ e. Wood ☐
f. Other ☒ Concrete
15. Is the structure: a. On its original site? ☒ b. Moved? ☐ c. Unknown? ☐
16. Year of initial construction 1912 This date is: a. Factual ☐ b. Estimated ☐
17. Architect (if known): Irving Gill
18. Builder (if known): Unknown
19. Related features: a. Barn ☐ b. Carriage house ☐ c. Outhouse ☐ d. Shed(s) ☐ e. Formal garden(s) ☐
f. Windmill ☐ g. Watertower/tankhouse ☐ h. Other ☐ i. None ☒

SIGNIFICANCE

20. Briefly state historical and/or architectural importance (include dates, events, and persons associated with the site when known):

One of the City's earliest structures was this bridge designed by Irving Gill. Its location at Torrance's eastern border made it the symbolic and literal gateway to the City.

The relation of this structure to the Olmsted Plan, the fact that it was designed by Gill, and its long and continued use in its original capacity, are the three most important aspects of this structure. It is a landmark in the community.

21. Main theme of the historic resource: (Check only one): a. Architecture ☒ b. Arts & Leisure ☐
c. Economic/Industrial ☐ d. Exploration/Settlement ☐ e. Government ☐ f. Military ☐
g. Religion ☐ h. Social/Education ☐

22. Sources: List books, documents, surveys, personal interviews, and their dates:

Tax Assessor's Rolls;
History of Torrance, Torrance Unified School District.

23. Date form prepared: Jan. 1979 By (name): Roger Hatheway and John Chase
Address: 3031 Torrance Blvd. City: Torrance ZIP: 90503
Phone: (213) 328-5310, X295 Organization: City of Torrance

(State Use Only)

HISTORIC RESOURCES INVENTORY

0501 - 122 - 0

Ser. No. _____
HABS _____ HAER _____ Loc _____ SHL No. _____ NR Status 3
UTM: A 11/3744/20/378700 C _____
B _____ D _____

IDENTIFICATION

- (222)
1. Common name: Irving Gill - Torrance Boulevard Bridge (88-19-055)
 2. Historic name: Dominquez Land Company's "El Prado Bridge"
 3. Street or rural address: 1800 block of Torrance Boulevard (NE of Torrance/Cabrillo
City Torrance Zip 90503 County Los Angeles
 4. Parcel number: 117
 5. Present Owner: City of Torrance Address: 3031 Torrance Boulevard
City Torrance, CA Zip 90503 Ownership is: Public x Private _____
 6. Present Use: none Original use: Bridge

DESCRIPTION

- 7a. Architectural style:
- 7b. Briefly describe the present *physical appearance* of the site or structure and describe any major alterations from its original condition:

see attachement

Attach Photo Envelope Here

NO PHOTO

8. Construction date:
Estimated 1912 Factual _____
9. Architect Irving Gill
10. Builder Unknown
11. Approx. property size (in feet)
Frontage 50 Depth 150
or approx. acreage _____
12. Date(s) of enclosed photograph(s)
1921, 1988

13. Condition: Excellent ___ Good ___ Fair ___ Deteriorated ___ No longer existence ___
14. Alterations: Removal of telephone poles; addition of railing and height signs
15. Surroundings: (Check more than one if necessary) Open land ___ Scattered buildings ___ Densely built-up X
Residential X Industrial X Commercial X Other: Redevelopment area of combined use
16. Threats to site: None known ___ Private development ___ Zoning ___ Vandalism X
Public Works project ___ Other: ___
17. Is the structure: On its original site? X Moved? ___ Unknown? ___
18. Related features: ___

SIGNIFICANCE

19. Briefly state historical and/or architectural importance (include dates, events, and persons associated with the site.)

see attachemnt

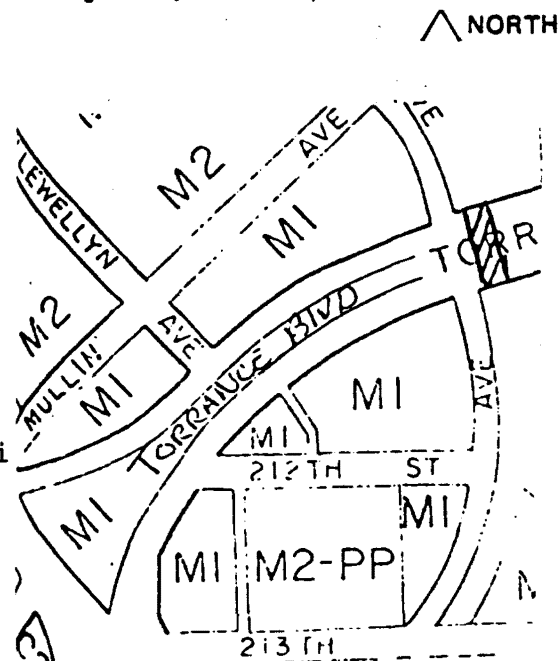
20. Main theme of the historic resource: (If more than one is checked, number in order of importance.)
- Architecture 1 Arts & Leisure ___
Economic/Industrial ___ Exploration/Settlement 2
Government ___ Military ___
Religion ___ Social/Education 3

21. Sources (List books, documents, surveys, personal interviews and their dates).

The Torrance Historic Resources Survey:
Research Guide & Field Manual;
Chase, John & Roger Hatheway
Historic Torrance: a pictorial history
of Torrance, California; Shanahan, Dennis

22. Date form prepared 12/01/88
By (name) Toni Alario
Organization City of Torrance
Address: 3031 Torrance Boulevard
City Torrance, CA Zip 90503
Phone: (213) 618-2941

Locational sketch map (draw and label site and surrounding streets, roads, and prominent landmarks):



7B. BRIEF DESCRIPTION OF PHYSICAL APPEARANCE AND ALTERATIONS

- o spans 152 feet across Torrance Boulevard, just northeast of the Torrance/Cabrillo Avenue intersection
- o A deck-arch railroad bridge, with six arches facing the Boulevard; a pair of tall, half-circle arches in the center, on each side of these are big segmental arches, and then, at each end, is another arch, like the middle ones but lower. In addition to the six arches spanning Torrance Boulevard, there are five perpendicular arches underneath the bridge.
- o Architecturally representative of Modern Movement and made of poured-concrete construction from local materials
- o Several cracks resulting from age and weather span the bridge but are camouflaged by vines
- o Alterations are minor; they consist of the removal of one telephone pole attached to each side of the parapet, the addition of a wooden guard rail on each side of the deck, and a small height sign posted on each side of the bridge facing the vehicular traffic

Summary

The Irving Gill Bridge is historically significant under criteria '3': it embodies the distinctive characteristics of a type, period, and method of construction. Moreover, the bridge represents the work of a master. The bridge is significant in three areas: community planning, architecture, and engineering.

First, the bridge played a prominent role in community planning and is the only remaining unaltered structure representing the original Torrance General Plan. Secondly, the architecture is important since the structure was designed by the master Irving Gill and reflected his direction towards greater simplicity which significantly contributed to the modern movement of Architecture. Third, engineering is significant due to use of innovative construction materials. Finally, the bridge has recently been selected as logo for the city's 75th Anniversary therefore gaining further symbolic value as the gateway to Torrance.

Community Planning and Development

In January of 1912, Dominguez Land Company, headed by Jared Sidney Torrance, hired John C. and Frederick Law Olmsted. These well respected landscape architects were commissioned to prepare the first general plan for a model industrial townsite which later became the City of Torrance.

Irving Gill was appointed Chief Architect of Torrance in June of 1912. Throughout the following year, Gill implemented the Olmsted Brothers' plan and designed several buildings; most of which were built, including the railroad bridge.

Torrance was designed as a balanced city by combining industry, commercial and residential land use. Location of the bridge is directly linked to the original general plan which was based upon the shape of a 'Y' figure. Industry was located on the outside of the area bounded by the figure, while commercial and residential structures were located within the open area formed by the 'Y' (Roger Hatheway, 1979). The bridge was carefully placed near the base of the 'Y', serving as the original gateway to the city.

Narrative from Historic Torrance: A Pictorial History of Torrance, California describes the intent and purpose of the bridge:

"The layout of the town, with its view of Mount Baldy and factories to the east of houses and stores, so prevailing winds would carry factory smoke away from them, also called for a graceful

concrete bridge over Torrance Boulevard, a bridge designed by Gill. The bridge kept trains from blocking major streets as they travelled to and from the depot and kept their noise and vibrations away from the new residential areas."

In summary, the bridge symbolizes the Olmsted-Gill original intent to thoughtfully plan a progressive industrial city for the early twentieth century.

Architecture

During this era, imitation of historical decoration was the predominant architectural approach. In contrast, Gill pioneered the modern movement by daring to simplify design and demonstrate heightened sensitivity of form. In his own words, "any deviation from simplicity results in a loss of dignity. Ornaments tend to cheapen rather than enrich."

The bridge was Gill's first design built in Torrance and well represents his passion for simple form (the straight line, arch and circle). Furthermore, the bridge has even aged in the Gill tradition by vines graciously breaking the severity of the architectural lines.

"We should build our house simple, plain and substantial as a boulder, then leave the ornamentation of it to Nature, who will tone it with lichens, chisel it with storms, make it gracious and friendly with vines and flower shadows as she does the stone in the meadow." (Gill, 1916)

Upon conferring with Steve Mikesell, Architectural Historian for CalTrans, we have found that only a handful of bridges in the State of California have been designed by Architects. Mikesell has participated in the recent Federal Highway Survey of every vehicular bridge in the state (2,000 observations).

Based on this research and his personal observation of non-vehicular bridges, Mikesell has made additional comments. First, there have been no other bridges designed by Gill in the Los Angeles area. Moreover, the Green and Green bridge in Pasadena is the only other bridge designed by an Architect in the entire Los Angeles region.

Engineering

This bridge is also distinctive because reinforced concrete was used. This innovative type of construction was attractive to local authorities in the early 20th century since locally produced materials could be used in construction which would lower maintenance costs. Use of these materials reflect the physical and philosophical

elements of Irving Gill's ideals.

"Concrete was a material to which Gill was especially sympathetic; its plasticity appealed to him, as well as its durability...Reinforced concrete had been employed in scattered buildings in the U.S. since 1877...but Gill was one of the first to bring architectural convictions to the system and to develop a body of detailing which would make it accessible for general use. He was an inventor out of necessity." (McCoy, 1959)

Period of Significance

The period of significance (1912-1914) are those years in which Pacific Electric Railroad constructed the single-track lines to Torrance Townsite. During this period, the Torrance lines were placed in operation.

The tracks crossing over the Irving Gill Bridge led to the U.S. Steel Company. The set of tracks passing underneath, provided passenger service to Torrance at the nearby Pacific Electric Depot. Pacific Electric continued to own and operate these lines until November 13, 1965 when they merged with Southern Pacific Transportation Company.

Since then, the U.S. Steel factory has been demolished, and the tracks across the Bridge have been abandoned. The trackage on each side of the bridge has been removed for new development, but the tracks on the bridge remain.

Community Commitment

The city council has passed a resolution expressing the city's commitment to the preservation of the historically and architecturally significant bridge (Attachment #/,.

On November 16, 1986, following four years of pursuit, the City of Torrance successfully obtained ownership of the bridge by indenture with Southern Pacific for the specific purpose of preserving the bridge. Southern Pacific has reserved the right to maintain and operate the track structure. A copy of the indenture agreement is attached to this registration form (Attachment #3).

The Torrance Historical Society consistently displays the bridge on brochure covers. This organization has expressed concern over the future of the bridge through written endorsement (Attachment #2).

United States Department of the Interior
National Park ServiceNational Register of Historic Places
Registration Form

This form is for use in nominating or requesting determinations of eligibility for individual properties or districts. See instructions in *Guidelines for Completing National Register Forms* (National Register Bulletin 16). Complete each item by marking "x" in the appropriate box or by entering the requested information. If an item does not apply to the property being documented, enter "N/A" for "not applicable." For functions, styles, materials, and areas of significance, enter only the categories and subcategories listed in the instructions. For additional space use continuation sheets (Form 10-900a). Type all entries.

1. Name of Property

historic name Pacific Electric Railroad Bridge
other names/site number Gill, Irving Bridge

2. Location

street & number Torrance Boulevard and Bow Street N/A not for publication
city, town Torrance N/A vicinity
state California code CA county Los Angeles code 037 zip code 90503

3. Classification

Ownership of Property

☐ private
☒ public-local
☐ public-State
☐ public-Federal

Category of Property

☐ building(s)
☐ district
☐ site
☒ structure
☐ object

Number of Resources within Property

Contributing	Noncontributing
0	0 buildings
0	0 sites
1	0 structures
0	0 objects
1	0 Total

Name of related multiple property listing:

N/A

Number of contributing resources previously
listed in the National Register 0

4. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act of 1966, as amended, I hereby certify that this
☒ nomination ☐ request for determination of eligibility meets the documentation standards for registering properties in the
National Register of Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60.
In my opinion, the property ☒ meets ☐ does not meet the National Register criteria. ☐ See continuation sheet.

Signature of certifying official

Date

6/7/89

State or Federal agency and bureau

In my opinion, the property ☐ meets ☐ does not meet the National Register criteria. ☐ See continuation sheet.

Signature of commenting or other official

Date

State or Federal agency and bureau

5. National Park Service Certification

I, hereby, certify that this property is:

- ☐ entered in the National Register.
☐ See continuation sheet.
- ☐ determined eligible for the National
Register. ☐ See continuation sheet.
- ☐ determined not eligible for the
National Register.
- ☐ removed from the National Register.
- ☐ other, (explain:)

Signature of the Keeper

Date of Action

6. Function or Use

Historic Functions (enter categories from instructions)

TRANSPORTATION/rail-related
road-related
pedestrian-related

Current Functions (enter categories from instructions)

TRANSPORTATION/rail-related
road-related
pedestrian-related

7. Description

Architectural Classification
(enter categories from instructions)

Modern Movement
Other: Deck-arch bridge

Materials (enter categories from instructions)

foundation Concrete
walls Concrete
roof Concrete
other

Describe present and historic physical appearance.

The Irving Gill Bridge still serves as both the literal and symbolic gateway to the City of Torrance, as it was originally intended when designed and built as one of the first structures in 1913 during the founding of the City of Torrance. This deck-arch reinforced concrete railroad bridge spanning Torrance Boulevard was built to allow three modes of transportation to operate simultaneously. The striking structure represents the original urban design for the City of Torrance and was designed by Architect Irving Gill who has since gained broad recognition for pioneering California's early modern movement in architectural styles (Handbook for Local Communities, COHP). The Irving Gill Bridge has retained historical integrity by sustaining only minor alterations unlike other Irving Gill structures in Torrance.

The bridge was designed 76 years ago to allow transportation of materials and goods to and from factories without blocking traffic. The structure is aptly described in The City Observed Los Angeles:

"The best introduction to Gill's original part of Torrance is through one of the six arches in his railroad bridge...This delicately proportioned, poured-concrete structure serves as a kind of latter-day city gate, accommodating every means of transportation into town as well as the Southern Pacific Railroad tracks across the top. A pair of tall, half-circle arches in the center served Pacific Electric streetcars; on each side of them is a big segmental arch, wide enough for two lanes of automobiles; and then, at each end, is another arch, like the middle ones but lower, for pedestrians. This bridge is typical of Gill's best work, straightforward and bold, with flat surfaces ornamented only by form marks; but with its little arches and big, it seems at once grand and almost fragile. There is, too, a sort of off-center energy in the rhythm of the openings, set against the varying levels of the several roadways, the vines that crawl all around and the groves of eucalyptus at each end. You wouldn't guess, at first glance, that the whole wall is unerringly symmetrical."

In addition to the six arches spanning Torrance Boulevard, there are five perpendicular arches underneath the bridge. The specific dimensions of the

8. Statement of Significance

Certifying official has considered the significance of this property in relation to other properties:

☐ nationally ☐ statewide ☒ locally

Applicable National Register Criteria ☐ A ☐ B ☒ C ☐ D

Criteria Considerations (Exceptions) ☐ A ☐ B ☐ C ☐ D ☐ E ☐ F ☐ G

Areas of Significance (enter categories from instructions)

Engineering
Architecture
Community Planning and Development

Period of Significance

1912

Significant Dates

1912

Cultural Affiliation

N/A

Significant Person

N/A

Architect/Builder

Gill, Irving/Pacific Electric Ry.

State significance of property, and justify criteria, criteria considerations, and areas and periods of significance noted above.

The Irving Gill Bridge is historically significant under criterion C. It embodies the distinctive characteristics of a type, period, method of construction, and represents the work of a master. The bridge is significant in three areas: community planning, architecture, and engineering. First, it played a prominent role in community planning and is the only remaining unaltered structure representing the original Torrance General Plan. Secondly, the architecture is important because the structure was designed by the master Irving Gill and reflects his direction toward greater simplicity which significantly contributed to the modern movement in architecture. Thirdly, the bridge is significant from an engineering standpoint for its highly unusual use of non-load-bearing walls to create the appearance of an arched bridge. Finally, the bridge has recently been selected as logo for the City's 75th Anniversary, therefore gaining further symbolic value as the gateway to Torrance.

Community Planning and Development

In January of 1912, Dominguez Land Company, headed by Jared Sidney Torrance, hired John C. and Frederick Law Olmsted. These well-respected landscape architects were commissioned to prepare the first general plan for a model industrial townsite which later became the City of Torrance.

Irving Gill was appointed Chief Architect of Torrance in June of 1912. Throughout the following year, Gill implemented the Olmsted Brothers' plan and designed several buildings; most of which were built, including the railroad bridge.

Torrance was designed as a balanced City by combining industry, commercial, and residential land use. Location of the bridge is directly linked to the original general plan which was based upon the shape of a 'Y' figure. Industry was located on the outside of the area bounded by the figure, while commercial and residential structures were located within the open area formed by the 'Y' (Roger Hatheway, 1979). The bridge was carefully placed near the base of the 'Y', serving as the original gateway to the City.

☒ See continuation sheet

9. Major Bibliographical References

Gill, Irving. "Home of the Future." The Craftsman. May 1916.

Hatheway, Roger. "Irving Gill, Torrance and the Industrial Dream."
L.A. Architect, July 1979.

Hatheway, Roger, and Chase, John. "Torrance Historical Resources
Survey." May 1979.

McCoy, Ester. Five California Masterbuilders. Reinhold 1959.

Moore, Charles, Becker, Peter, and Campbell, Regula.
The City Observed: Los Angeles. Vintage Books 1984.

(Continued)

☒ See continuation sheet

Previous documentation on file (NPS):

- ☐ preliminary determination of individual listing (36 CFR 67)
has been requested
- ☐ previously listed in the National Register
- ☐ previously determined eligible by the National Register
- ☐ designated a National Historic Landmark
- ☐ recorded by Historic American Buildings
Survey # _____
- ☐ recorded by Historic American Engineering
Record # _____

Primary location of additional data:

- ☐ State historic preservation office
- ☐ Other State agency
- ☐ Federal agency
- ☐ Local government
- ☐ University
- ☒ Other

Specify repository:

Southern Pacific Transportation Company

10. Geographical Data

Acreage of property .11

UTM References

A 11 378700 3744720
Zone Easting Northing

C

B
Zone Easting Northing

D

☐ See continuation sheet

Verbal Boundary Description

The boundary of the Irving Gill Bridge is shown on the accompanying tract map entitled "Southern Pacific Transportation Company (Torrance Properties)". The scale of the map is 1" = 100' (Attachment #4).

☐ See continuation sheet

Boundary Justification

The boundary chosen is the boundary of the property donated to the City of Torrance, by indenture agreement, from Southern Pacific Transportation Company (Attachment #4).

☐ See continuation sheet

11. Form Prepared By Michael G. Bihn/Principal Planner

name/title Kim Payne/Planning Assistant

organization City of Torrance Planning Dept./Redevelop. date 4/19/89

street & number 3031 Torrance Boulevard telephone (213) 618-5990

city or town Torrance state CA zip code 90503

United States Department of the Interior
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**National Register of Historic Places
Continuation Sheet**

Section number 7 Page 2

bridge are shown on enclosed copies of the original plans, titled "El Prado Bridge" (Attachment #2).

Alterations and Deterioration

Alterations to the bridge are minor and are estimated to be made in the early 1950s. They consist of the removal of one telephone pole attached to each side of the parapet, (photograph #1) and the addition of a wooden guardrail on each side of the deck.

There is a small height sign posted on each side of the bridge facing the vehicular traffic (photograph #3).

The Irving Gill Bridge is gradually suffering from deterioration. Several cracks span the bridge as a result of age and weather. However, most signs of deterioration are camouflaged by vines (photographs #9 and #10).

(See bridge photographs #1-10.)

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National Park Service

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Continuation Sheet

Section number 8 Page 2

Narrative from Historic Torrance: A Pictorial History of Torrance, California describes the intent and purpose of the bridge:

"The layout of the town, with its view of Mount Baldy and factories to the east of houses and stores, so prevailing winds would carry factory smoke away from them, also called for a graceful concrete bridge over Torrance Boulevard, a bridge designed by Gill. The bridge kept trains from blocking major streets as they travelled to and from the depot and kept their noise and vibrations away from the new residential areas."

In summary, the bridge symbolizes the Olmsted-Gill original intent to thoughtfully plan a progressive industrial city for the early 20th century. Other structures representing the original Torrance General Plan have been destroyed or altered significantly. Existing Gill structures include the Pacific Electric Depot, the El Roi Tan Hotel, several manufacturing buildings, and a few homes. All of these buildings have been structurally altered as described in the 1979 Torrance Historic Resources Survey.

Architecture

Irving Gill pioneered the modern architecture movement by daring to simplify design and demonstrate heightened sensitivity of form, despite the predominance of imitation of historical decoration during this era. Gill is best known for his work in San Diego where he gained recognition for developing "one of the few wholly original styles of architecture in the United States." (McCoy) Gill's role in Torrance (1912-1913) was during a critical development point for his career as demonstrated in his use of a range of construction techniques on his buildings (wood, brick, slab, concrete and cement). During his stay in Torrance, Gill was developing a cohesive design philosophy and experimenting with technological innovation.

Gill has been recognized as a master due to his passion for simple form (the straight line, arch, and circle). He has stated "any deviation from simplicity results in a loss of dignity. Ornaments tend to cheapen rather than enrich." This philosophy is well represented in his first design built in Torrance, the monolithic concrete bridge with the graceful arches. Furthermore, the bridge has even aged in the Gill tradition by vines graciously breaking the severity of the architectural lines.

"We should build our house simple, plain, and substantial as a boulder, then leave the ornamentation of it to Nature, who will tone it with lichens, chisel it with storms, make it gracious and friendly with vines and flower shadows as she does the stone in the meadow."
(Gill, 1916)

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Continuation Sheet

Section number 8 Page 3

Upon conferring with Steve Mikesell, Architectural Historian for Caltrans, we have found that only a handful of bridges in the State of California have been designed by architects. Mikesell has participated in the recent Federal Highway Survey of every vehicular bridge in the State (2,000 observations).

Based on this research and his personal observation of non-vehicular bridges, Mikesell has made additional comments. First, there have been no other bridges designed by Gill in the Los Angeles area. Moreover, the Green and Green bridge in Pasadena is the only other bridge designed by an architect in the entire Los Angeles region.

Engineering

This bridge is significant from an engineering standpoint for its highly unusual use of non-load-bearing walls to create the appearance of an arched bridge. Structurally, the bridge is built around four concrete girders, which are carried on two-column bents and run the length of the bridge, and a concrete slab which comprises the deck. Stripped to its load-bearing elements, the structure would appear as a rather plain concrete girder and slab bridge on concrete columns. Similar to dozens of other bridges built in California in the period, 1900-1917, and not unlike the typical two-column concrete freeway overcrossing built in California in the 1960s and 1970s. The significance of the bridge from an engineering standpoint lies in the highly unusual use of decorative elements to disguise the structural system and to create a more pleasing aesthetic appearance.

As shown on the attached exhibits, the principal members in the bridge substructure are the heavily reinforced concrete columns (labeled 1 on Attachment A). These carry four concrete girders (2, Attachment A) and the concrete slab deck, which extend the length of the bridge. There are five sets of these paired columns, which serve as bents for the six spans.

The columns and girders are hidden behind three elements, which appear to be entirely decorative and not load-bearing. The first (4, Attachment A) is a wide wall, probably hollow, which brings the exterior walls to the vertical plane of the deck, creating a flush surface on the outside of the bridge. The second (3, Attachment A) is an arched form poured between the columns. This element is repeated in each bent, creating an arcade that runs the length of the bridge. The third element (5, Attachment A) is an arched form poured between the bents. This creates an arcade that runs the length of the bridge. Although the geometry of the arches differs with the different lengths of the spans, the pedestrian spans, at 15' each, and the railroad spans, at 18' each, feature round-headed arches, while the wider highway spans (29'4") feature elliptical arches.

The three decorative elements transform what would have been a very plain concrete bridge into a very elegant and interesting structure. It is a very

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Section number 8 Page 4

complex composition because of the different shapes of arches and the double arcade, but also very simple and elegant because of the unbroken vertical plane and the absence of applied decorative features.

This design is highly unusual. The California Historic Bridge Inventory, which investigated more than 100 historic concrete girder bridges in California, revealed no other example of this particular technology. It is likely that the design is unique to this structure, developed for the purpose of improving the appearance of the prominently-sited structure.

Pacific Electric Railroad

The Pacific Electric Railroad constructed the single-track lines to Torrance Townsite from 1912-1914. During this period, the Torrance lines were placed in operation.

The tracks crossing over the Irving Gill Bridge led to the U.S. Steel Company. The set of tracks passing underneath, provided passenger service to Torrance at the nearby Pacific Electric Depot. Pacific Electric continued to own and operate these lines until November 13, 1965 when they merged with Southern Pacific Transportation Company.

Since then, the U.S. Steel factory has been demolished, and the tracks across the bridge have been abandoned. The trackage on each side of the bridge has been removed for new development but the tracks on the bridge remain.

Community Commitment

The City Council has passed a resolution expressing the City's commitment to the preservation of the historically and architecturally significant bridge (Attachment #5).

On November 16, 1986, following four years of pursuit, the City of Torrance successfully obtained ownership of the bridge by indenture with Southern Pacific for the specific purpose of preserving the bridge. Southern Pacific has reserved the right to maintain and operate the track structure. A copy of the indenture agreement is attached to this registration form (Attachment #4).

The Torrance Historical Society consistently displays the bridge on brochure covers. This organization has expressed concern over the future of the bridge through written endorsement (Attachment #6).

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National Park Service

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Continuation Sheet**

Section number 9 Page 2

9. MAJOR BIBLIOGRAPHICAL REFERENCES (Continued)

Shanahan, D. F. and Elliott Jr., Charles. Historic Torrance:
A Pictorial History of Torrance, California, Legend Press 1984.

Torrance City Clerk, History of Torrance. 1988.

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National Park Service

**National Register of Historic Places
Continuation Sheet**

PHOTOGRAPHS

Section number _____ Page 1

There are ten photographs submitted with this registration form. All are of the Irving Gill Bridge in Torrance, California.

With the exception of photograph #1, all photos were taken by Julie Dombrowski on July 15, 1988.

PHOTOGRAPH: #1

1. Irving Gill Bridge
2. Torrance, California
3. Photographer Unknown
4. 1921
5. Torrance Historical Society
6. View from Torrance Boulevard-West side of bridge

PHOTOGRAPH: #2

- 1-2. Same as above.
3. Julie Dombrowski
4. July 15, 1988
5. City of Torrance Planning Department
6. View from South Wall of bridge

PHOTOGRAPH: #3

- 1-5. Same as above
6. View from Torrance Boulevard-West side of bridge

PHOTOGRAPH: #4

- 1-5. Same as above
6. Pedestrian arch-from East

PHOTOGRAPH: #5

- 1-5. Same as above
6. Center arches-from East

PHOTOGRAPH: #6

- 1-5. Same as above
6. Street arch with landscaping-from East

PHOTOGRAPH: #7

- 1-5. Same as above
6. Deck of bridge-from South

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**National Register of Historic Places
Continuation Sheet**

PHOTOGRAPHS

Section number _____ Page 2

PHOTOGRAPH: #8

- 1-5. Same as above
- 6. Parapet-Northeast corner of bridge

PHOTOGRAPH: #9

- 1-5. Same as above
- 6. Base of pier-Southeasterly

PHOTOGRAPH: #10

- 1-5. Same as above
- 6. Top of pier-close-up

Irving Gill Bridge
Torrance, CA

Staff Evaluation

The Irving Gill Railroad Bridge was designed in 1913 by Irving Gill as one of the first structures in the planned community of Torrance. The reinforced concrete structure consists of six arches spanning Torrance Blvd. A pair of tall half-circle arches in the center served Pacific Electric streetcars; on each side of them is a large segmental arch for automobile traffic; on each end is a smaller arch for pedestrians. Across the top were Southern Pacific Railroad tracks. In addition to the six arches spanning the boulevard, there are five perpendicular arches underneath the bridge.

Structurally, the bridge is built around four concrete girders, which are carried on two-column bents and run the length of the bridge, and a concrete slab which comprises the deck. The application of the different shapes of arches and the double arcade give great visual interest to an otherwise plain concrete girder and slab bridge on concrete columns, similar to dozens of other bridges built in California in the period 1900-1917.

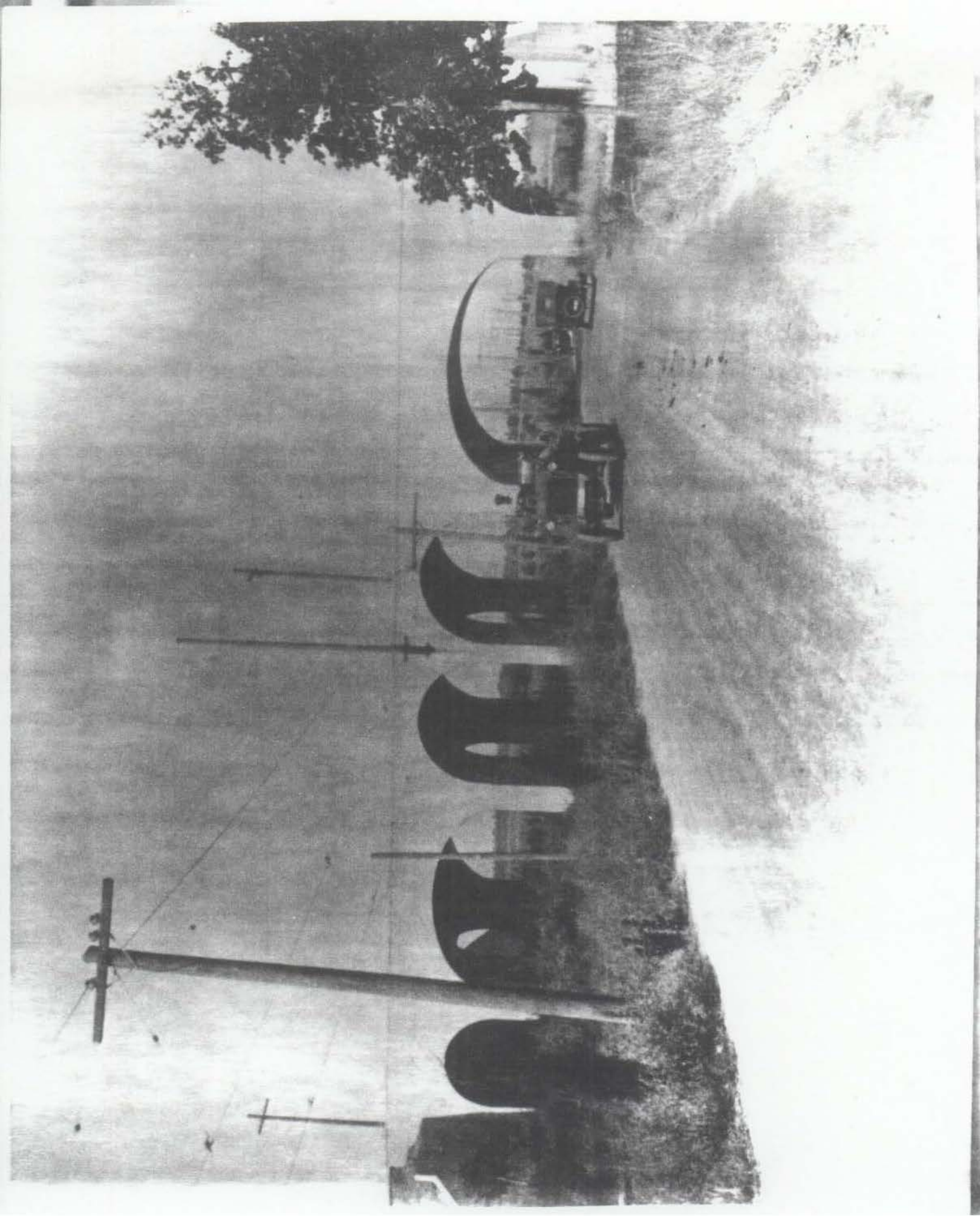
Alterations to the bridge have been minimal. It therefore retains a high level of integrity.

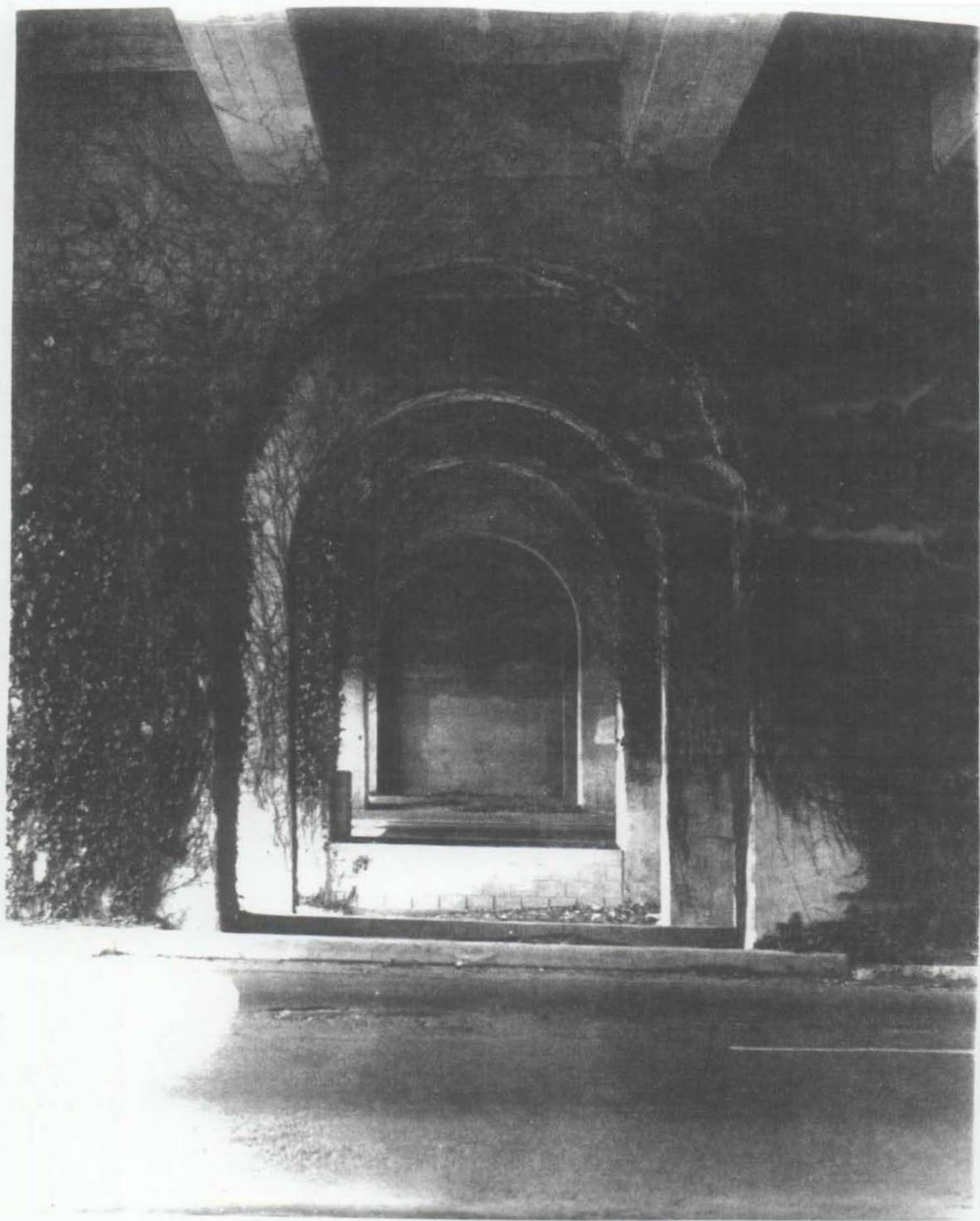
The Irving Gill Bridge is significant in the architectural, engineering, and community planning and development history of Torrance under Criterion C. The architecture is important because the bridge was designed by Irving Gill and is highly reflective of his direction toward greater simplicity which significantly contributed to the modern movement in architecture. The bridge is significant from an engineering standpoint for its highly unusual use of non-load-bearing walls to create the appearance of an arched bridge. It played a prominent and symbolic role in the planned community and is the only remaining unaltered structure from the original Torrance General Plan as conceived by Frederick Law, Jr. and John C. Olmsted. Staff recommends listing at the local level of significance.

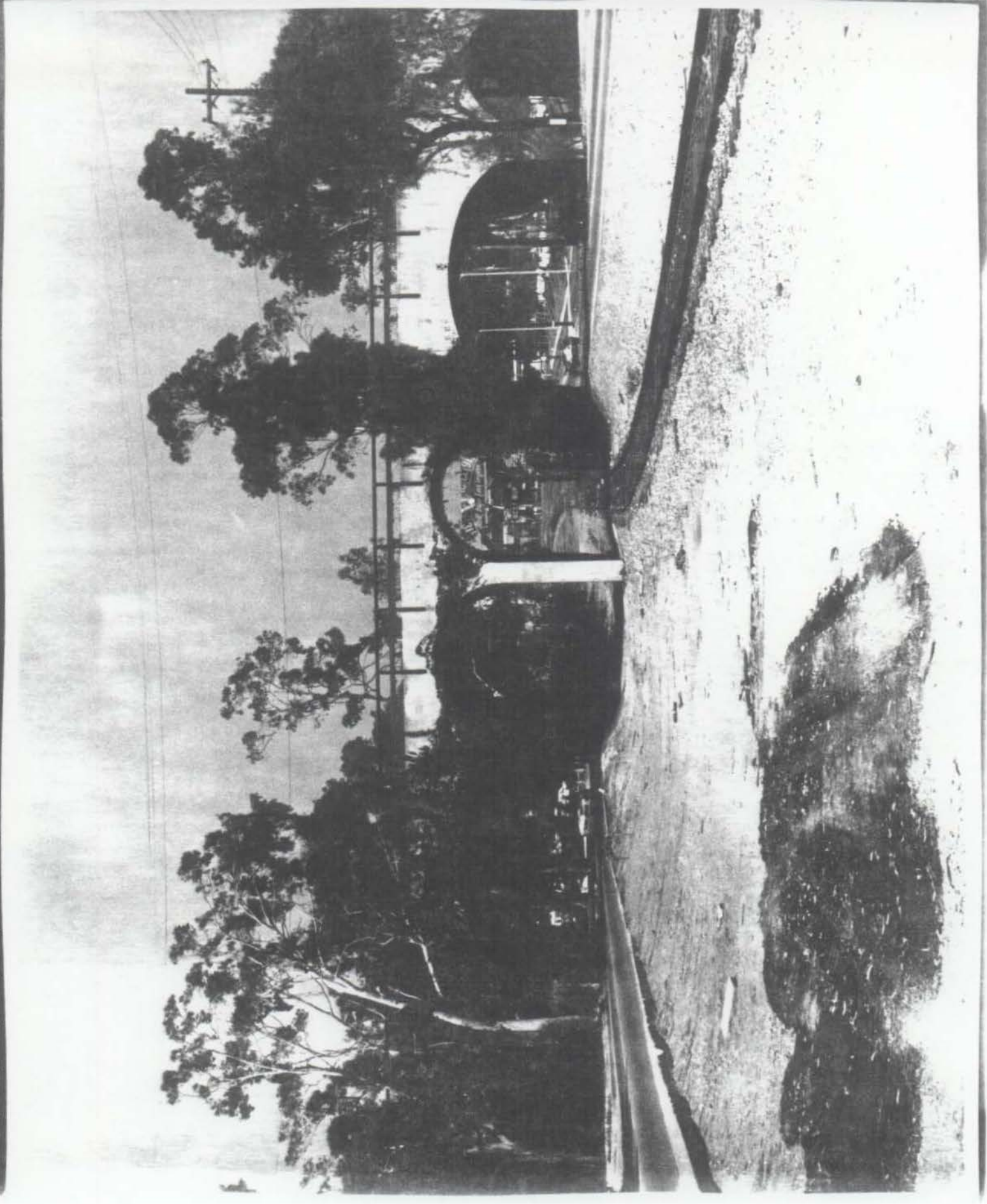
Cynthia Howse
May 3, 1989

ATTACHMENTS

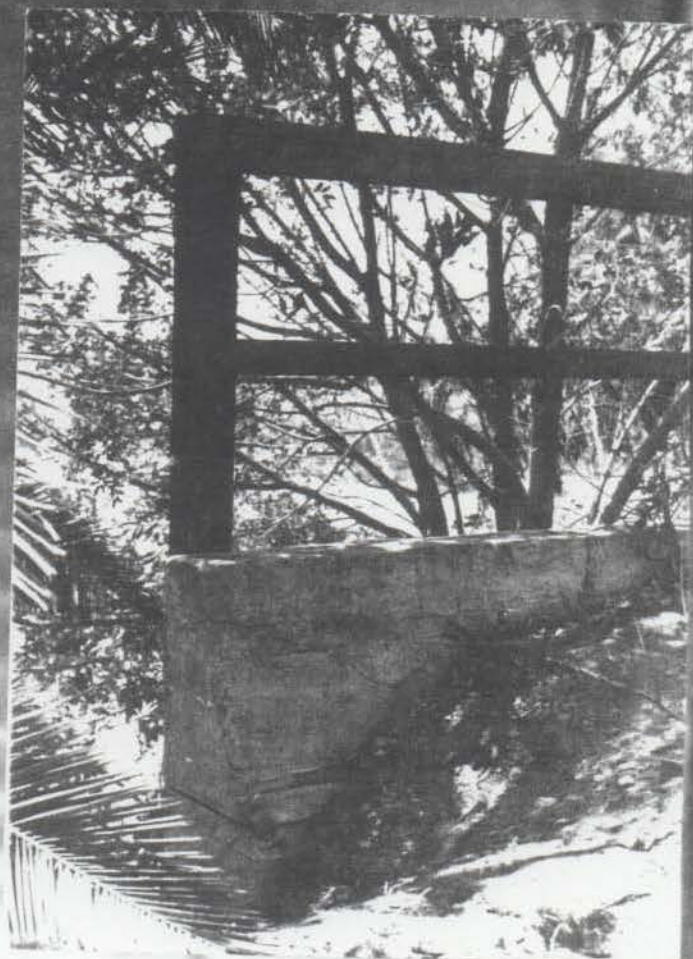
1. PHOTOGRAPHS
Two sets of 10 photographs, numbered 1-10. They include: One 8x10 reproduction of a 1921 photograph of the bridge. Two 8x10 photographs of the bridge, dated July 1988. And seven 3x5 close-up photographs showing the detail and damage to the bridge.
2. BRIDGE PLANS
Two sets of reduced copies of the original bridge plans titled "El Prado Bridge" dated June, 1913.
3. TOPO MAP
U.S. Geological Survey Topographical Map, 7.5 minute series, at a scale of 1:24,000.
4. INDENTURE AGREEMENT
Two copies of the Indenture Agreement between Southern Pacific Transportation Company and the City of Torrance granting the City ownership of the bridge and a plat map depicting the boundary of the property nominated.
5. CITY COUNCIL RESOLUTION
Two copies of resolution 85-78 which expresses the City's commitment to the preservation of the Bridge.
6. ENDORSEMENT
Letter from Torrance Historical Society
7. LOGO SAMPLES











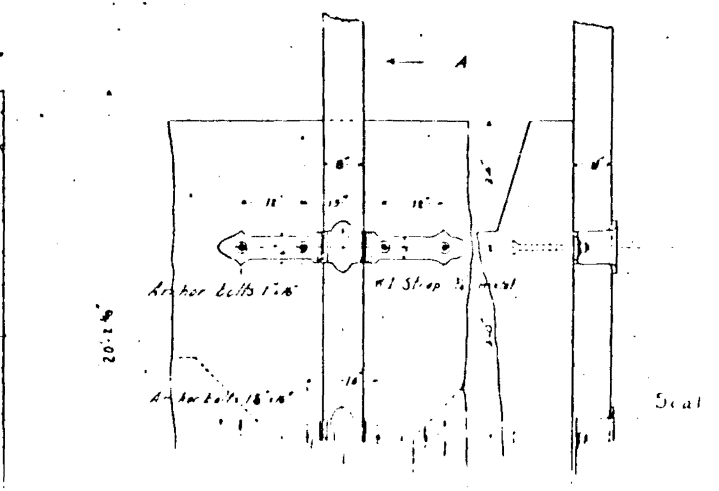
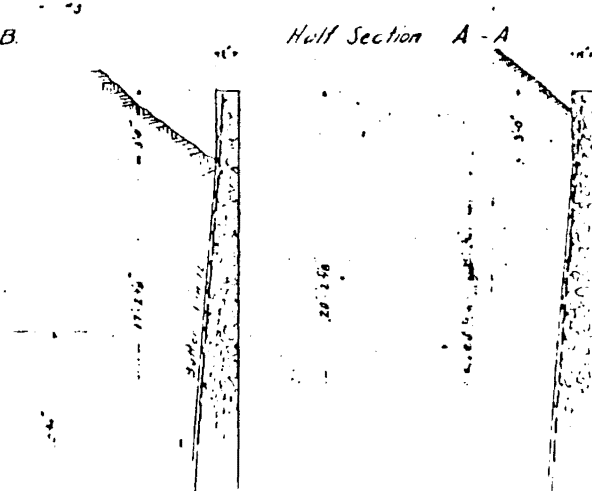
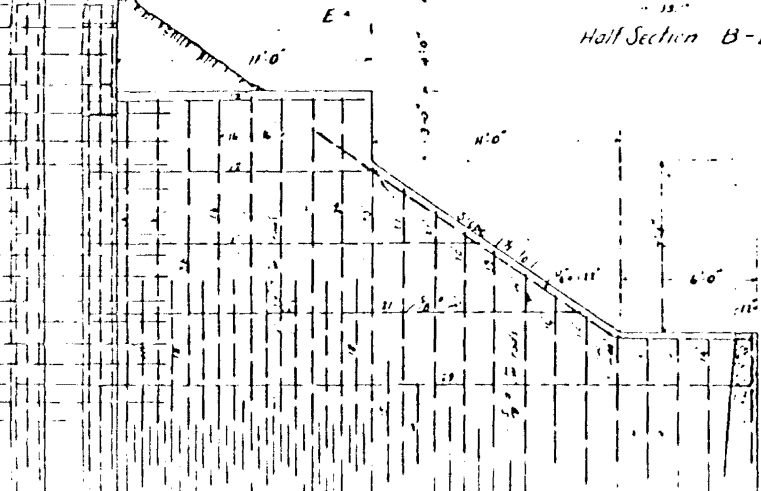
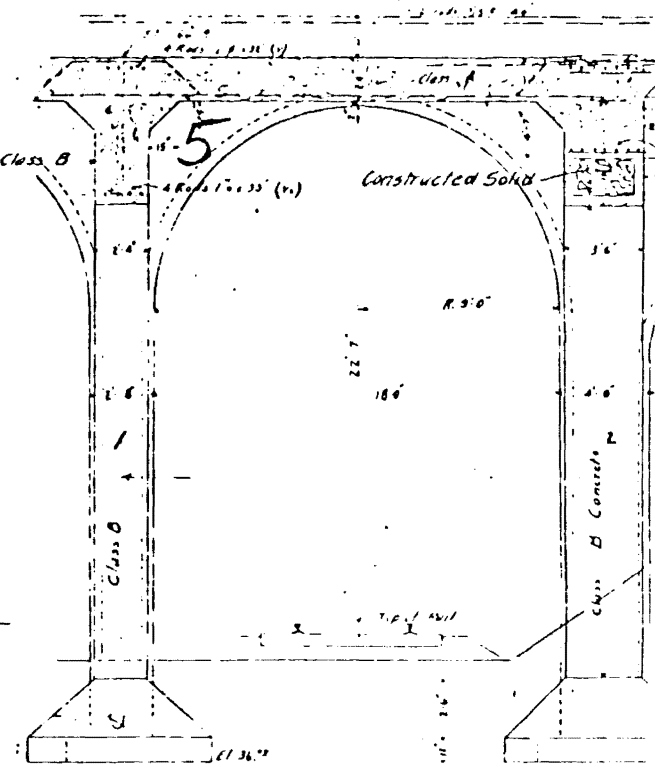
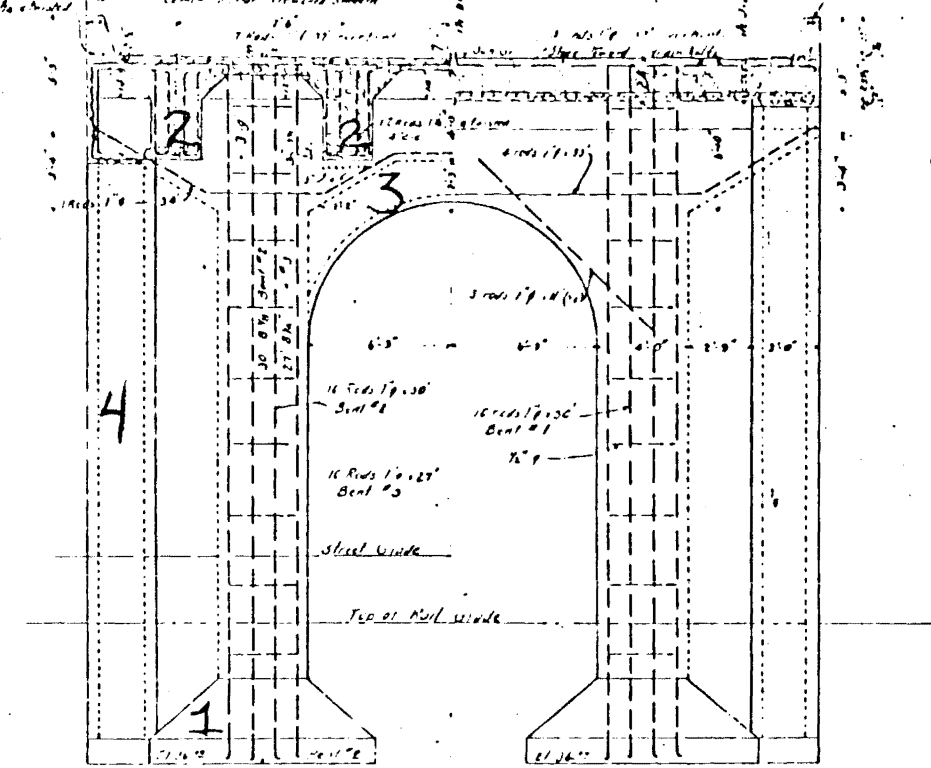
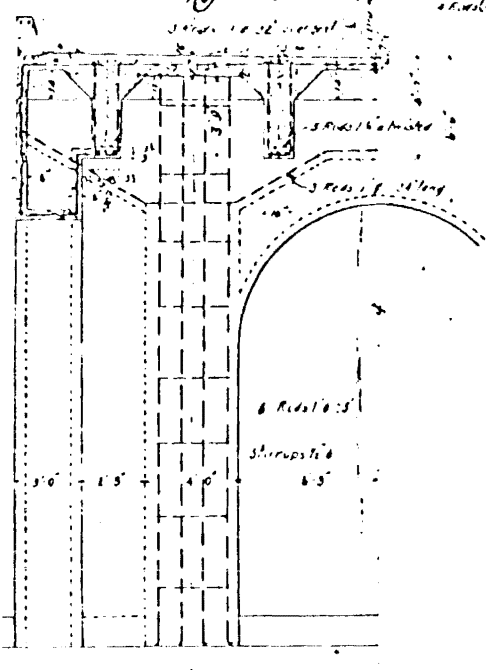


Pacific Electric Bridge

Terrance Blvd. + Boyle St. Interchange

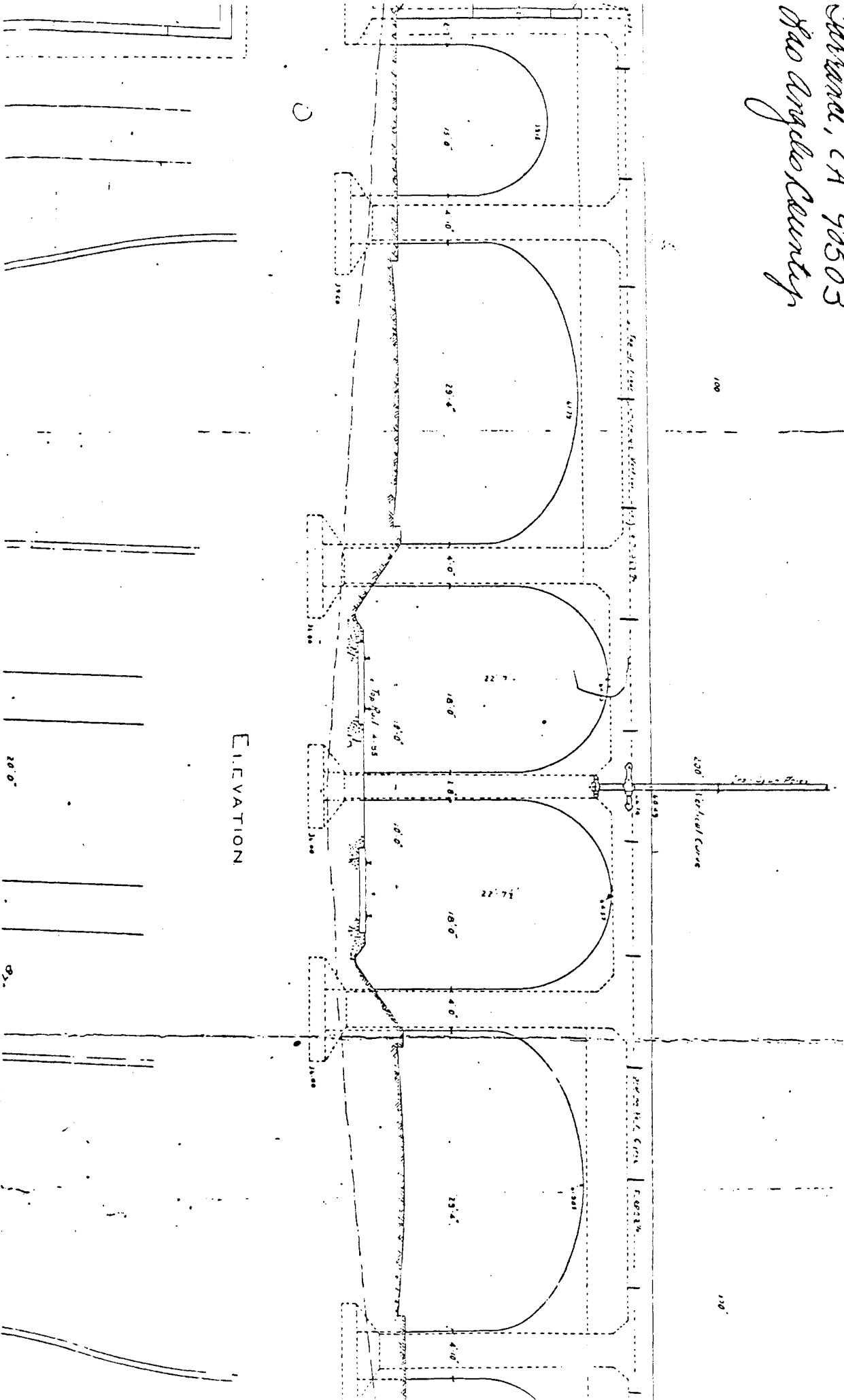
Terrance, CA 90503

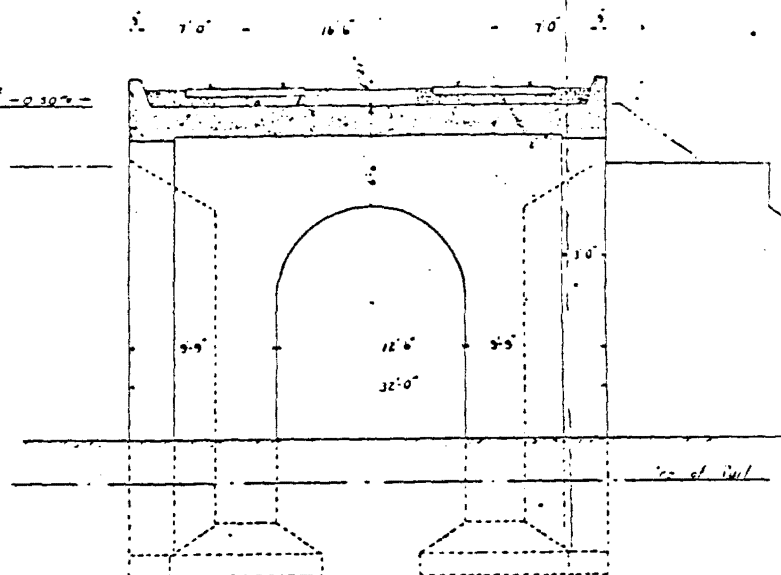
Los Angeles County



A

Pacific Electric Bridge
 Sonoma Blvd. & Bone St.
 Marin, CA 94023
 San Angelo County

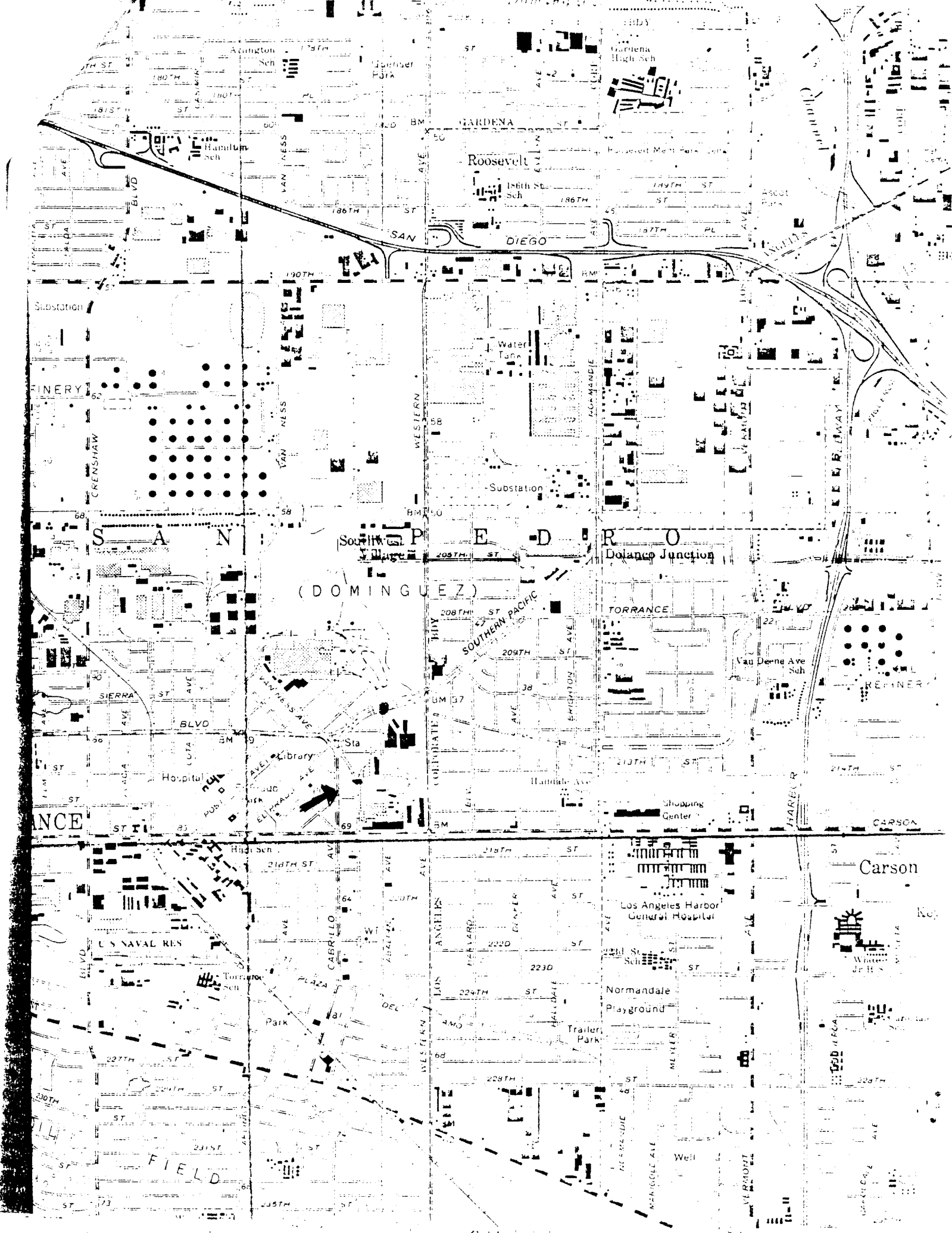




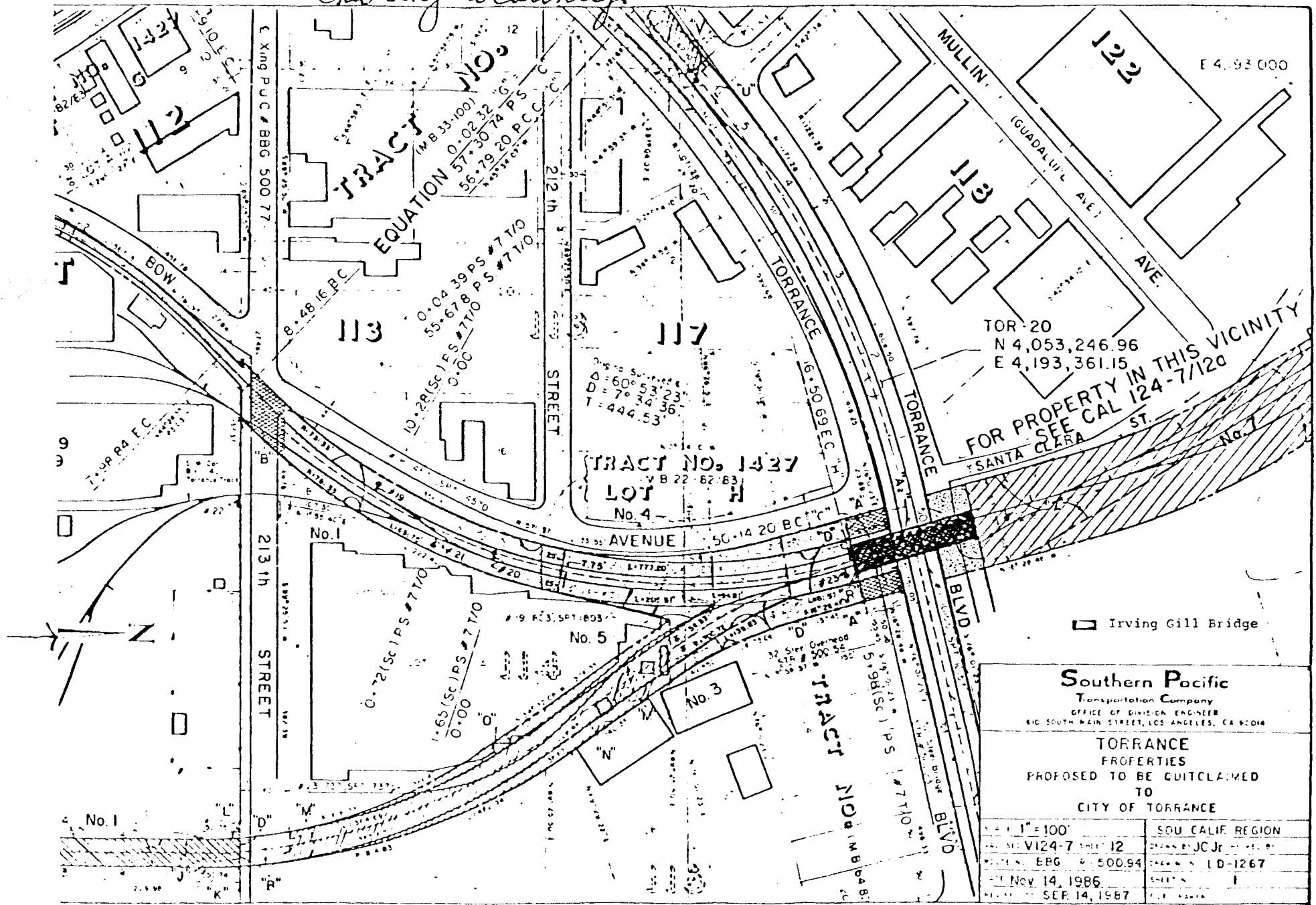
Pacific Electric Bridge
Terrance Blvd. & Bow St
Terrance, CA 90503
Los Angeles County

N Y

TORRANCE LINES
EL PRADO BRIDGE - GENERAL



Pacific Electric Bridge
 Torrance Blvd. + Bow Street
 Torrance, CA 90503
 Los Angeles County



Attachment #4

RESOLUTION NO. 85-78

A RESOLUTION OF THE CITY COUNCIL OF THE CITY
OF TORRANCE, CALIFORNIA, EXPRESSING THE CITY'S
COMMITMENT TO THE PRESERVATION OF THE
HISTORICALLY AND ARCHITECTURALLY SIGNIFICANT
SOUTHERN PACIFIC DEPOT AND SOUTHERN PACIFIC
RAILROAD BRIDGE

WHEREAS, the Southern Pacific Depot and the Southern Pacific Railroad Bridge have been identified as structures of historical and architectural significance; and

WHEREAS, both the depot and the bridge are prominent and notable landmarks of Torrance's history; and

WHEREAS, the Southern Pacific Depot, historically known as the Pacific Electric Depot, is one of the first buildings constructed in Torrance; and

WHEREAS, the Pacific Electric Depot was the focal point of the transportation system that was the foundation of Torrance's physical and economic growth; and

WHEREAS, the Southern Pacific Railroad Bridge is the landmark and gateway to all who enter the City from the east; and

WHEREAS, the Southern Pacific Depot and Railroad Bridge are representative of the best of California architecture and has played an integral role in the City's transportation system.

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Torrance expresses its commitment to the preservation of the Southern Pacific Depot and Railroad Bridge as structures of historical and architectural significance.

Introduced, approved and adopted this 2nd day of April, 1985.

/s/ James R. Armstrong
Mayor of the City of Torrance

ATTEST:

/s/ Donna M. Babb
City Clerk of the City of Torrance

APPROVED AS TO FORM:

STANLEY E. REMELMEYER, City Attorney

By _____

Attachment #5
Irving Gill Bridge
City Council Resolution

2

CERTIFICATE OF ACCEPTANCE

THIS IS TO CERTIFY that the interest in real property conveyed by
the deed or grant dated November 16, 1986 from
Southern Pacific Transportation Company

to the CITY OF TORRANCE, a municipal corporation, is hereby accepted
by order of Resolution No. 3417 adopted by the City Council of the
City of Torrance on February 18, 1958, and duly recorded on the 26th
day of February, 1958, in Book 56502, Page 139, Official Records, in
the office of the County Recorder of Los Angeles County, and that the
Grantee consents to the recordation thereof by its duly authorized
officer.

CITY OF TORRANCE

By *Donald E. Wilson*
Donald E. Wilson
City Clerk

DATED: November 25, 1986

(Section 27281 of Government Code; as amended)

PURPOSE: Transfer rights, title, interest in Railroad Bridge over
Torrance Blvd.

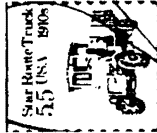
86-1685917

D-5136

CITY OF TORRANCE



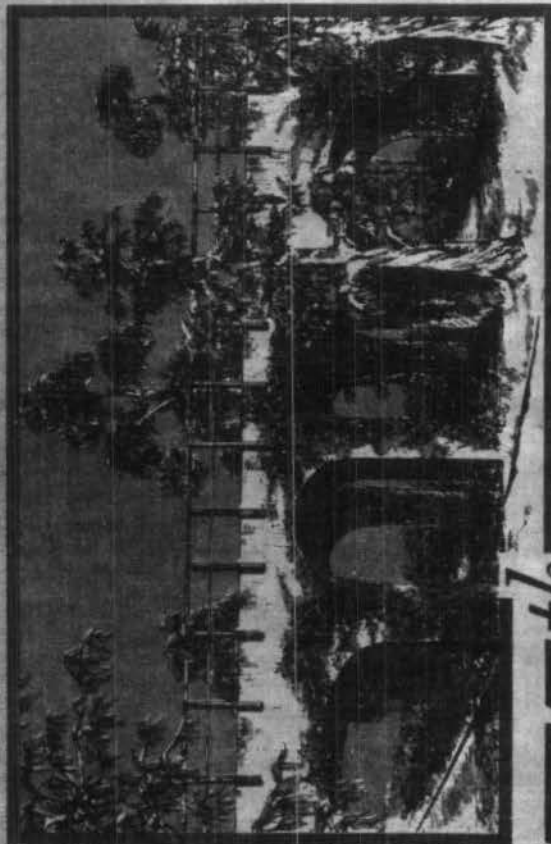
75th Anniversary
1912-1987



Number _____ of a Limited Edition

A Plan for Program Management and Allocation of Resources

1987-89 BUDGET



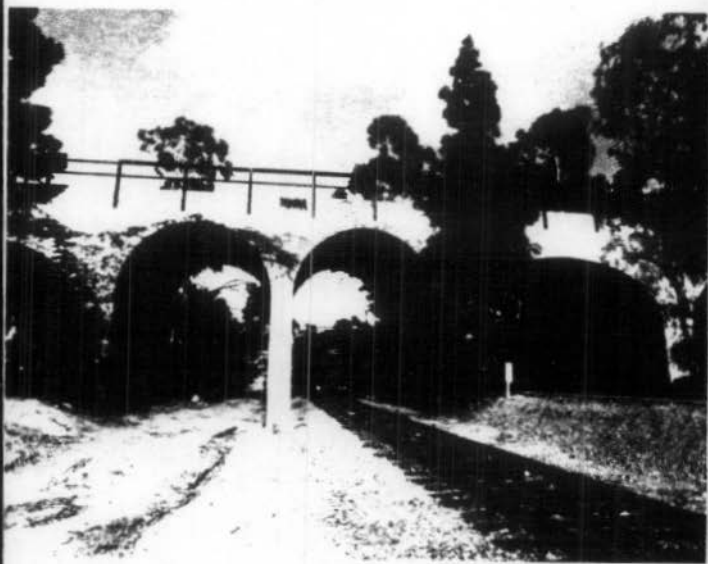
75th Anniversary
1912-1987



CITY OF TORRANCE • CALIFORNIA

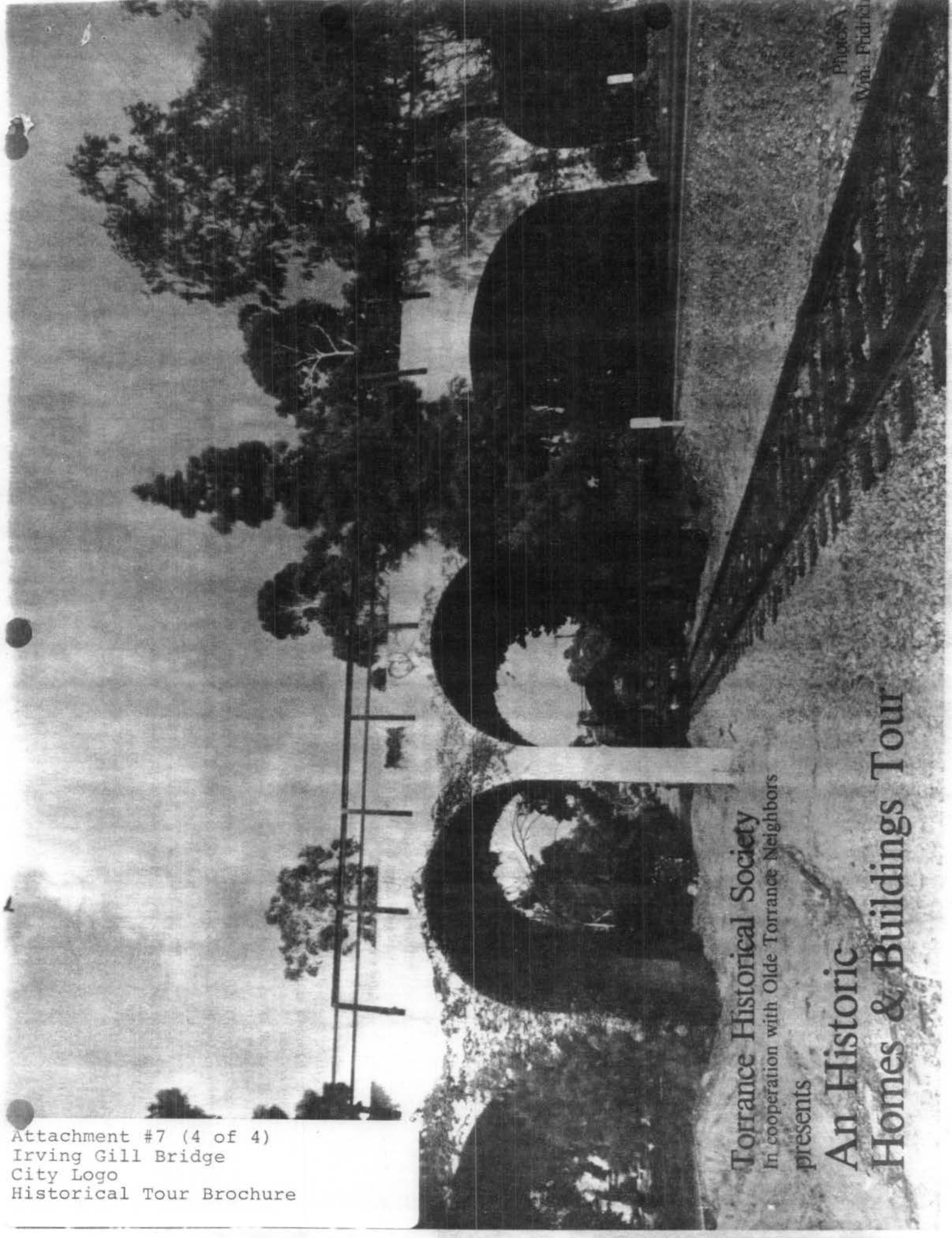
TORRANCE HISTORICAL SOCIETY

*A true adventure
in local history.*



MUSEUM HOURS
1:00 p.m. - 4:00 p.m. Daily
except Saturdays

1345 POST AVENUE
TORRANCE, CA 90501
(213) 328-5392

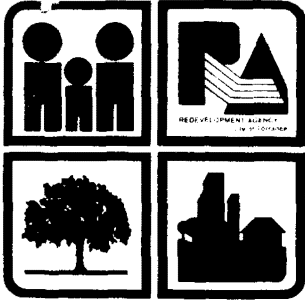


Attachment #7 (4 of 4)
Irving Gill Bridge
City Logo
Historical Tour Brochure

Torrance Historical Society
In cooperation with Olde Torrance Neighbors
presents

An Historic Homes & Buildings Tour

Photo by
Wm. Fridrich



REDEVELOPMENT
AGENCY of the
CITY OF TORRANCE

KATY GEISSERT, CHAIRWOMAN

MEMBERS

BILL APPLIGATE GEORGE NAKANO
DEE HARDISON DAN WALKER
TIM MOCK MARK WIRTH

LEROY J. JACKSON
EXECUTIVE DIRECTOR

DAVID S. FERREN
DEPUTY EXECUTIVE DIRECTOR

RECEIVED

APR 12 1989

OHP

April 10, 1989

Cynthia Howse
State Historian
Office of Historic Preservation
California Department of Parks & Recreation
P.O. Box 2390
Sacramento, CA 95811

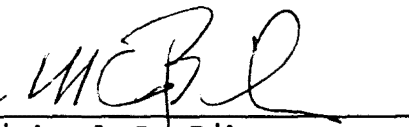
Dear Ms. Howse:

At your request we have enclosed a copy of the L.A. Architect
article regarding Irving Gill.

Sincerely,

David S. Ferren
Deputy Executive Director

by

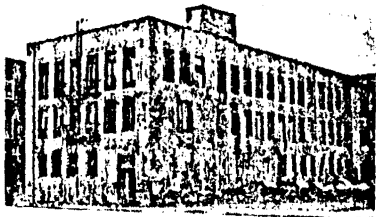

Michael G. Bihn
Principal Planner

Attachment

MB:KP

IRVING GILL, TORRANCE, AND THE INDUSTRIAL DREAM

Irving Gill is one of Southern California's most interesting, yet enigmatic, architects. Recent discoveries of previously unidentified Gill buildings in the city of Torrance provide information pertaining to several significant aspects of his career. The relationship of Gill's design philosophy to early 20th-century industrial city planning is clearly stated



El Roi Tan Hotel, 1912 (photos: Roger Hatheway).

in his Torrance work, and the structural features of several "new" Gill buildings add another dimension to his already well-known architectural technology.

Background

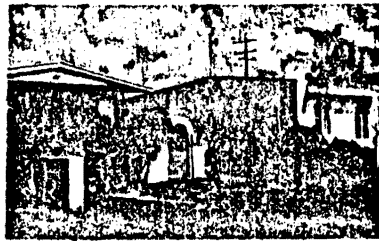
In 1911, real estate promoter Jared Sydney Torrance purchased a large tract of land from the pioneer Dominguez family. Torrance then joined into partnership with several other Los Angeles businessmen, forming the Dominguez Land Company. In order to better promote their real estate venture, the investors decided to establish a progressive industrial city. They named the new city Torrance.

Initially, the investors engaged the services of John C. and Frederick Law Olmsted, landscape architects from Brookline, Massachusetts. The formal contract between the Olmsted Brothers and the Dominguez Land Company was signed on January 5, 1912. Later in the same month, five Southern California architects, R.D. Farquhar, Elmer Grey, Samuel P. Hunt and Parker Wright, all of Los Angeles, and Irving Gill of San

Diego, were invited to submit designs for several types of buildings for the new city.

Gill was officially appointed chief architect of Torrance in June of 1912. He assumed responsibility for the design of each building erected by the Dominguez Land Company, and for the implementation of the Olmsted Plan.

The Torrance plan itself was based upon the shape of a "Y". Industry was located on the outside of the area bounded by the figure, while commercial and residential structures were located within the open area formed at the top of the "Y". The plan was oriented so that prevailing westerly winds blew industrial smoke away from the commercial and residential areas. Trees were planted to shield the factories from view. And a transportation network was incorporated as part of the overall plan. "Real city planning,"



Workers' cottages, showing zero-lot line placement, 1913.

declared Frederick Law Olmsted, "is concerned with every line of development which affects the physical conditions under which the people of today and the people of years to come must live and work."

Gill enthusiastically adopted the physical and philosophical elements of the Olmsted Plan, for the ideals were remarkably similar to his own. Since 1908, Gill had increasingly devoted himself to a utilitarian, low-cost and socially conscious form of architecture, exemplified by his 1910 Lewis Courts in

Sierra Madre.

As a result of the combined Olmsted-Gill effort, Torrance was at its inception one of the more thoughtfully planned industrial cities in the country. Despite the fact that the city's growth would



Pacific Electric Depot, 1912.

soon exceed the limitations of the plan, Torrance is a tribute to the progressive, industrial idea of the early 20th century.

Gill's Torrance Work

The range of Gill's Torrance work is impressive. Commercial, public, residential and industrial structures were designed as part of the investment and planning scheme. Today they provide a detailed record of a brief but highly productive period of Gill's career. Although Gill only served as chief architect in Torrance for approximately one year, 1912-1913, nearly every structure that he designed during this period was built. In recent years several of his buildings have been demolished, but at least 18 remain.

Although each structure employs cement or concrete, the materials with which Gill is most closely identified, the majority of the Torrance buildings do not use them as primary construction materials. The Pacific Electric station is built of hollow tile with a cement-plastered exterior. Gill's worker cottages are wood-frame structures with a cement-plastered exterior lath-up over lath and wire mesh. The Brighton and Colonial Hotels are composed of concrete, brick and steel. Constructed of brick with slab floors, the El Roi Tan and Murray Hotels have only a cement-plastered exterior. With the exception of the Southern Pacific Bridge, the main building of the Hendrie

Rubber Company is the only known Gill building in Torrance having monolithic concrete construction.

The variety of construction techniques suggests that Gill was still formulating a consistent technical methodology. As well as furnishing easily manipulated, relatively inexpensive durable and waterproof surfaces, concrete and cement provided Gill with a plastic design material. By smoothing over serrated lines of brick work, hollow tile, and irregular wood-frame joinery, Gill achieved a simple yet powerful form. Gill's use of concrete and cement constituted both the ideal working solution to technical problems, and a means by which to best approximate his own aesthetic goals.

The design of individual structures reflects the underlying theme of the Olmsted Plan — responsive to the needs of people in their living and working environments. Gill's residential cottages are, for example, built to the extreme side of each piece of property. The advantages offered by zero-lot line placement are obvious, since more continuous yard space is created for gardens or recreational activity. Gill's industrial and commercial buildings are carefully designed so as to allow for a maximum of interior lighting and ventilation. Remarkably, the Hendrie Rubber



Rubbercraft (Hendrie Rubber Co.), 1913.

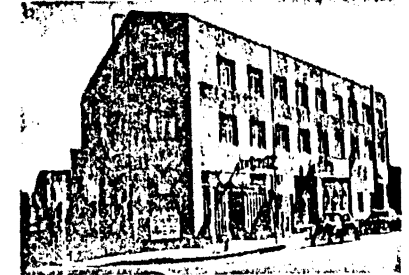
Company (now called Rubbercraft) is still a fully operational industrial facility, 65 years after its construction.

A Formative Period

Torrance was undoubtedly an important and formative period for Gill. His work there provides a unique opportunity to assess his architecture because it occurs at a critical developmental point

in his career, prior to the mature style that would later characterize the Banning and Dodge Houses of Los Angeles.

Through an examination of his Torrance buildings, one can observe Gill as he formulates a cohesive design



Colonial Hotel, 1912.

philosophy and experiments with technological innovation. It is a comprehensive statement, and an example of Gill's creative versatility.

Roger Hatheway

Architectural historian Roger Hatheway recently completed work on an architectural survey for the City of Torrance.

GILL'S TORRANCE BUILDINGS

1. Bridge, near corner of Torrance Blvd. and Western Ave.
2. Pacific Electric Depot, 1200 Cabrillo.
3. El Roi Tan Hotel, 1211 El Prado.
4. Murray Hotel, 1210 El Prado.
5. Colonial Hotel, 1601-1605 Cabrillo.
6. Brighton Hotel, 1639 Cabrillo.
7. Salm Manufacturing, 1805 Abalone.
8. Rubbercraft, 1800 W. 220th St.
9. Bungalow, 1815 Gramercy.
10. Bungalow, 1819 Gramercy.
11. Bungalow, 1903 Gramercy.
12. Bungalow, 1904 Gramercy.
13. Bungalow, 1907 Gramercy.
14. Bungalow, 1916 Gramercy.
15. Bungalow, 1919 Gramercy.
16. Bungalow, 1920 Gramercy.
17. Casa Del Amo Apartments, 1860 Torrance Blvd.
18. Shop/Apartments, 1610-1612 Cabrillo.

INTERIOR TOUR HOMES



1329 Engracia Avenue
Bungalow Style, 1922
Original Owner: Ludlow

1

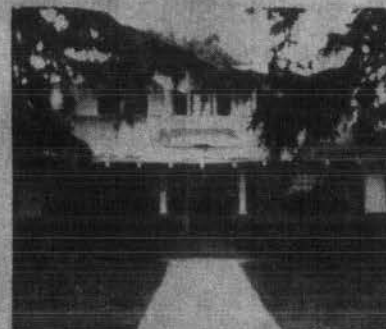
This two-story gable-roofed, clapboarded home was, in 1922, one of the "typical 2-bedroom, one story bungalows" being built in the newly incorporated City of Torrance. A few years later, however, a Mr. Monte Darling (prominent builder during the 1920's construction boom in Torrance), added a second story to this home, and also enclosed an exterior gable-roofed porch; today used as a sun room. A unique feature of this particular home, is the outdoor patio off the master bedroom, which was a part of the second story addition.



1329 Arlington Avenue
Spanish Style, 1940
Original Owner/Builder: Frank & Mable Steinhilber

2

This house, constructed near the end of the great depression, is a two-story Spanish Colonial Revival style home. Modified in 1974, it still maintains its original spacious layout including the one-story front wing. French style doors and windows are a feature of the living and dining rooms. There have been three owners of this house. The widow of the original owner is still a Torrance residence.



1313 Cota Avenue
Bungalow Style, 1914

3

This is a story-and-a-half wood clapboarded bungalow with the slope of the pitched roof placed at right angles to the street. The roof plane dominates the design, sheltering the porch under its eaves and interrupted only by a pair of dormers in the second story. This is a good example of early Torrance housing built for middle to upper income families employed by industrial manufacturing companies.



1551 Post Avenue
Bungalow Style, 1919
Original Owner: Judge George Post

4

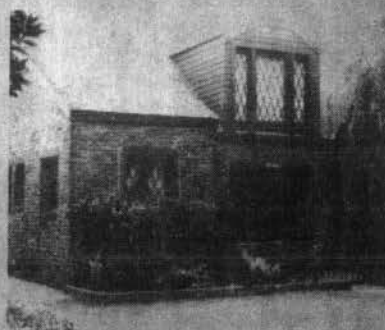
This is a one-story rectangular plan bungalow with steep gable and narrow shed dormer in front. The house is clapboarded. Judge Post, for whom the street was named, was a renowned banker in the East and his valuable experience helped to strengthen young Torrance's economy. The house was an early substantial residence in the city and derives its importance primarily through its association with Judge Post.



1518 Post Avenue
Spanish Colonial Revival, 1924
Original Owner: George Peckham

5

This one story stucco bungalow was designed by Rex Weston and built by Monty Darling in 1924. French doors and an elliptical ceiling in the formal dining room combine with a coved ceiling and semi-circular window in the adjacent living room. The living room also features batch elder ceramic tile on the fireplace and hearth. White oak hardwood floors and hammered silver door knobs are two of the unique features of this home. Gabled and flat roofs and a courtyard in the front entrance were included in the price of \$6,000 by George Peckham, the original owner, and proprietor of the first Ford dealership in Torrance.



1620 Post Avenue
Tudor Revival Cottage, 1925
Original Owner: Grover C. and Pluma Whyte

6

This clinker-brick Tudor Revival Cottage has a foreshortened U-shaped plan creating a small entrance courtyard. The windows are small-paned, some of them diamond patterned. This house gives the feeling of a rustic cottage and is architecturally unique in Torrance. Mr. Whyte played an important part in Torrance's early development due to his career as editor and publisher of *The Torrance Herald*. Torrance's largest early newspaper.



1507 El Prado
Spanish Colonial Revival Cottage, 1928
Original Owner: Felix and Amanda Winkler

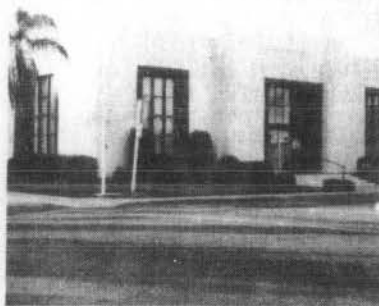
7

This house has an L-shape plan with a front patio and an entrance tower which is placed at the intersection of the wings. Its castle-like facade, blends well with the tree-lined El Prado Park situated down the center of the street.

Thanks to the gracious people who are allowing us to view the interiors of their property.

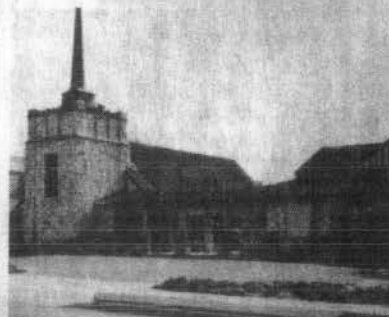
Delores and Bill Sutherland
Dick and Alice Kuhns
Gene and Bob Hassan
Bill and Eileen Kalish
Linda Bellaart
Derf and Monica Fredericks
John and Chris Alter
Dr. Harold Klonecky
Mr. Edward C. Frederick
Rev. George A. Detor
Fr. Patrick J. McHugh
Pastor Lloyd H. Newlin
Rev. David J. Harada

INTERIOR TOUR BUILDINGS



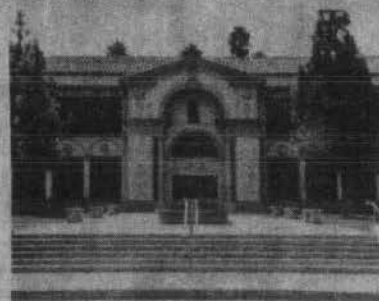
Post Library, 1936
1345 Post Avenue

The Post Library was built as part of the WPA Program and is a good example of "WPA Moderne" style of architecture. The Post Library was originally known as the Torrance City Library. It is currently the Torrance Historical Society. T.H.S. plaque



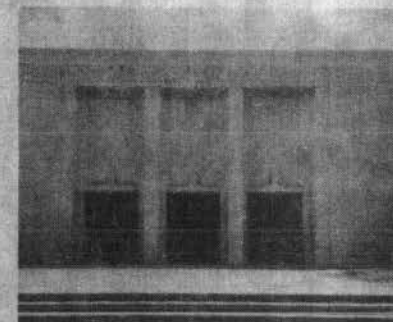
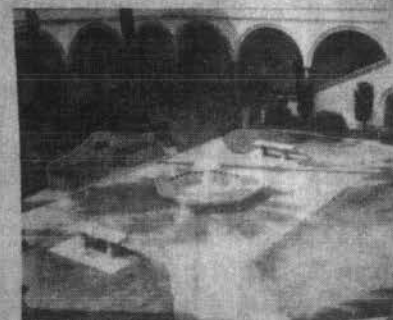
Halverson, Stone and Myer
Mortuary Chapel, 1955
1223 Cravens Avenue

There are few buildings like this anywhere in Southern California. Architect, Carlton M. Winslow, used some highly stylized and simplified English Gothic forms, suggesting the ranch house mode. The copper spire, placed directly on the stubby tower, is a Winslow trademark. The flagstone-based chapel forms a courtyard, with a rustic waterfall and the entrance portico gable end being half-timbered. The chapel windows are narrow slits like stained glass windows.



Torrance High School Campus, 1917
2000 Carson Street

The school consists of a complex of buildings and structures spanning the years 1917 through the 1960's. The main building, designed by architect Robert Allen Farrell, is Mediterranean in style influenced by Classical and Spanish Colonial design elements. In 1921 came the addition of a brick patio with tile fountain later referred to as the "Senior Patio." The Assembly Hall, designed in the WPA Moderne Style, was built in 1935 to replace the original auditorium destroyed by the 1933 earthquake. Torrance High School is the first property in Torrance to be placed on the National Register of Historic Places - U.S. Dept. of the Interior. T.H.S. plaque



CHURCHES 1 - 4 P.M. ONLY



St. Andrew's Episcopal Church, 1927
1432 Engracia Avenue

St. Andrews is the oldest church building in continual use in Torrance. It is a simple structure with four stripped pilaster-like ribs bordering the symmetrical center section, with elongated arch windows. A two-tiered mission bell tower rises on the north, topped by a bulbous dome. Of great interest are the memorial windows for some of the city's original leading families, including the Torrance's, Post's, Shidler's and the Henderson's.



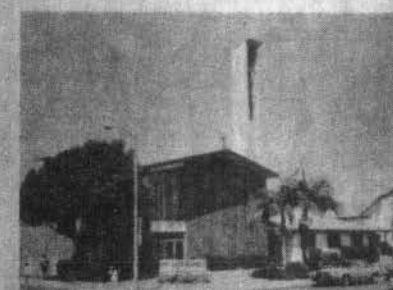
Nativity Catholic Church, 1939
1447 Engracia Avenue

A simple Latin-cross plan Spanish Colonial Revival in style suggests economy with most ornamentation around the front door. The most important interior feature is the simple wooden trusses which traverse the ceiling and are supported on pilasters. The base of the church is a ribbed Deco molding. The stained glass windows bear many important family names.



First Baptist Church, 1948
2118 Carson Street
Architect: JNO. Fleming

This Spanish Colonial Revival building has several eclectic references to other types of architecture. The bell tower, topped by a pyramidally roofed cupola, has different types of windows, including Mission Carmel star windows. Unusual decorations in the eaves, lancet-like windows and round window over the entrance, give this main section a slightly medieval tone. Beautiful stained glass windows are on all sides. A new 39-rank pipe organ is now in use, and has been since 1982.

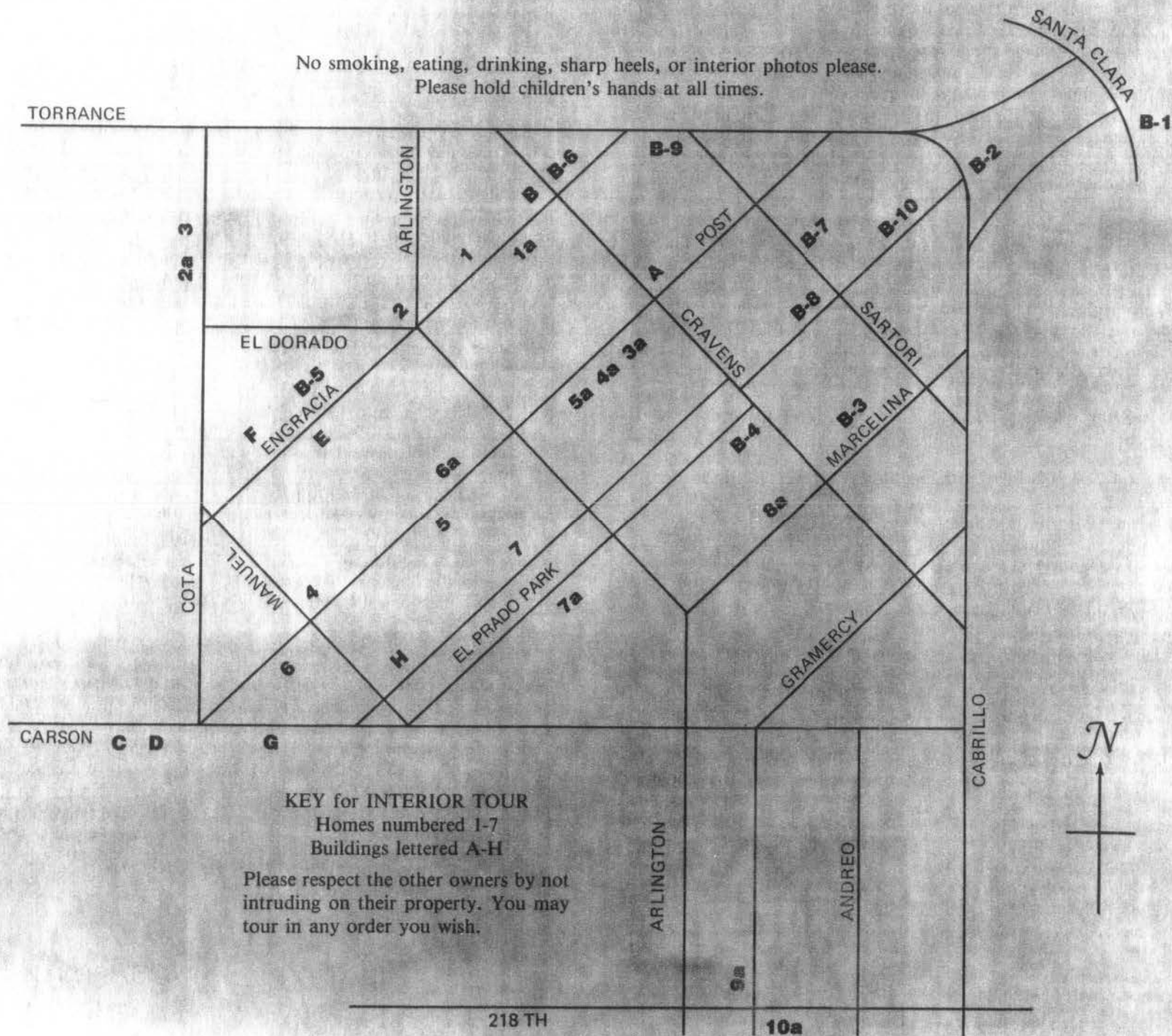


First United Methodist Church, 1923
1551 El Prado

The first church was dedicated in 1923 and, with additions and remodels, it is now the recreation hall. The new sanctuary was completed in 1961 and is very contemporary in style. Over the main entrance is a beautiful grillwork window depicting Christ in a gesture of invitation, "Come unto me..." Just below are four cast plaques representing the Evangelists, Matthew, Mark, Luke and John. Inside the memorial windows are colored faceted glass placed in an unusual mosaic style setting, each telling a particular story. This church boasts a bell tower and a 12-rank Moller pipe organ.

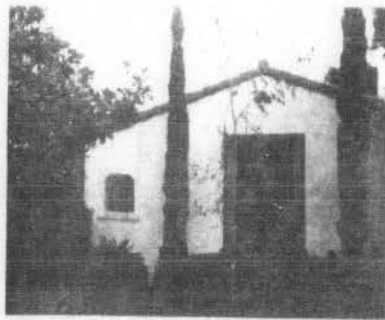
GUIDE MAP

No smoking, eating, drinking, sharp heels, or interior photos please.
Please hold children's hands at all times.



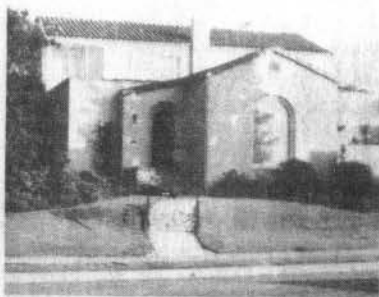
EXTERIOR TOUR HOMES

These homes have been included in past tours. Please do NOT intrude on their property.



1318 Engracia Avenue 1a
Spanish Colonial Revival, 1936
Original Owners: Harry & Margaret Massie

Moving past a leaded and stained glass front window, through a plant shaded front courtyard gains entry into this red tiled, stucco finished home exhibiting many craftsmanship details. Built by Harry Massie in 1936, it was purchased from his son, Harold in 1978 by its present owners. Mahogany woodwork, lath and plaster walls with curved ceiling junctures and art deco hardware are among noteworthy features. All the latest '36 innovations - attic antenna with wall plugs for the "wireless," built-in ironing board, clothes rack and shoeshine kit, and lavish tile in the kitchen and bathrooms are all in original condition.



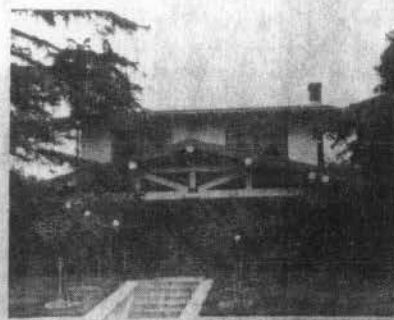
1448 Post Avenue 5a
Spanish Colonial Revival, 1927
Original Owner: Charles Chapman & M. Shidler

A two-story gable-roofed, plastered house of irregular plan with a side courtyard and sunroom off the side. There is a semi-circular topped wooden door, tiled gabled roofs and an arched picture window. Dr. Shidler, a family doctor, lived here during the early days of Torrance. In the 1940's, Rosemary DeCamp lived here with her husband Judge John Shidler.



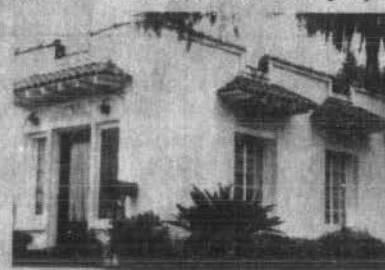
1323 Cota Avenue 2a
Bungalow Style, 1916
Original Owner: Arthur & Annie Mullin

This two-story clapboard, on 1½ lots, has gabled dormers and an L-shaped veranda overlooking extensive gardens and a handsome gazebo. Fine cabinetry distinguishes the interior-coffered dining room ceiling, built-in buffet and secretary, wainscotting and paneling. All stained glass is the work of the present owner. Jared S. Torrance built this home for visiting Columbia Steel officials. The house was sold in 1918 to local Oldsmobile dealer A. Mullin and remained the family home for over 50 years.



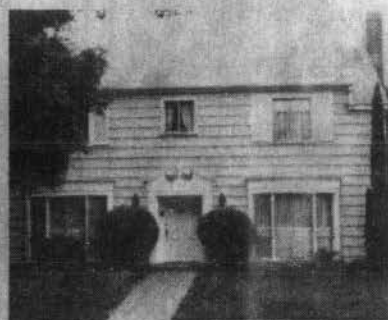
1511 Post Avenue 6a
Bungalow Style, 1918
Original Owners: George W. & Jane R. Neill

This large two-story home of elegance is rectilinear with clapboarded first story and shingled second story. There are broad overhanging eaves and exposed rafters. The veranda porch is covered by gabled roof. It is one of the two oldest houses on Post Avenue. George Neill was involved in Torrance's early government and was President of the Torrance Development Company.



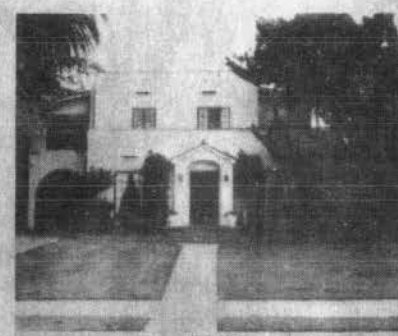
1434 Post Avenue 3a
French Neoclassic/Mission Revival, 1920
Original Owner: Frank Sammons

The regimentation and regularity of the facade, and the large casement windows which almost appear as French windows, are the French elements. The window hoods, with their exposed rafters, and the tiled parapet wall are the Hispanic elements. The house contains many original features including the built-in hutch, natural draft flue over the stove, built-in ironing board, basement and walls of all brick construction (under the plaster). The rear was originally a Model T garage and is now a residence. This house was constructed at the beginning of an architectural trend to capture the romance of Europe in California architecture.



1512 El Prado 7a
Cape Cod Style, 1948
Original Owners: Mr. & Mrs. John B. Melville

This two-story frame residence has a pitched roof, a large exterior chimney, round bay windows, shingle wood siding, shutters and a decorative swan's neck doorway flanked by carriage lamps. The living room and dining room have parquet floors and the den is paneled in mahogany. This house was designed by George Morlan of Redondo Beach. Mr. and Mrs. Melville were the owners of the



1444 Post Avenue 4a
Mission Revival Style, 1922
Original Owner: Rudolph A. Huber

This house sits on two lots. It is a two-story flat roof plastered house with the second story smaller than the first, making the first story roof appear like a balcony. There are French doors, double casement windows and a carved wooden front door. Mr. Huber, the first owner of this elegant house, was first a banker in Torrance and later a mortgage loan broker.



1525 Marcelina Avenue 8a
Bungalow Style, 1915
Original Owner: Dr. Norman Leake

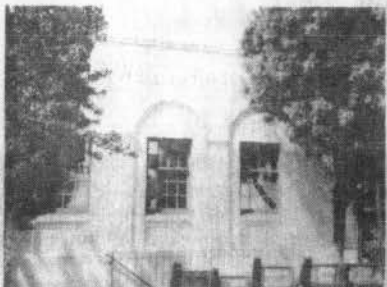
This large two-story house is made mainly of redwood. There are two large gables in the back and one in the front. The house is heated by a large fireplace and a solar system. Dr. Leake had his office and treatment rooms on the east side of the house and the family rooms on the west side and upstairs.

EXTERIOR TOUR BUILDINGS



1753 Gramercy Avenue **9a**
Early Ranch Style Adobe, 1946-1948
Original Owners: George C. & Eunice F. Crabtree

This is an "L" shaped building with three gables and a roof of cement tile. The house has walls 18" thick with the usual advantages of adobe—adding to summer coolness and less heat loss in winter. The heating is unique in that it circulates hot air between a slab and a wood floor. Mr. Crabtree built the house himself—including mixing all the adobe.



U.S. Post Office, 1935 **B-3**
1433 Marcelina Avenue

Built with Federal W.P.A. and P.W.A. funds, this building is a stripped Hispanic building of Spanish Colonial Revival style. Its roof is hidden by a parapet wall. The south corner suggests a pavilion with raised roof line and pyramidal roof. Flat windows are contained within bas relief arches. Minor decorative details have been removed.

T.H.S. plaque



1804 Gramercy Avenue **10a**
Bungalow Style, 1912
Original Owner: Isabel Henderson

This is a simple one-story bungalow with gable-end to the street. The building is clapboarded and the porch is supported by wood posts on piers. Mrs. Henderson was a cousin of the city's founder, Jared Sidney Torrance, and was greatly interested in the development of Torrance. She opened the city's first library in her home in November 1912. Originally 300 books were donated by Mr. Torrance and his friends.



City Hall, 1936 **B-4**
1511 Cravens Avenue

Designed by Walker and Eisen, this building was built with W.P.A. funds, providing spacious headquarters for city services. Built structurally from steel produced at the Columbia Torrance Mill and finished in concrete, the design is austere. The facade is divided into three large flat bay openings with decorative sidelights set into the windows. Extensively remodeled in 1959 for use by Home Savings, it is now occupied by Group W Cable.

T.H.S. plaque



The Bridge, 1912 **B-1**
Torrance Boulevard & Santa Clara Street

This graceful bridge was the original gateway to the City. This was designed by Irving Gill, and was built for and operated by the Pacific Electric Railway.



Jared Sidney Torrance Memorial Hospital, 1924 **B-5**
1425 Engracia Avenue
Architect: Merrill W. Baird

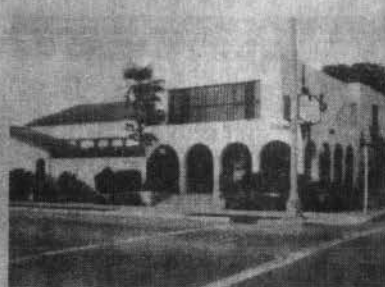
The original building consists of a central two-story block, with one-story wings influenced by Spanish Colonial elements. Opened in 1925, it was erected as a direct result of the provision of \$100,000 for the building of a hospital in the will of Mr. Torrance. There is a major three-story addition adjoining the original structure. The hospital was used until 1971 and is now the American Prevention Institute—a health center for the aging.

T.H.S. plaque



The Pacific Electric Depot, 1912 **B-2**
1300 Cabrillo Avenue

The Pacific Electric Depot was the major focal point of Downtown Torrance. It was designed by Irving Gill. This building is the Mission Style of architecture. Originally the building had a large bronze dome and a red tile roof which have been removed.



Stone & Myers Mortuary, 1924 **B-6**
1221 Engracia Avenue

The building was built in 1924 by O. W. Stone and his daughter Bessie, in the old Spanish style with tile roof, etc. Upstairs was all one apartment; now it is a smaller apartment and offices.

In the mid 1950's new garages and a bachelor apartment was added in the back. That apartment is now used for a preparation room.

In 1963 a major remodeling project moved the front door from Engracia to Cravens Avenue, added the arched windows, bells in the arches in the wall, rock walled patio and chapel front.



Credit Bureau of Torrance, 1928 **B-7**
1266 Sartori Avenue

This is an example of Spanish Colonial Revival architecture blended with Mission Revival architecture. The Pacific Telephone and Telegraph Company originally owned and operated this building.



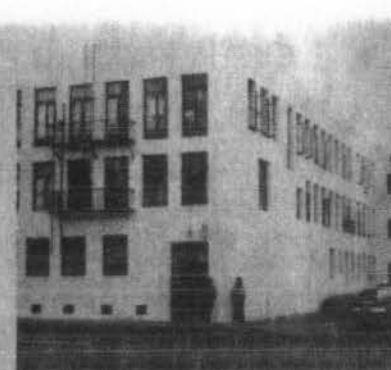
J. J. Newberry Company, 1930 **B-8**
1315 El Prado

J. J. Newberry's is notable for its Zig Zag Moderne style of architecture, which was very typical of the early 1930's.



Mayfair Apartments, 1927 **B-9**
2014 Torrance Boulevard

The Mayfair Apartments were built at the peak of the oil boom in Torrance. These apartments helped to satisfy the housing demands brought on by the City's fast growth.



El Roi Tan Hotel, 1912 **B-10**
1210 El Prado

The El Roi Tan is notable because it was designed by Irving J. Gill. Two other downtown hotels, the Colonial at 1607 Cabrillo, and the Brighton at 1639 Cabrillo were also designed by Gill.

TORRANCE HISTORICAL SOCIETY

Board of Directors - 1984/85

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This tour brochure was made possible by the generous donations of the following people interested in the Torrance Historical Society.

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Jules and Cheryl Walder
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HOME TOUR COMMITTEE

Torrance Historical Society
Chris Alter
Georgian Griswold
Rob Katherman
Olde Torrance Neighbors
Derf Fredericks
Janet Payne

Saturday & Sunday — October 13 & 14, 1984
10:00 a.m. — 4:00 p.m.

HISTORY OF TORRANCE

Labor troubles in Los Angeles in 1910, which led to the dynamiting of the Los Angeles Times building and the Llewellyn Iron Works plant there, planted the seed of the City of Torrance. Jared Sidney Torrance, a prominent millionaire-financier who lived in South Pasadena, decided to build a model industrial city halfway between Los Angeles and its harbor at San Pedro, which expected to boom when the Panama Canal opened in 1914. Torrance and his associates bought several thousand acres of the Rancho Dominguez from the Dominguez family. Then, with their extensive business contacts, they persuaded the Union Tool Co., Llewellyn, the Pacific Electric Railway, and other manufacturing and related firms to establish facilities in the area now known as downtown Torrance.

Torrance's planned industrial community was laid out by the sons of famed landscape architect Frederick Law Olmsted of Boston, with shops and residences to the west and factories to the east so prevailing winds would blow the smoke away. Architect Irving Gill of San Diego was hired by the developer, the Dominguez Land Corp., to serve as architect for the project. Gill designed the Pacific Electric depot, the Torrance Boulevard railroad bridge, and many other structures in the new city. A 1913 recession and the onset of World War I in 1914 brought economic hard times to Torrance. After the war growth resumed, the city incorporated to avoid annexation by Los Angeles in 1921, and discovery of a large oil field brought prosperity through the 1920's. Torrance suffered with the nation through the hard times of the 1930's and the sacrifices of World War II. That war also brought new activity in the fledgling local aircraft industry and strained available housing as people moved in to work in that field.

By the early 1950's, the new housing tracts between the Hollywood Riviera and Waverly to the south and North Torrance shifted the population center of the city away from downtown, and the civic center moved west from downtown to its current location on Torrance Boulevard. Despite rapid growth, Torrance received an "All American City" award in 1956 for handling development "without strain." During the late 1950's, a new commercial center was established on Del Amo property along Hawthorne Boulevard, a center which has grown until today it is the largest enclosed shopping mall in the world.

Torrance remains a vital community, balancing industry, commerce, and residential needs to ensure a strong tax base and a quality environment in which to live, work, and shop.

THE TORRANCE HISTORICAL SOCIETY

The Torrance Historical Society was created in 1973 to acquire, display, and encourage public interest in the preservation of items important to the understanding of the history of Torrance.

Dr. Albert Posner, of the Torrance Unified School District, was the first president of the Society. Attorney, John Alter, was his vice president. Claire and John Crain were two of the most active early members.

At that time, the Post Avenue Library at 1345 Post Avenue, formerly the main library, was still a functioning branch of the Torrance Public Library system. One of the Society's goals was to establish an historical room at that branch. The first such display was mounted in 1974, and the Society's newsletter, edited by City Librarian, Russell West, premiered in April, 1975.

Though the Post Avenue Branch Library later closed, the Historical Society continued its strong interest in establishing a permanent museum at the site. With the cooperation of city government, the museum was opened on March 15, 1979.

On May 21, 1981, the museum had a grand opening, officially "showing off" the extensive displays designed and built by Bob Wade, a Museum of Natural History retiree. Those displays form the core of the current museum and vividly depict the history of the city. Their unveiling coincided with the 60th Anniversary Celebration of the City of Torrance.

The Society actively seeks to gather photographs, documents, books, artifacts, and other materials relevant to the city's past. Volunteers conduct tape interviews with early pioneers for future references. Plans for a historical marker, memorializing the achievement of Mr. Torrance in creating the city, is underway, and a long-term program of placing bronze plaques on historic landmarks has been inaugurated.

Additional public awareness is made possible through cable television and a traveling slide show for those who may not be cognizant of the city's rich and interesting past.

Since its inception, the Society has had a variety of social activities and fundraisers to include its members and interested persons. These have included an annual dinner, lectures about the history of other community groups, ice cream socials, an Annual Christmas Tree Decorating Party, publication of a calendar featuring historic pictures, and a road rally keyed to historic sites. The most popular event has been the walking tour of historic downtown homes and buildings. Beginning in 1980, this fall tour has featured many significant structures associated with local history. Gracious neighbors have opened their homes for interior viewing, and many public and private buildings, including our chapels and churches, are following this same community involvement.

In an historic undertaking, the Society has cooperated with Legends Press in the writing and publishing of "Historic Torrance," a beautiful, limited edition pictorial history of Torrance. This hardbound collector book, written by Chuck Elliott and Dennis Shanahan, with graphic design by William Fridrich, is available for purchase at the museum.

The Torrance Historical Society Museum is open, free to the public, Sunday through Friday from 1-4 p.m. and by special arrangement for groups requiring a docent. Membership is open to all interested citizens.

19-178539



United States Department of the Interior

NATIONAL PARK SERVICE

P.O. BOX 37127

WASHINGTON, D.C. 20013-7127



IN REPLY REFER TO:

The Director of the National Park Service is pleased to inform you that the following properties have been entered in the National Register of Historic Places. For further information call 202/343-9542.

JUL 21 1989

WEEKLY LIST OF LISTED PROPERTIES
7/10/89 THROUGH 7/14/89

KEY: Property Name, Multiple Name, Address/Boundary, City,
Vicinity, Certification Date, Reference Number, NHL status

ARIZONA

Yavapai County
Mayer Apartments
Central Ave. SW of Ash St.
Mayer 7/13/89 89000860
Mayer Business Block
Oak St. and Central Ave.
Mayer 7/13/89 89000859

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AUG 3 1989

CHF

CALIFORNIA

Alameda County
Loring House
1730 Spruce St.
Berkeley 7/13/89 89000857

Los Angeles County
Pacific Electric Railroad Bridge
Torrance Blvd. and Bow St.
Torrance 7/13/89 89000854

Santa Cruz County
Bockius, Godfrey M., House
322 E. Beach St.
Watsonville 7/13/89 89000937

CONNECTICUT

Hartford County
Worthington Ridge Historic District
Roughly Worthington Ridge from Mill St. to Sunset Ln.
Berlin 7/13/89 89000925

DOCUMENTA * TRANSFER TAX \$ NO TAX DUE

WHEN RECORDED RETURN TO: **86 1685917**City Clerk
3001 Torrance Blvd.
Torrance, California 90503*Donald E. Wilson*
Donald E. Wilson, City Clerk City of Torrance

THIS INDENTURE, made this 16th day of November, 1986,
by and between SOUTHERN PACIFIC TRANSPORTATION COMPANY, a Delaware
corporation, Grantor, and CITY OF TORRANCE, a municipal corporation
of the State of California, Grantee;

FREE

L

WITNESSETH:

1. That Grantor hereby quitclaims, assigns, transfers and sets over unto Grantee all of its right, title and interest in and to that certain bridge structure No. 501.23 located at Mile Post BBG-500.94, at or near Torrance, in the County of Los Angeles, State of California, in the location shown shaded on the print of Drawing C-1870, Sheet No. 1, dated January 13, 1986, attached and made a part hereof.

2. NO WARRANTY IS MADE BY GRANTOR, EITHER EXPRESS OR IMPLIED, AS TO THE CONDITION OF SAID BRIDGE STRUCTURE, OR ITS COMPONENT PARTS, OR THE FITNESS THEREOF FOR USE OR OTHERWISE. GRANTEE ACCEPTS THE SAME IN ITS PRESENT CONDITION WITH ALL FAULTS AND ASSUMES ALL RISKS INCIDENT THERETO.

3. Grantor reserves for itself, its successors and assigns, the right to maintain the track structure on said bridge and to operate thereover with existing and future transportation facilities and appurtenances.

4. The said bridge structure shall be owned and maintained by Grantee. The track structure shall be owned and maintained by Grantor.

IN WITNESS WHEREOF, the parties hereto have caused these presents to be executed in duplicate the day and year first herein written.

SOUTHERN PACIFIC TRANSPORTATION
COMPANY,

By

(Title)

L. V. Peters
Manager - Miscellaneous Contracts

CITY OF TORRANCE,

By

Mayor

By

Clerk

APPROVED AS TO FORM
STANLEY E. REMELMEYER
CITY ATTORNEY

BY

Stanley E. Remelmeier
ASSISTANTRECORDED IN OFFICIAL RECORDS
RECORDER'S OFFICE
LOS ANGELES COUNTY
CALIFORNIA

31 MIN. 9 A.M. DEC 5 1986

Attachment #4 (1 of 3)
Irving Gill Bridge
Indenture Agreement

D-5136

RECEIVED

APR 27 1989

OHP

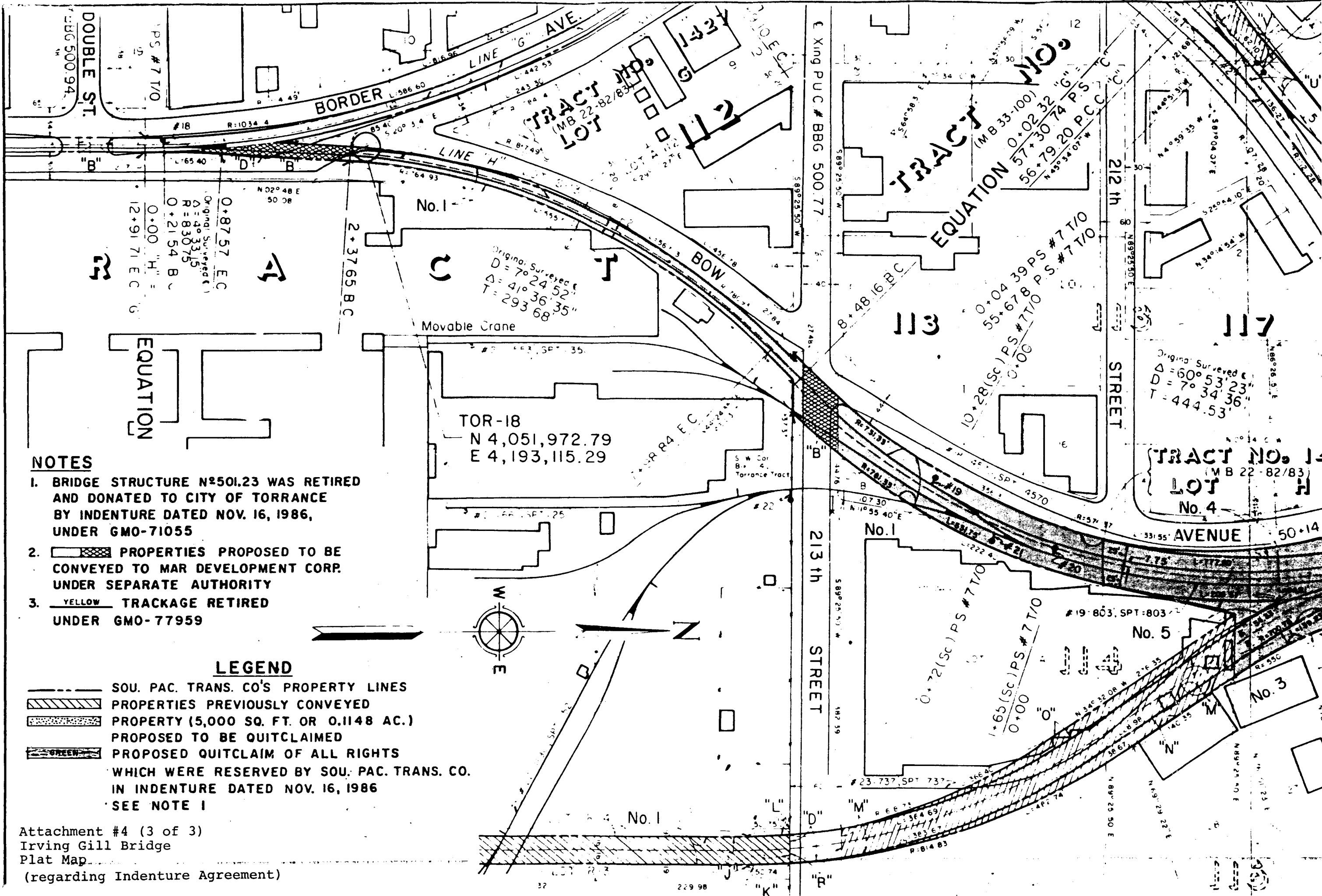
This bridge is significant from an engineering standpoint for its highly unusual use of non-load-bearing walls to create the appearance of an arched bridge. Structurally, the bridge is built around four concrete girders, which are carried on two-column bents and run the length of the bridge, and a concrete slab which comprises the deck. Stripped to its load-bearing elements, the structure would appear as a rather plain concrete girder and slab bridge on concrete columns, similar to dozens of other bridges built in California in the period, 1900-1917, and not unlike the typical two-column concrete freeway overcrossing built in California in the 1960s and 1970s. The significance of the bridge from an engineering standpoint lies in the highly unusual use of decorative elements to disguise the structural system and to create a more pleasing aesthetic appearance.

As shown on the attached exhibits, the principal members in the bridge substructure are the heavily reinforced concrete columns (labeled 1 on attachment A). These carry four concrete girders (2, Attachment A) and the concrete slab deck, which extend the length of the bridge. There are five sets of these paired columns, which serve as bents for the six spans.



The columns and girders are hidden behind three elements, which appear to be entirely decorative and not load-bearing. The first (4, Attachment A) is a wide wall, probably hollow, which brings the exterior walls to the vertical plane of the deck, creating a flush surface on the outside of the bridge. The second (3, Attachment A) is an arched form poured between the columns. This element is repeated in each bent, creating an arcade that runs the length of the bridge. The third element (5, Attachment A) is an arched form poured between the bents. This creates an arcade that runs the length of the bridge, although the geometry of the arches differs with the different lengths of the spans. The pedestrian spans, at 15' each, and the railroad spans, at 18' each, feature round-headed arches, while the wider highway spans (29'4") feature elliptical arches.

The three decorative elements transform what would have been a very plain concrete bridge into a very elegant and interesting structure. It is a very complex composition because of the different shapes of arches and the double arcade, but also very simple and elegant because of the unbroken vertical plane and the absence of applied decorative features.





This design is highly unusual. The California Historic Bridge Inventory, which investigated more than 100 historic concrete girder bridges in California, revealed no other example of this particular technology. It is likely that the design is unique to this structure, developed for the purpose of improving the appearance of the prominently-sited structure.



NOTES

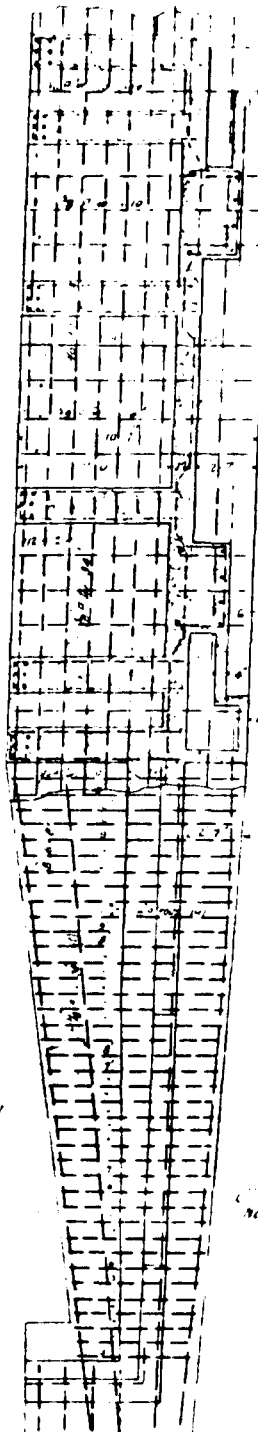
1. BRIDGE STRUCTURE N^o501.23 WAS RETIRED AND DONATED TO CITY OF TORRANCE BY INDENTURE DATED NOV. 16, 1986, UNDER GMO-71055
2.  PROPERTIES PROPOSED TO BE CONVEYED TO MAR DEVELOPMENT CORP. UNDER SEPARATE AUTHORITY
3.  TRACKAGE RETIRED UNDER GMO-77959

LEGEND

-  SOU. PAC. TRANS. CO'S PROPERTY LINES
 -  PROPERTIES PREVIOUSLY CONVEYED
 -  PROPERTY (5,000 SQ. FT. OR 0.1148 AC.) PROPOSED TO BE QUITCLAIMED
 -  PROPOSED QUITCLAIM OF ALL RIGHTS WHICH WERE RESERVED BY SOU. PAC. TRANS. CO. IN INDENTURE DATED NOV. 16, 1986
- SEE NOTE 1

Attachment #4 (3 of 3)
Irving Gill Bridge
Plat Map
(regarding Indenture Agreement)

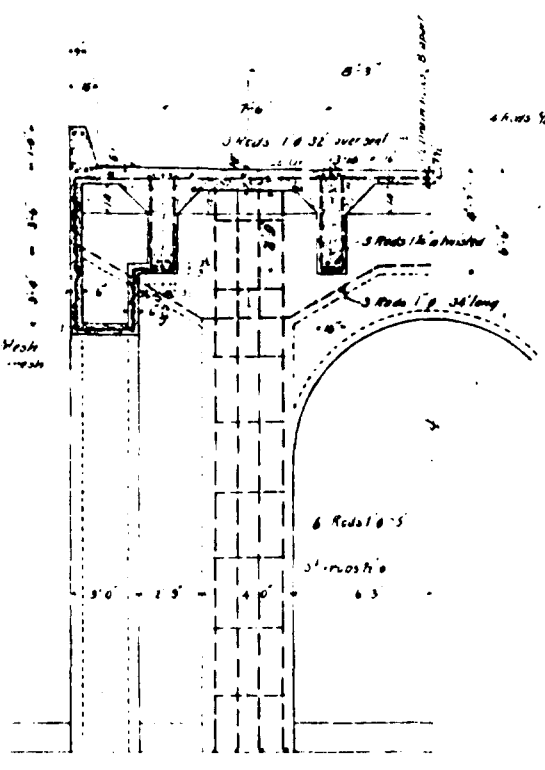
Attachment #2 (1 of 2)
Irving Gill Bridge
Original Plans
(reduced to 63%)



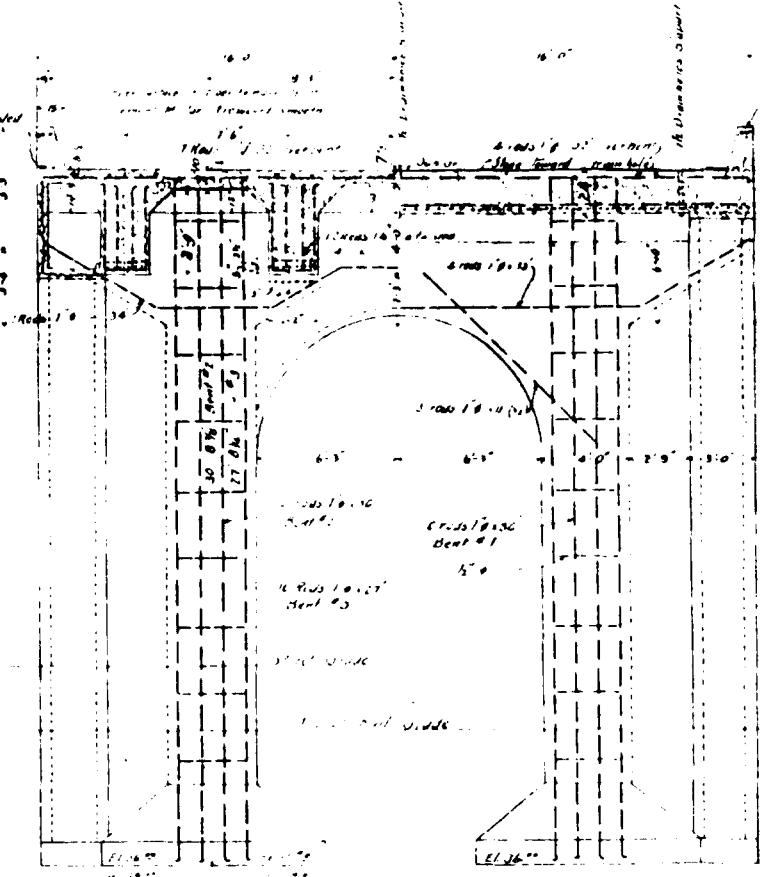
Plan of wing showing ribs in base

A.S. & Co. (Close Air Mail)
Style #6

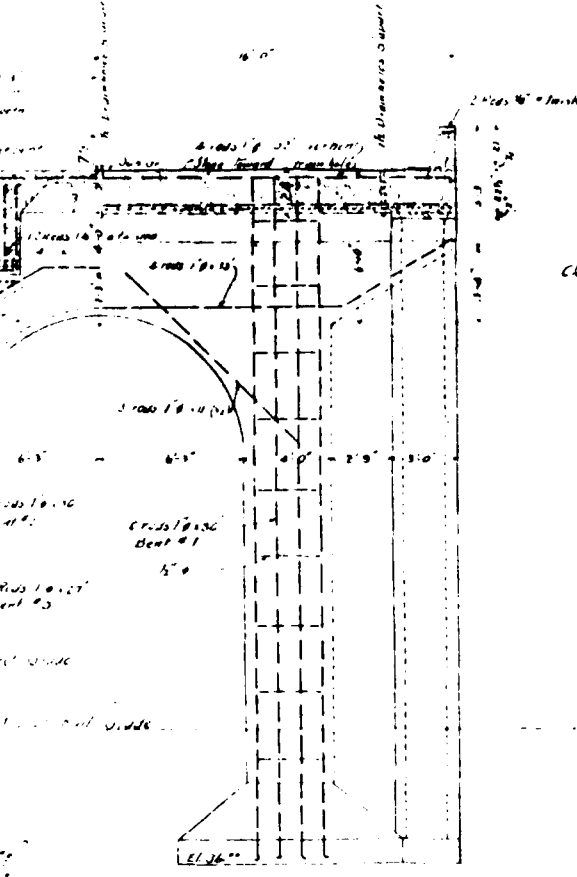
Plan of Footings



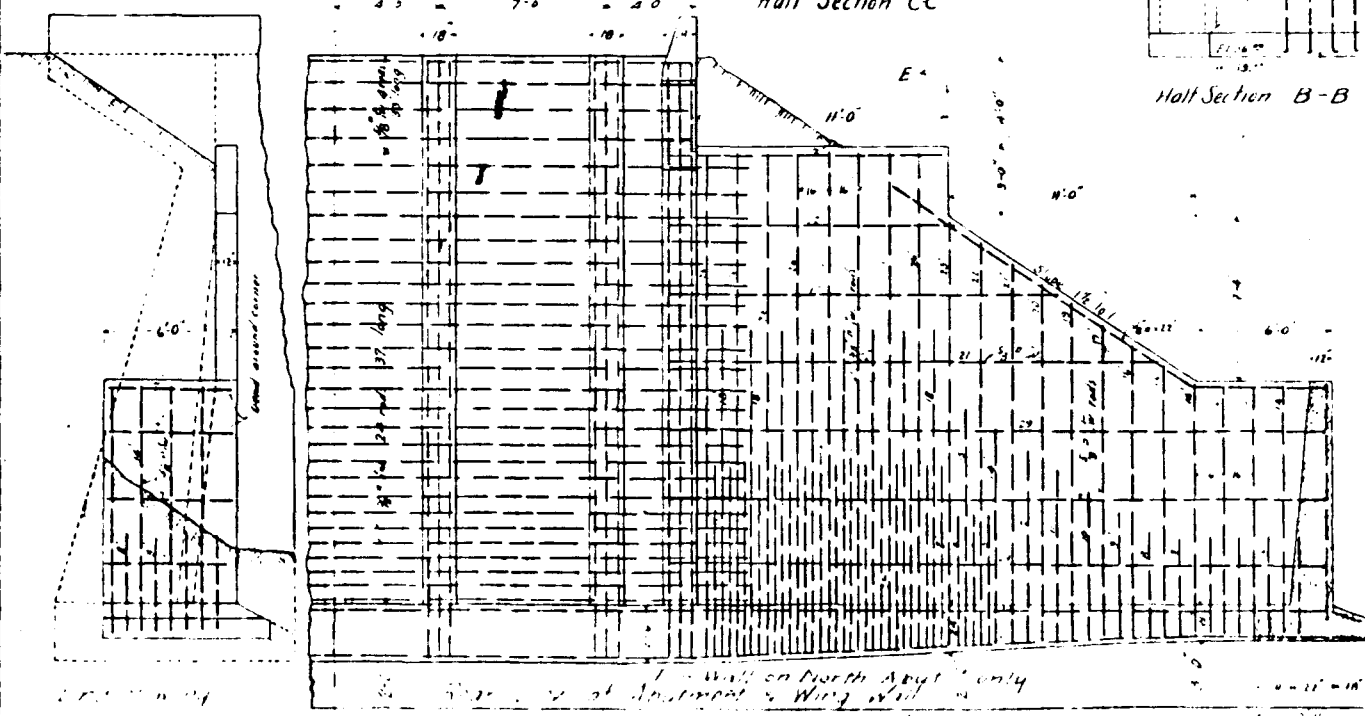
Half Section CC



Half Section B-B



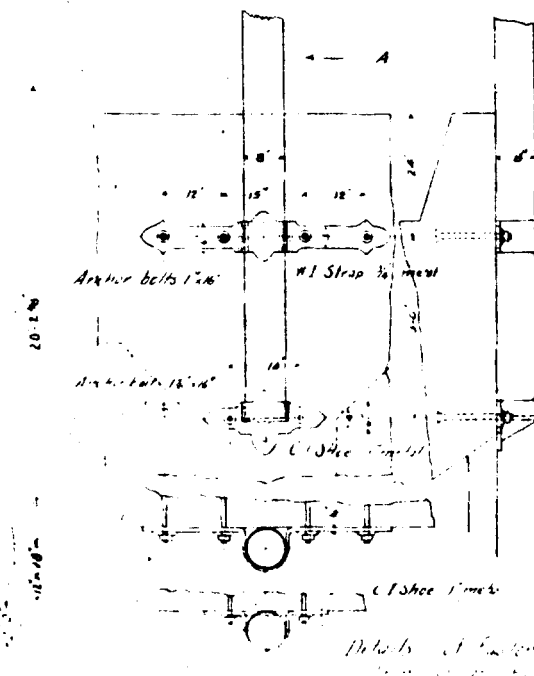
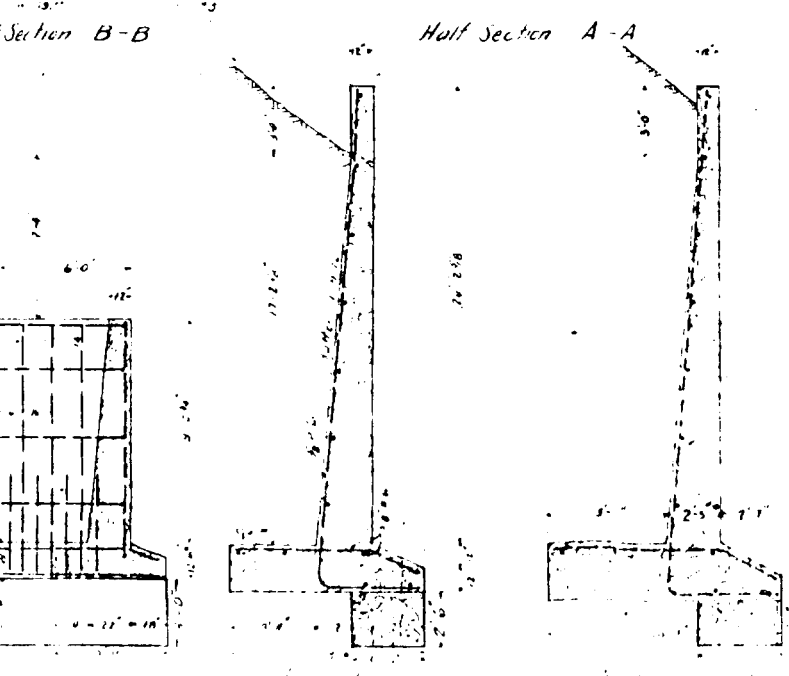
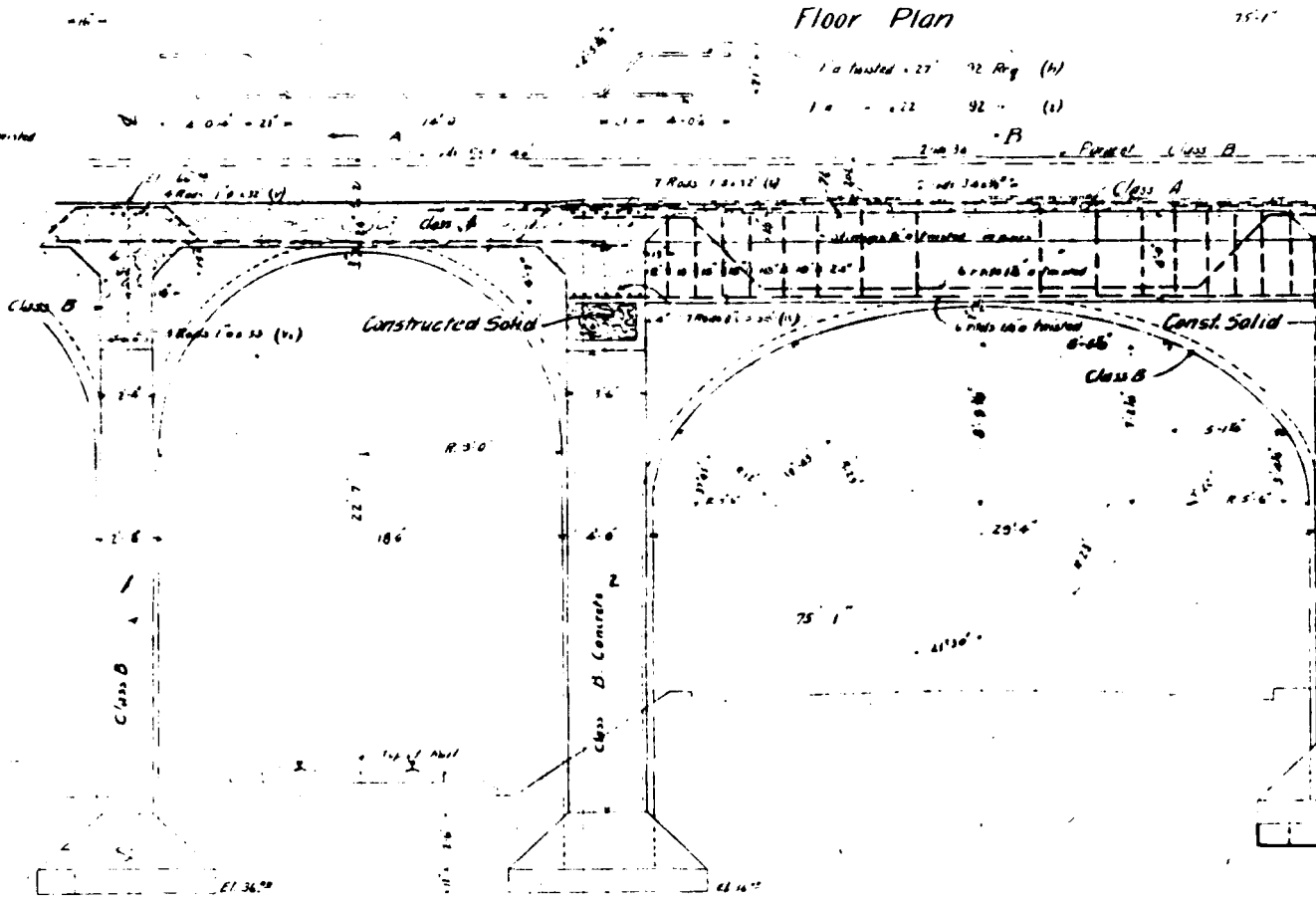
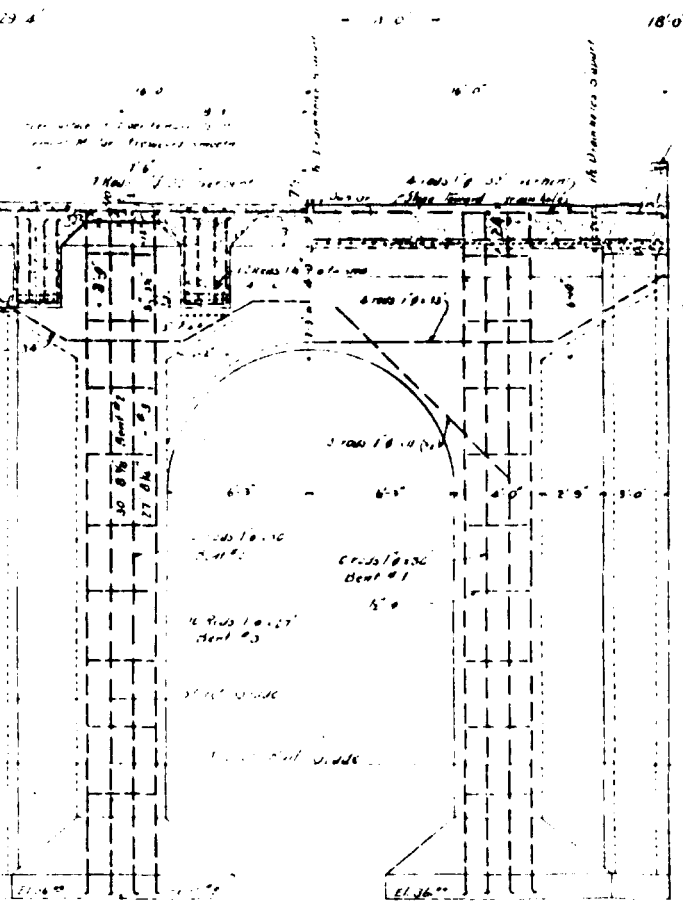
Half Section A-A



Plan of wing showing ribs in base

5' Footings

Floor Plan



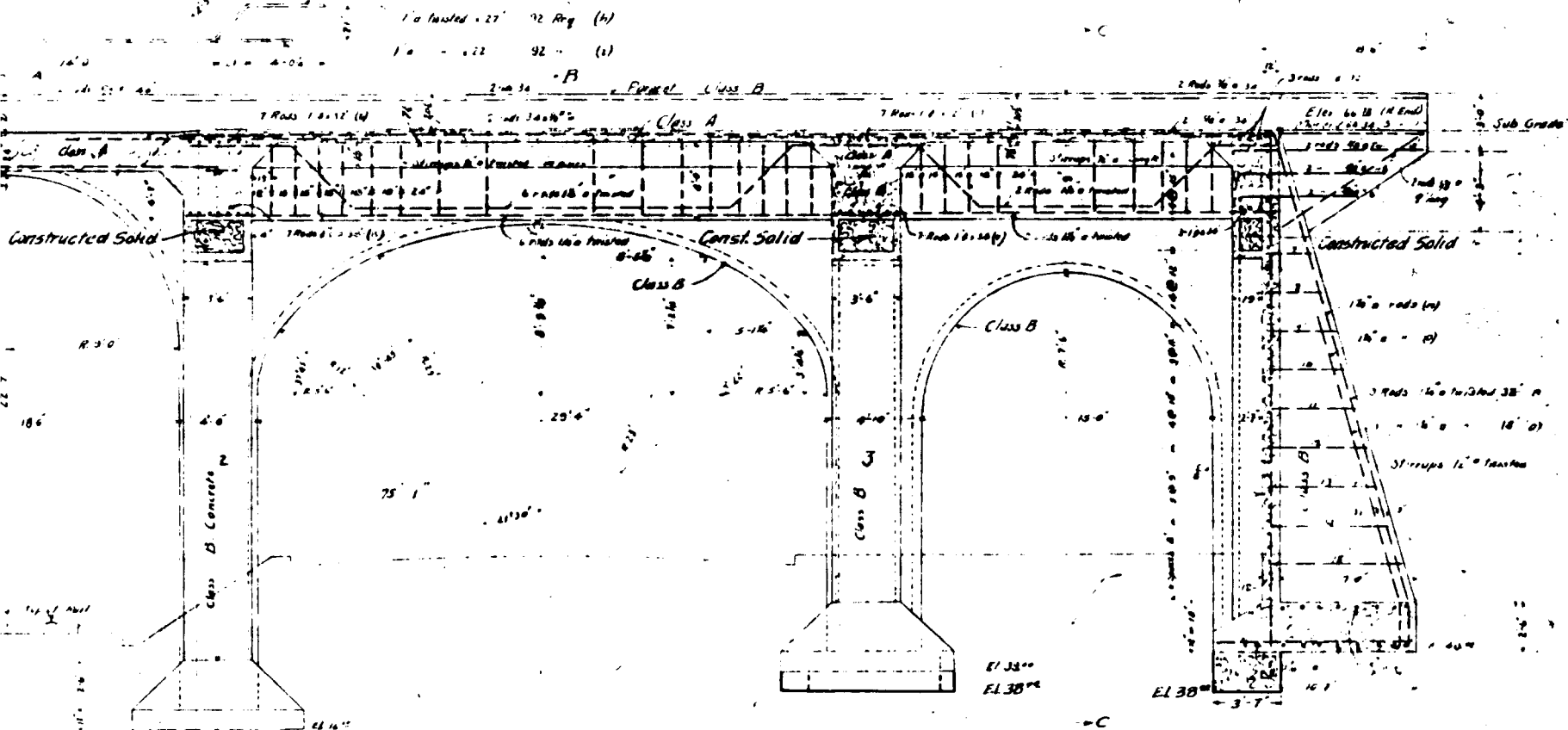
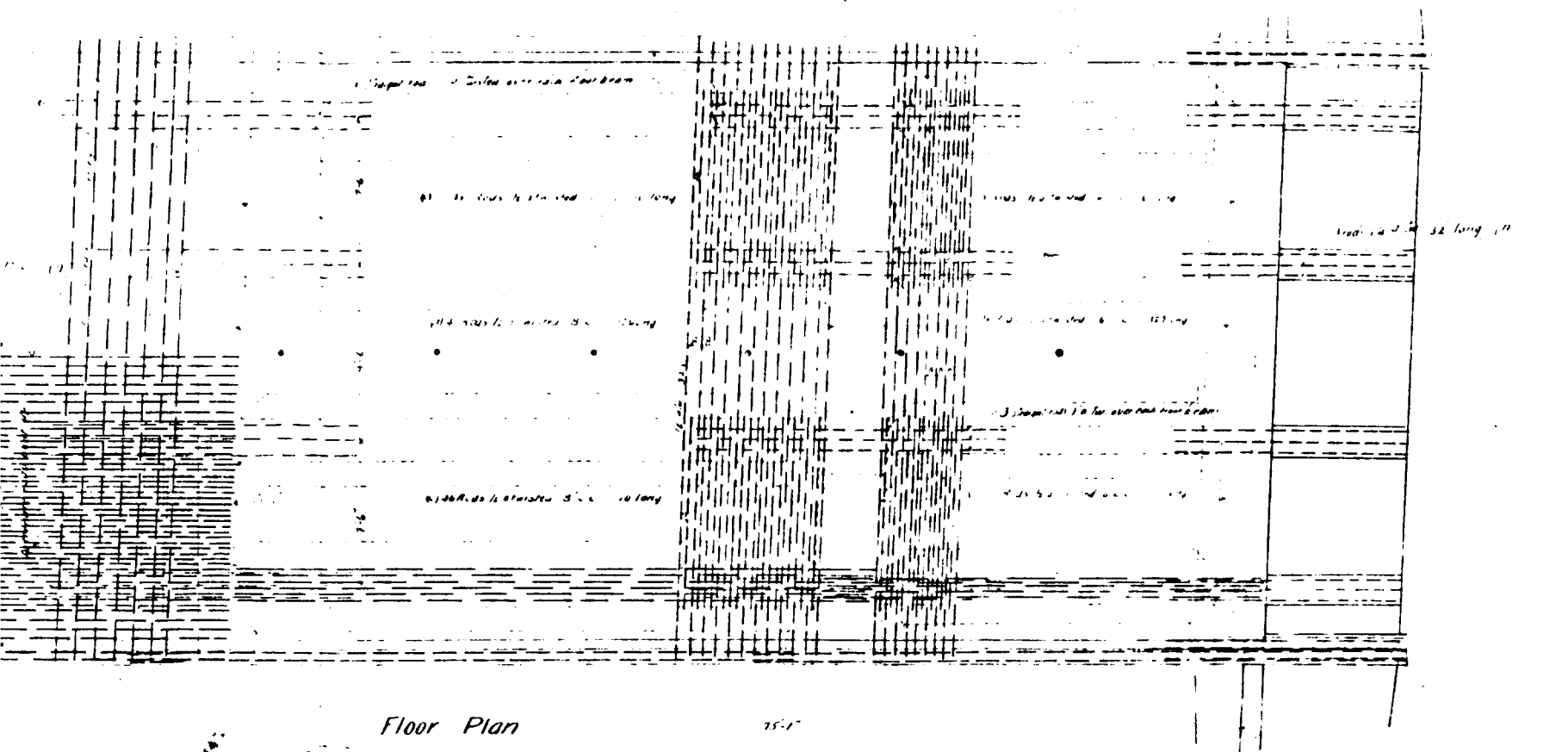
Longitudinal Section on \angle of Bridge

TORRANCE LINES
EL PRADO BRIDGE - DETAILS
PACIFIC ELECTRIC RY.

Scale: $\frac{3}{8}$ " = 1 Foot

June 3, 1913

APPROVED *[Signature]*
CHIEF ENGINEER P.E.R.
APPROVED DOMINGUEZ LAND CORP.
[Signature] R. Bennett
CHIEF ENG.



Longitudinal Section on Q of Bridge

TORRANCE LINES
EL PRADO BRIDGE - DETAILS
PACIFIC ELECTRIC RY.

Scale: 3/8" = 1 Foot

June 3, 1913

APPROVED

CHIEF ENGINEER PER
APPROVED DOMINGUEZ LAND CORP.

W. H. Bennett
CH. Eng.

Class A Concrete 1:2:4
Class B 1:2:5
Subs. & Beams to be Class A Concrete.
Piers, Footings & Abutments to be Class B Concrete.
Steel to conform to Standard Specifications for Concrete.
Reinforcement Bars as adopted by the Association
of American Steel Manufacturers 1910, Revised 1912.
Loading: LL - Coopers E 55. Impact - LL + 1.0, D.L. 6000' hnt track
Stress in Steel 15000 round bars; 22000 cold twisted bars.
Compression in Columns, including Impact 386' sq. in.

RECEIVED

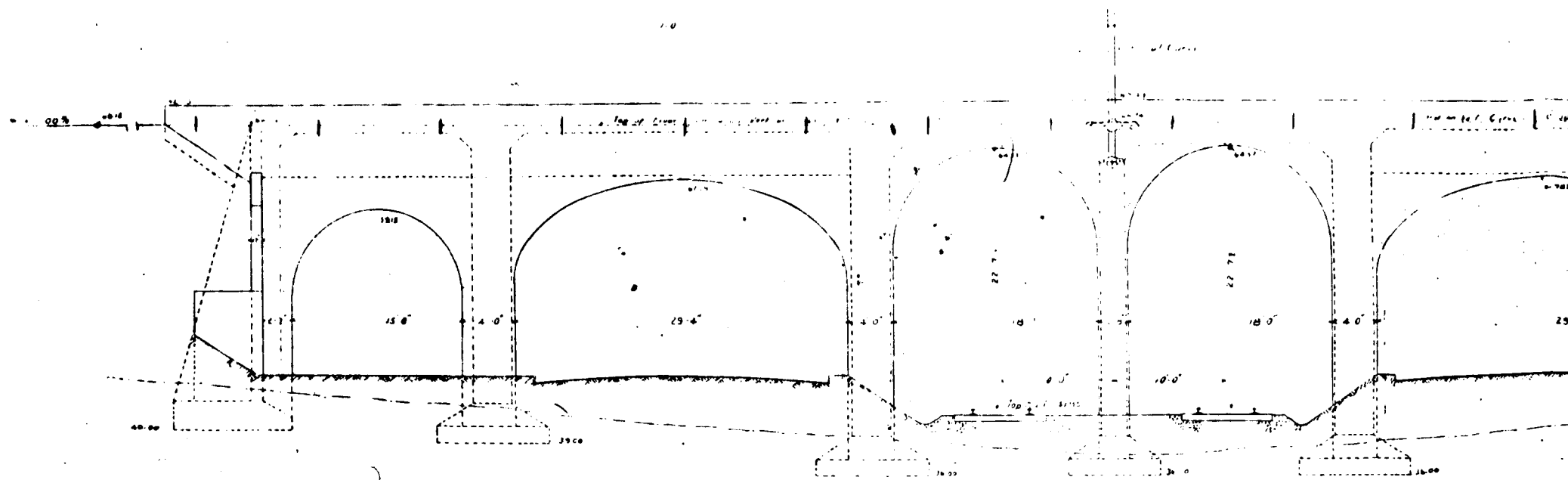
MAR 24 1952

OFFICE OF CITY ENGINEER

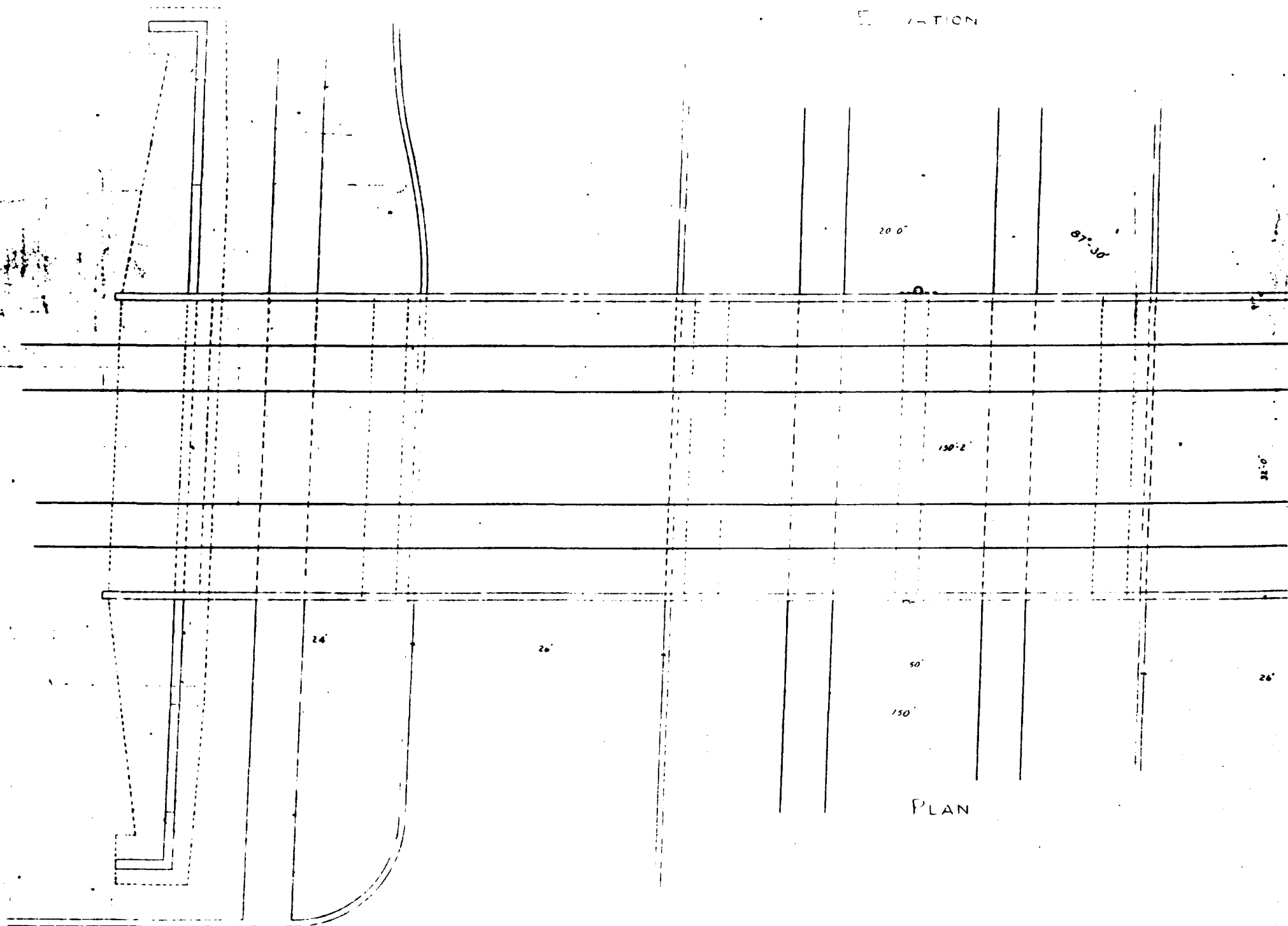
PACIFIC ELECTRIC RY
Office of Chief Engineer

C-270-a

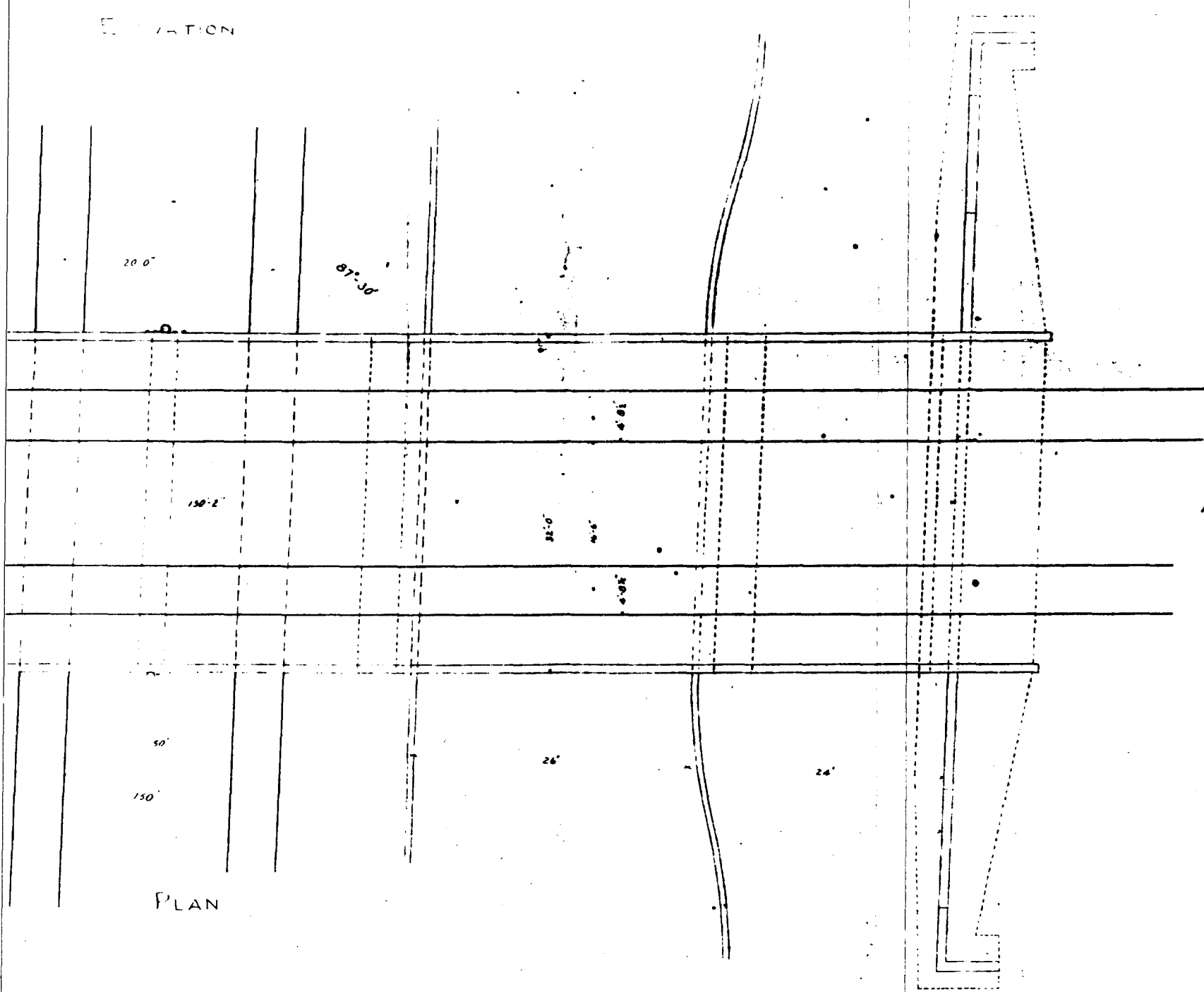
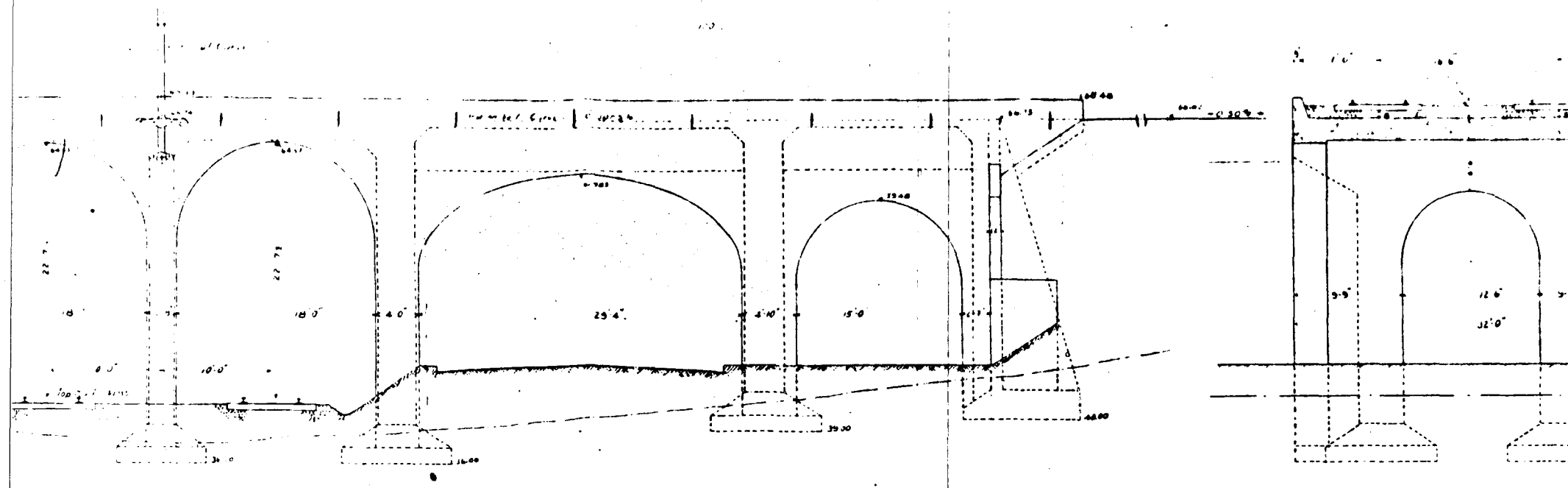
Attachment #2 (2 of 2)
Irving Gill Bridge
Original Plans
(reduced to 63%)



ELEVATION

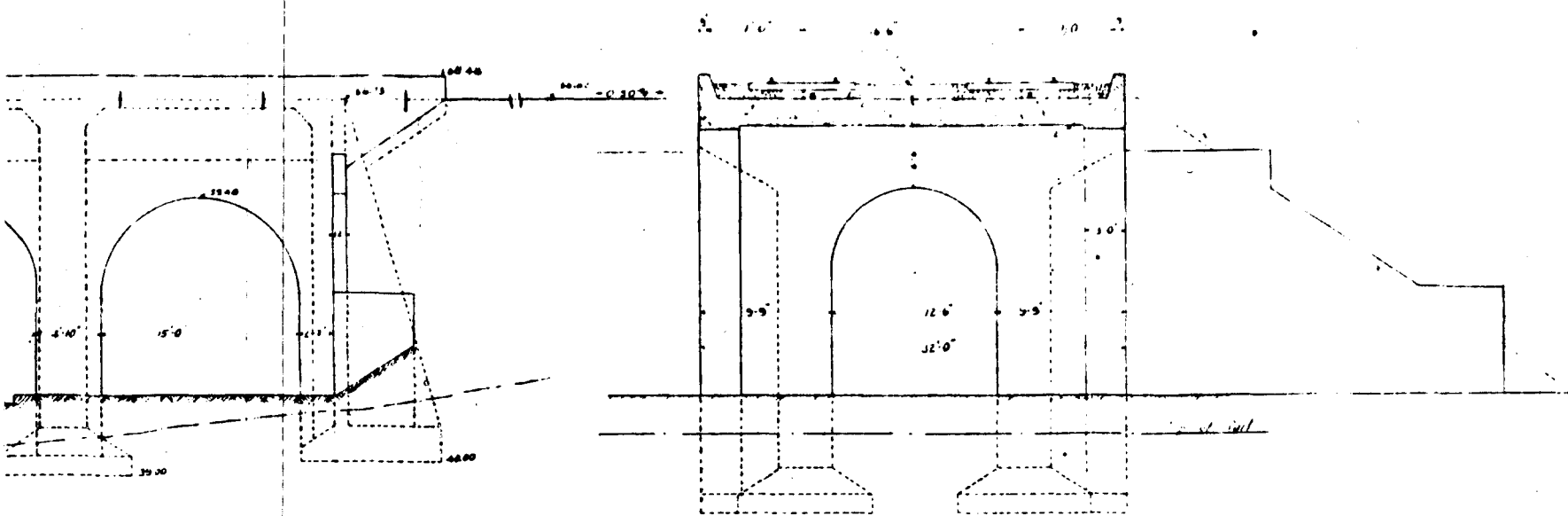


PLAN

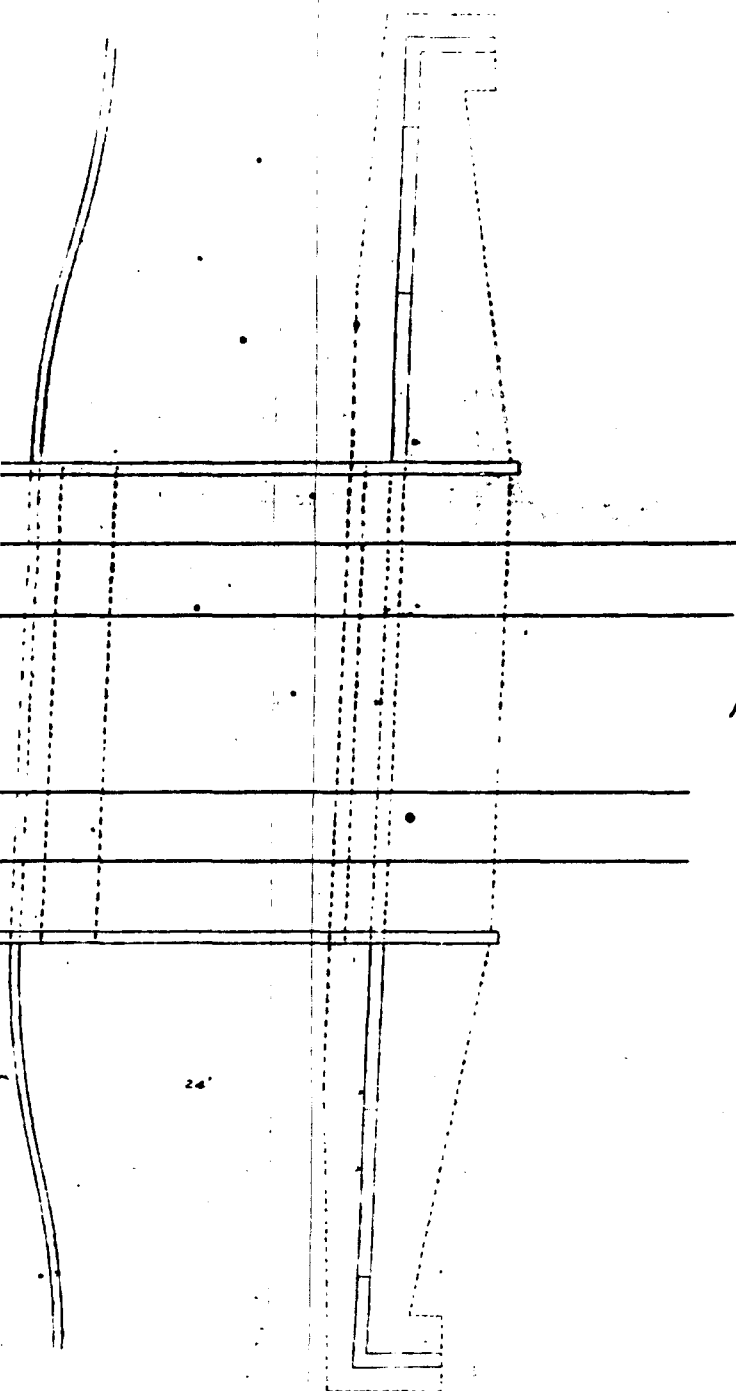


N >

TORRAN
EL PRADO BRIDGE
PACIFIC EL
Scale 1/8" = 1 Ft



SECTION



N >

TORRANCE LINES
EL PRADO BRIDGE - GENERAL PLAN
PACIFIC ELECTRIC RY CO.

Scale 1/8" = 1 Ft

June 3, 1913.

APPROVED

CHIEF ENGINEER

APPROVED DOMINGUEZ LAND COR.

Ralph B. Bennett

6-17-13

Ch. Eng.

RECEIVED

MAR 24 1952

OFFICE OF CITY ENGINEER

PACIFIC ELECTRIC RY
CHIEF OF CHIEF ENGINEER

C-270

ST. AL. LAMIN. AND 8-20-13 11-11-13
COPY 1-1-13 1-1-13

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code _____
Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 8

*Resource Name or #: (Assigned by recorder) Dow Chemical Plant

P1. Other Identifier: Parcel 7352-002-017

- *P2. Location: ☐ Not for Publication ☒ Unrestricted *a. County Los Angeles
and P2c, P2e, and P2b or P2d. (Attach Location Map as necessary.)
*b. USGS 7.5' Quad Torrance Date 1981 T 4 S; R 14 W : Unsectioned ; San Bernardino B.M.
c. Address 305 Crenshaw Boulevard City Torrance Zip 90504
d. UTM: (Give more than one for large and/or linear resources) Zone: 11 ; NW corner 376353 mE/ 3745820 mN; NE corner 377135 mE/ 3745813 mN; SE corner 377133 mE/ 3745427 mN; SW corner 376771 mE/ 3745427 mN
*e. Other Locational Data: (E.g., parcel #, directions to resource, elevation, etc., as appropriate.) The Dow Chemical Plant is located at 305 Crenshaw Boulevard, at the southwest corner of the intersection with Del Amo Boulevard.

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)
The Dow Chemical Plant district is a 52-acre plastics production facility. The plant is an industrial production and manufacturing facility with numerous buildings, structures and heavy equipment. Typically, the buildings, structures, and equipment are surrounded by pavement or dirt, with limited plantings around the administration buildings. The plant's first buildings were constructed in 1952-1953, with continuous development through present time. Although the plant now consists of numerous buildings and structures, this district is described as the 52-acre Dow Chemical Plant, with five contributing elements: 1) Building 111; 2) Building 112; 3) Building 121; 4) Building 121A and 121B; and 5) Three steel tanks.

*P3b. Resource Attributes: (See attributes and codes) HP8. Industrial building HP11. Engineering structure

*P4. Resources Present: ☐ Building ☐ Structure ☐ Object ☐ Site ☒ District ☐ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo:
(View, date, accession #) View to the South,
5/1/2003, 0H156-MS-3044

*P6. Date Constructed / Age and Sources: ☒ Historic
☐ Prehistoric ☐ Both
1953 - present (factual)

*P7. Owner and Address:
Dow Chemical
305 Crenshaw Blvd.
Torrance, CA 90504

*P8. Recorded by: (Name, affiliation, and address) M. Strauss
EDAW, Inc.
3780 Wilshire Blvd, Ste. 250
Los Angeles, CA 90010

*P9. Date Recorded: 05/01/2003

*P10. Survey Type: (Describe)
Reconnaissance

*P11. Report Citation: (Cite Survey report and other sources, or enter "none.")

Dolan, Christy 2003. Historic Resources Evaluation Report, Del Amo Boulevard Extension Project, City of Torrance, Los Angeles County, California. Prepared for the City of Torrance and the U.S. Federal Highway Administration c/o California Department of Transportation, District 12.

*Attachments: ☐ None ☒ Location Map ☐ Sketch Map ☐ Continuation Sheet ☐ Building, Structure, and Object Record
☐ Linear Resource Record ☐ Archaeological Record ☒ District Record ☐ Milling Station Record ☐ Rock Art Record
☐ Artifact Record ☐ Photograph Record ☒ Other (List) Primary forms (5)

LOCATION MAP

Primary # 19-186929
HRI #
Trinomial

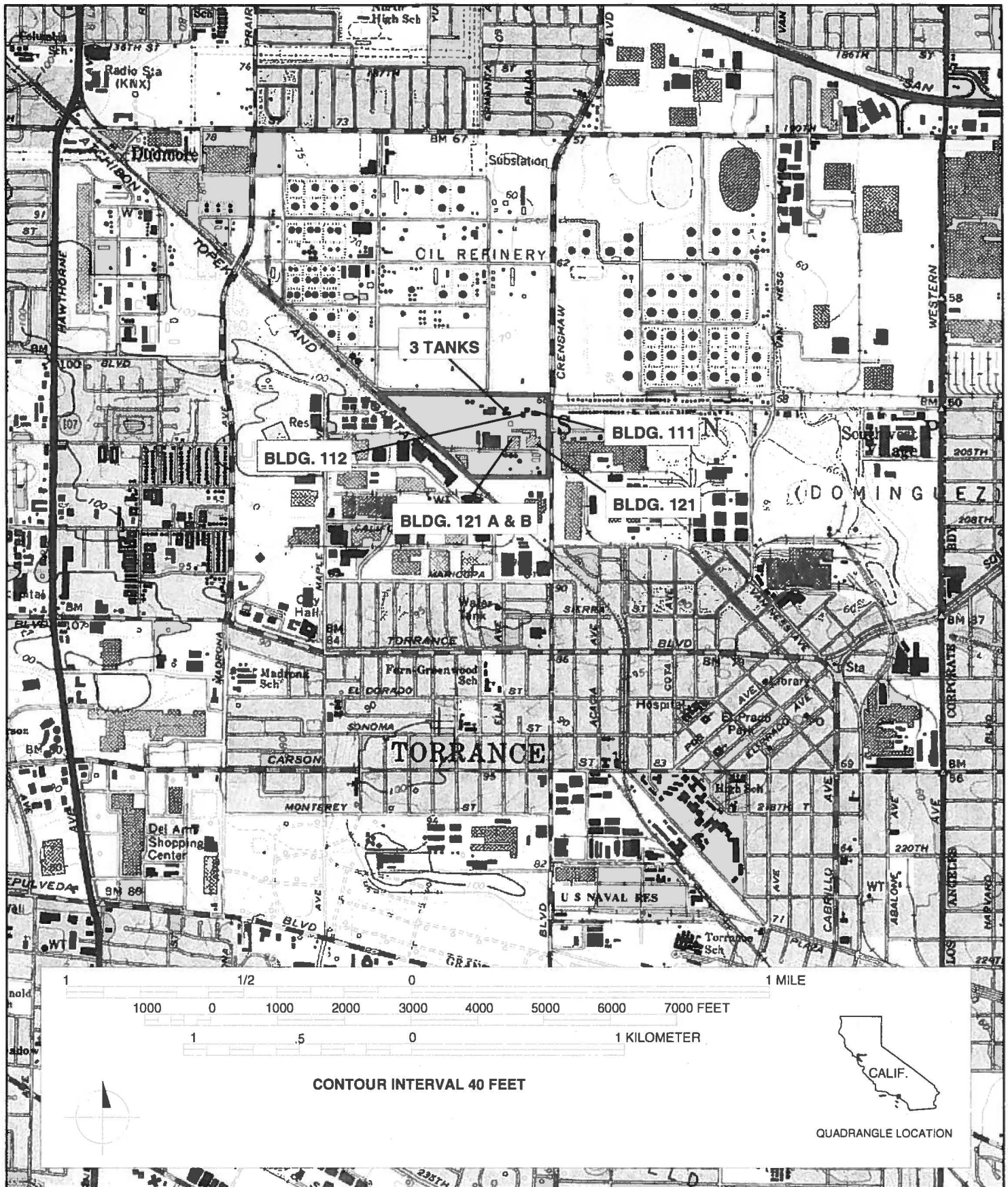
Page 2 of 8

* Resource Name or # (Assigned by recorder) Dow Chemical Plant

* Map Name: Torrance, Calif. 7.5' Quadrangle

* Scale: 1:24,000

* Date of Map: 1981



State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
DISTRICT RECORD

Primary # _____
HRI # 19-186929
Trinomial _____

Page 3 of 8

*NRHP Status Code 6Z

*Resource Name or # (Assigned by recorder) Dow Chemical Plant

D1. **Historic Name:** Dow Chemical Plant

D2. **Common Name:** Same

*D3. **Detailed Description** (Discuss overall coherence of the district, its setting, visual characteristics, and minor features. List all elements of district):
The Dow Chemical Plant is a 52-acre plastics production facility located in Torrance, California among other industrial facilities. Although Dow Chemical began operations in Torrance during World War II, the plant's initial development occurred in the early 1950s. The plant is an industrial production and manufacturing facility with numerous buildings, structures and heavy equipment. Typically, the buildings, structures, and equipment are surrounded by pavement or dirt, with limited plantings around the administration buildings. The plant's first buildings were constructed in 1953, with continuous development through present time. Although the plant now consists of numerous buildings and structures, this district is described as the 52-acre Dow Chemical Plant, with five contributing elements: 1) Building 111; 2) Building 112; 3) Building 121; 4) Building 121 A and 121B; and 5) Three steel tanks.

*D4. **Boundary Description** (Describe limits of district and attach map showing boundary and district elements.):
The boundaries of the Dow Chemical Plant is consistent with that of parcel 7352-002-017. The property is bound by the Alcoa Spur Line to the north, Crenshaw Boulevard to the east, parcel 7352-002-008 to the south, and the Harbor Subdivision of the Burlington Northern Santa Fe Railroad to the west.

*D5. **Boundary Justification:**
The boundaries of the district encompass the entire Dow Chemical Torrance Plant property.

*D6. **Significance: Theme** Industrial Development **Area** Southern California
Period of Significance Post World War II era 1945-1960 **Applicable Criteria** N/A
(Discuss district's importance in terms of its historical context as defined by theme, period of significance, and geographic scope. Also address the integrity of the district as a whole.)
Torrance was designed as an industrial community in 1912, with industry moving to the area in the late 1910s. Oil was discovered in the region in the early 1920s and the first well came in during 1922. Oil industries became prominent during the following decades. Although several factories closed during the Great Depression, World War II brought prosperity and opportunity back to Torrance. The War brought contracts to some of the existing factories and spurred the opening of several others. Torrance's post-World War II boom was spawned by the aerospace industry. A total of 78,000 new residents moved to Torrance between 1950 and 1960.

Dow Chemical began operations in Torrance during World War II. Natural supplies of rubber from Malaysia and Java were depleted when the Japanese invaded. The Shell Chemical Torrance Plant was part of a crash program to develop and produce a synthetic rubber. The Government invited Dow Chemical to build and operate the styrene manufacturing department at the facility, as Dow had successfully developed that process. In 1953, the Dow Chemical Torrance plant began producing STYRON, followed by STYROFOAM in 1956. By the mid-1950s, Dow Chemical was one of the mainstay industries in Torrance. The Torrance plant began producing polystyrene in 1959, followed by polypropylene in 1961. Since a major STYRON production expansion in 1973, the Torrance plant has had a number of new products introduced to the complex. The 52-acre Torrance plant is currently one of the largest plastics production facilities in the Western United States.

Although the district does retain some integrity, and all of the contributing elements have remained in use, the district does not appear to be significant. Most of the seven buildings and structures at 305 Crenshaw Boulevard have been altered over time. Their setting has changed by the addition of several more buildings and tanks between 1964 and 1981. None of the buildings or structures are outstanding examples of their style or design. They are not associated with any events or people important in our history. Further study of this complex is unlikely to yield information important to our understanding of history. This property is not eligible for inclusion on the National Register.

*D7. **References** (Give full citations including the names and addresses of any informants, where possible.):
Personal Communication with Fred Smalling, Dow Chemical Plant employee, 305 Crenshaw Blvd., Torrance, California.
History of Torrance Plant marketing brochure provided by Dow Chemical.
Shanahan, Dennis F. and Charles Elliott, Jr. 1984 *Historic Torrance: A Pictorial History of Torrance, California*. Legends Press, Redondo Beach, California.

*D8. **Evaluator:** C. Dolan **Date:** 05/06/2003
Affiliation and Address: EDAW, Inc.
1420 Kettner Boulevard, Suite 620
San Diego, California 92101

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # 19-186929
HRI # _____
Trinomial _____

NRHP Status Code _____

Other Listings _____

Review Code _____ Reviewer _____ Date _____

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*Resource Name or #: (Assigned by recorder) Building 111

P1. Other Identifier: Administration Building

*P2. Location: ☐ Not for Publication ☒ Unrestricted *a. County Los Angeles

and P2c, P2e, and P2b or P2d. (Attach Location Map as necessary.)

*b. USGS 7.5' Quad Torrance Date 1981 T 4 S; R 14 W : Unsectioned ; San Bernardino B.M.

c. Address 305 Crenshaw Boulevard City Torrance Zip 90504

d. UTM: (Give more than one for large and/or linear resources) Zone: 11 ; 377053 mE/ 3745740 mN

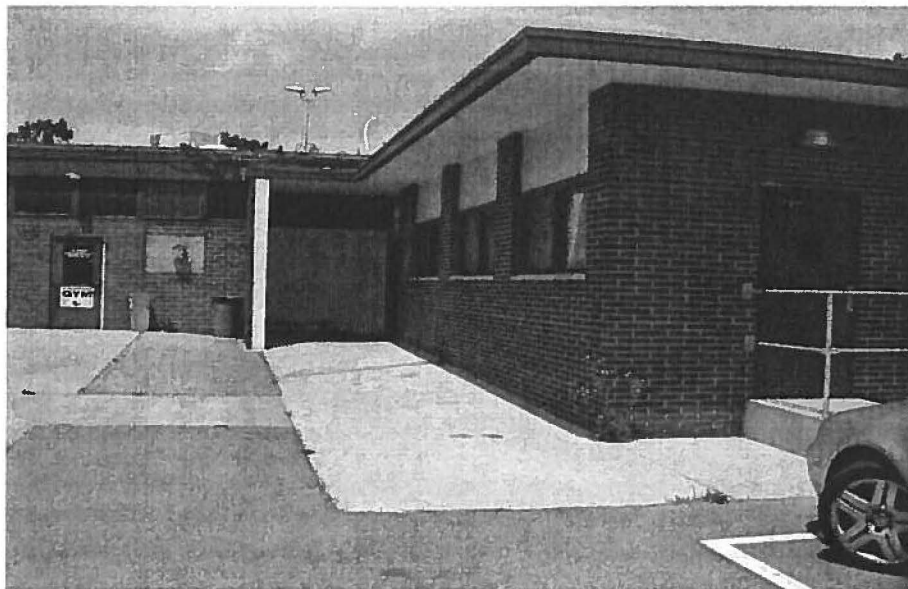
*e. Other Locational Data: (E.g., parcel #, directions to resource, elevation, etc., as appropriate.) Building 111 is located within the Dow Chemical Plant at 305 Crenshaw Boulevard. The plant property is located at the southwest corner of the intersection with Del Amo Boulevard.

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)

Building 111 is the Administration Building for the Dow Chemical Plant, a 52-acre plastics production facility. Building 111 is a 31 ft. by 97 ft. single-story brick building constructed in 1953. The building has a flat roof with a moderate overhang. Entry is gained through single metal doors and both fixed and pivot windows are placed near the roof line. It was originally built for administrative use, and maintains that use today. Building 116, built in 1973, is attached to Building 111, and both are used as office buildings. Building 111 is one of five contributing elements of the Dow Chemical Plant district.

*P3b. Resource Attributes: (See attributes and codes) HP8. Industrial building

*P4. Resources Present: ☐ Building ☐ Structure ☐ Object ☐ Site ☐ District ☒ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo:

(View, date, accession #) View to the North,
5/1/2003, 0H156-MS-3062

***P6. Date Constructed / Age and**

Sources: ☒ Historic

☐ Prehistoric ☐ Both

1953 - Building Permit

***P7. Owner and Address:**

Dow Chemical
305 Crenshaw Blvd.
Torrance, CA 90504

***P8. Recorded by:** (Name, affiliation, and
address) M. Strauss

EDAW, Inc.
3780 Wilshire Blvd, Ste. 250
Los Angeles, CA 90010

***P9. Date Recorded:** 05/01/2003

***P10. Survey Type:** (Describe)
Reconnaissance

***P11. Report Citation:** (Cite Survey report and other sources, or enter "none.")

Dolan, Christy 2003. Historic Resources Evaluation Report, Del Amo Boulevard Extension Project, City of Torrance, Los Angeles County, California. Prepared for the City of Torrance and the U.S. Federal Highway Administration c/o California Department of Transportation, District 12.

*Attachments: ☒ None ☐ Location Map ☐ Sketch Map ☐ Continuation Sheet ☐ Building, Structure, and Object Record
☐ Linear Resource Record ☐ Archaeological Record ☐ District Record ☐ Milling Station Record ☐ Rock Art Record
☐ Artifact Record ☐ Photograph Record ☐ Other (List)

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary #

19-186929

HRI #

Trinomial

NRHP Status Code

Other Listings

Review Code

Reviewer

Date

Page 5 of 8

*Resource Name or #: (Assigned by recorder) Building 112

P1. Other Identifier: Change Room

*P2. Location: ☐ Not for Publication ☒ Unrestricted *a. County Los Angeles

and P2c, P2e, and P2b or P2d. (Attach Location Map as necessary.)

*b. USGS 7.5' Quad Torrance Date 1981 T 4 S; R 14 W : Unsectioned ; San Bernardino B.M.

c. Address 305 Crenshaw Boulevard City Torrance Zip 90504

d. UTM: (Give more than one for large and/or linear resources) Zone: 11 ; 377002 mE/ 3745737 mN

*e. Other Locational Data: (E.g., parcel #, directions to resource, elevation, etc., as appropriate.) Building 112 is located within the Dow Chemical Plant at 305 Crenshaw Boulevard. The plant property is located at the southwest corner of the intersection with Del Amo Boulevard.

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)

Building 112 is the Change Room for the Dow Chemical Plant, a 52-acre plastics production facility. Building 112 is a single-story concrete block building constructed in 1953. The building has a flat roof. Building 112 was originally built as a change room, evidenced by its double metal doors accessed by a concrete loading ramp. Large, double, fixed windows are on several facades. Currently, the building houses administrative offices. Building 112 is one of five contributing elements of the Dow Chemical Plant district.

*P3b. Resource Attributes: (See attributes and codes) HP8. Industrial building

*P4. Resources Present: ☐ Building ☐ Structure ☐ Object ☐ Site ☐ District ☒ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo:

(View, date, accession #) View to the North, 5/1/2003, 0H156-MS-3065

*P6. Date Constructed / Age and

Sources: ☒ Historic

☐ Prehistoric ☐ Both

1953 - Building Permit

*P7. Owner and Address:

Dow Chemical
305 Crenshaw Blvd.
Torrance, CA 90504

*P8. Recorded by: (Name, affiliation, and address) M. Strauss

EDAW, Inc.
3780 Wilshire Blvd, Ste. 250
Los Angeles, CA 90010

*P9. Date Recorded: 05/01/2003

*P10. Survey Type: (Describe)
Reconnaissance

*P11. Report Citation: (Cite Survey report and other sources, or enter "none.")

Dolan, Christy 2003. Historic Resources Evaluation Report, Del Amo Boulevard Extension Project, City of Torrance, Los Angeles County, California. Prepared for the City of Torrance and the U.S. Federal Highway Administration c/o California Department of Transportation, District 12.

*Attachments: ☒ None ☐ Location Map ☐ Sketch Map ☐ Continuation Sheet ☐ Building, Structure, and Object Record
☐ Linear Resource Record ☐ Archaeological Record ☐ District Record ☐ Milling Station Record ☐ Rock Art Record
☐ Artifact Record ☐ Photograph Record ☐ Other (List)

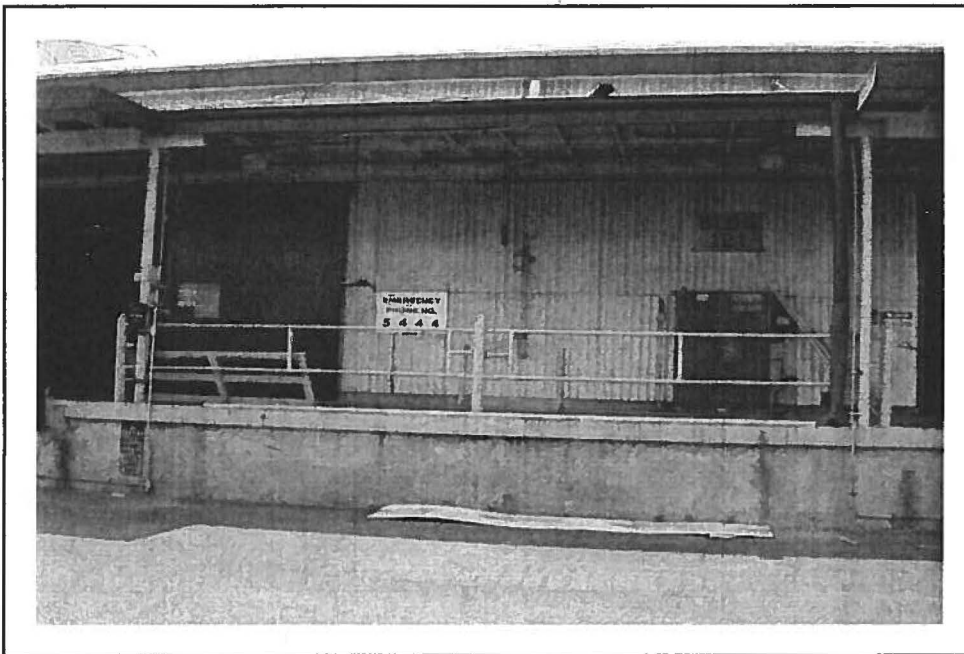
State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code _____
Other Listings _____
Review Code _____ Reviewer _____ Date _____

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*Resource Name or #: (Assigned by recorder) Building 121

- P1. Other Identifier:** Original Warehouse
- *P2. Location:** ☐ Not for Publication ☒ Unrestricted ***a. County** Los Angeles
and P2c, P2e, and P2b or P2d. (Attach Location Map as necessary.)
***b. USGS 7.5' Quad** Torrance **Date** 1981 **T 4 S ; R 14 W** : Unsectioned ; San Bernardino **B.M.**
c. Address 305 Crenshaw Boulevard **City** Torrance **Zip** 90504
d. UTM: (Give more than one for large and/or linear resources) **Zone:** 11 ; 377027 **mE/** 3745620 **mN**
***e. Other Locational Data:** (E.g., parcel #, directions to resource, elevation, etc., as appropriate.) Building 121 is located within the Dow Chemical Plant at 305 Crenshaw Boulevard. The plant property is located at the southwest corner of the intersection with Del Amo Boulevard.
- *P3a. Description:** (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)
Building 121 is an original warehouse for the Dow Chemical Plant, a 52-acre plastics production facility. Building 121 is a 181 ft. by 241 ft. single-story warehouse with a concrete block foundation and corrugated siding. Built in 1953, it has a moderately pitched end gable roof and both sliding and hinged double metal doors. There is a loading patio on the east facade with four loading docks and single sliding doors. Equipment has been added to the exterior over the years, changing the exterior appearance. Building 121 is one of five contributing elements of the Dow Chemical Plant district.
- *P3b. Resource Attributes:** (See attributes and codes) HP8. Industrial building
- *P4. Resources Present:** ☐ Building ☐ Structure ☐ Object ☐ Site ☐ District ☒ Element of District ☐ Other (Isolates, etc.)



- P5b. Description of Photo:**
(View, date, accession #) View to the West,
5/1/2003, 0H156-MS-3075
- *P6. Date Constructed / Age and Sources:** ☒ Historic
☐ Prehistoric ☐ Both
1953 - Building Permit
- *P7. Owner and Address:**
Dow Chemical
305 Crenshaw Blvd.
Torrance, CA 90504
- *P8. Recorded by:** (Name, affiliation, and address) M. Strauss
EDAW, Inc.
3780 Wilshire Blvd, Ste. 250
Los Angeles, CA 90010
- *P9. Date Recorded:** 05/01/2003
- *P10. Survey Type:** (Describe)
Reconnaissance

***P11. Report Citation:** (Cite Survey report and other sources, or enter "none.")
Dolan, Christy 2003. Historic Resources Evaluation Report, Del Amo Boulevard Extension Project, City of Torrance, Los Angeles County, California. Prepared for the City of Torrance and the U.S. Federal Highway Administration c/o California Department of Transportation, District 12.

***Attachments:** ☒ None ☐ Location Map ☐ Sketch Map ☐ Continuation Sheet ☐ Building, Structure, and Object Record
☐ Linear Resource Record ☐ Archaeological Record ☐ District Record ☐ Milling Station Record ☐ Rock Art Record
☐ Artifact Record ☐ Photograph Record ☐ Other (List)

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code _____
Other Listings _____
Review Code _____ Reviewer _____ Date _____

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*Resource Name or #: (Assigned by recorder) Building 121 A&B

- P1. Other Identifier:** Warehouse
- *P2. Location:** ☐ Not for Publication ☒ Unrestricted ***a. County** Los Angeles
and P2c, P2e, and P2b or P2d. (Attach Location Map as necessary.)
***b. USGS 7.5' Quad** Torrance **Date** 1981 **T** 4 S ; **R** 14 W : Unsectioned ; San Bernardino **B.M.**
c. Address 305 Crenshaw Boulevard **City** Torrance **Zip** 90504
d. UTM: (Give more than one for large and/or linear resources) **Zone:** 11 ; 376941 mE/ 3745625 mN
***e. Other Locational Data:** (E.g., parcel #, directions to resource, elevation, etc., as appropriate.) Building 121 A and 121B are located within the Dow Chemical Plant at 305 Crenshaw Boulevard. The plant property is located at the southwest corner of the intersection with Del Amo Boulevard.
- *P3a. Description:** (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)
Building 121A and 121B are warehouses for the Dow Chemical Plant, a 52-acre plastics production facility. Building 121A and 121B are single-story adjoining warehouses built in 1956. They have moderately pitched, gable roofs. Access is gained through large sliding metal doors at one end of the buildings and a double hinged metal door at the other. Metal rung ladders allow access to a system of conveyor belts that weave around the buildings. Building 121A and 121B is one of five contributing elements of the Dow Chemical Plant district.
- *P3b. Resource Attributes:** (See attributes and codes) HP8. Industrial building
- *P4. Resources Present:** ☐ Building ☐ Structure ☐ Object ☐ Site ☐ District ☒ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo:
(View, date, accession #) View to the North,
5/1/2003, 0H156-MS-3087

***P6. Date Constructed / Age and Sources:** ☒ Historic
☐ Prehistoric ☐ Both
1956 - Building Permit

***P7. Owner and Address:**
Dow Chemical
305 Crenshaw Blvd.
Torrance, CA 90504

***P8. Recorded by:** (Name, affiliation, and address) M. Strauss
EDAW, Inc.
3780 Wilshire Blvd, Ste. 250
Los Angeles, CA 90010

***P9. Date Recorded:** 05/01/2003

***P10. Survey Type:** (Describe)
Reconnaissance

***P11. Report Citation:** (Cite Survey report and other sources, or enter "none.")
Dolan, Christy 2003. Historic Resources Evaluation Report, Del Amo Boulevard Extension Project, City of Torrance, Los Angeles County, California. Prepared for the City of Torrance and the U.S. Federal Highway Administration c/o California Department of Transportation, District 12.

***Attachments:** ☒ None ☐ Location Map ☐ Sketch Map ☐ Continuation Sheet ☐ Building, Structure, and Object Record
☐ Linear Resource Record ☐ Archaeological Record ☐ District Record ☐ Milling Station Record ☐ Rock Art Record
☐ Artifact Record ☐ Photograph Record ☐ Other (List)

State of California — The Resources Agency
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19-186929
Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code _____
Other Listings _____
Review Code _____ Reviewer _____ Date _____

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*Resource Name or #: (Assigned by recorder) Three Tanks

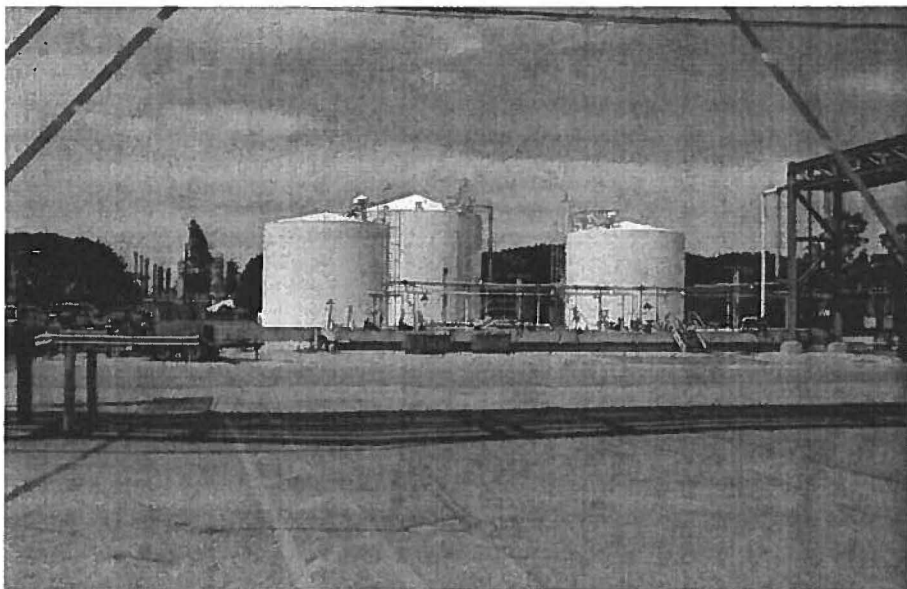
P1. Other Identifier:

- *P2. Location: ☐ Not for Publication ☒ Unrestricted *a. County Los Angeles
and P2c, P2e, and P2b or P2d. (Attach Location Map as necessary.)
*b. USGS 7.5' Quad Torrance Date 1981 T 4 S ; R 14 W : Unsectioned ; San Bernardino B.M.
c. Address 305 Crenshaw Boulevard City Torrance Zip 90503
d. UTM: (Give more than one for large and/or linear resources) Zone: 11 ; north tank 376939 mE/ 3745750 mN; east tank 376950 mE/ 3745728 mN; west tank 376927 mE/ 3745728 mN
*e. Other Locational Data: (E.g., parcel #, directions to resource, elevation, etc., as appropriate.) The three tanks are located within the Dow Chemical Plant at 305 Crenshaw Boulevard. The plant property is located at the southwest corner of the intersection with Del Amo Boulevard.

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)
The three tanks are steel holding tanks for the Dow Chemical Plant, a 52-acre plastics production facility. The three tanks consist of round metal oil storage tanks approximately 50 ft. in diameter. Unlike more recent tanks, where the tops are rounded, the tops of these tanks are the shape of an inverted cone. Each tank is accessed by a metal rung ladder. The three tanks are one of five contributing elements of the Dow Chemical Plant district.

*P3b. Resource Attributes: (See attributes and codes) HP11. Engineering structure

*P4. Resources Present: ☐ Building ☐ Structure ☐ Object ☐ Site ☐ District ☒ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo:
(View, date, accession #) View to the North,
5/1/2003, 0H156-MS-3091

*P6. Date Constructed / Age and

Sources: ☒ Historic

☐ Prehistoric ☐ Both

1953 approximate

*P7. Owner and Address:

Dow Chemical
305 Crenshaw Blvd.
Torrance, CA 90504

*P8. Recorded by: (Name, affiliation, and
address) M. Strauss

EDAW, Inc.
3780 Wilshire Blvd, Ste. 250
Los Angeles, CA 90010

*P9. Date Recorded: 05/01/2003

*P10. Survey Type: (Describe)
Reconnaissance

*P11. Report Citation: (Cite Survey report and other sources, or enter "none.")

Dolan, Christy 2003. Historic Resources Evaluation Report, Del Amo Boulevard Extension Project, City of Torrance, Los Angeles County, California. Prepared for the City of Torrance and the U.S. Federal Highway Administration c/o California Department of Transportation, District 12.

*Attachments: ☒ None ☐ Location Map ☐ Sketch Map ☐ Continuation Sheet ☐ Building, Structure, and Object Record
☐ Linear Resource Record ☐ Archaeological Record ☐ District Record ☐ Milling Station Record ☐ Rock Art Record
☐ Artifact Record ☐ Photograph Record ☐ Other (List)

*Required Information

P1. Other Identifier: Parcel 7352-002-017

***P2 e. Other Locational Data:** The Dow Chemical Plant is located at 305 Crenshaw Boulevard, at the southwest corner of the intersection with Del Amo Boulevard.

***P3a. Description:**

The 52-acre Dow Chemical Plant (**Photograph 1**) was previously recorded as a district including numerous industrial buildings, structures and heavy equipment. The district was identified in 2003 with five contributing elements. Building 111, the Administration Building, is a one-story brick building with a flat roof and pivot windows. Building 112, the Change Room, is a one-story concrete block building with a flat roof and double pane fixed windows. Building 121 is a warehouse with corrugated metal siding, a side gable roof, several loading docks and utility doors. Building 121A and 121B consists of one-story adjoining warehouses with metal siding, gabled roofs and large sliding utility doors. Finally, three steel holding tanks are identified in the district.

The resource was revisited in the current study and photographs were taken from the public right-of-way. A review of aerial photographs from 2003 to 2019 indicate the property has undergone some alteration (Historic Aerials 2020). The northern portion of Building 121 has been removed and two new tanks were installed to the north of the building. In addition, the three steel holding tanks previously identified within the district have been demolished.

***P3b. Resource Attributes:** HP8 – Industrial building; HP11 – Engineering Structure

P5a. Photograph:



Photograph 1. Dow Chemical Plant, 305 Crenshaw Boulevard, Torrance, November 5, 2020.

***P8. Recorded by:** M. Wilson, AECOM, 401 West A Street, Suite 1200, San Diego, CA 92101

***P9. Date Recorded:** November 2020

***P10. Survey Type:** Reconnaissance

***P11. Report Citation:** *Green Line Extension to Torrance Draft Environmental Impact Report: Cultural Resources Technical Study*, Prepared by AECOM for Los Angeles County Metropolitan Transportation Authority, 2020.

UPDATE SHEET

Primary # P-19-186929

HRI # _____

Trinomial _____

NRHP Status Code _____

Page 2 of 2

*Resource Name or #: (Assigned by recorder) Dow Chemical Plant

☐ Continuation ☒ Update

*B10. Significance: Theme Industrial Development

Area Southern California

Period of Significance Post World War II era 1945-1960 Property Type Industrial Applicable Criteria n/a

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope.

Also address integrity.)

M. Strauss of Edaw Inc. inventoried and evaluated this property in 2003 for the report titled *Historic Resources Evaluation Report, Del Amo Boulevard Extension Project, City of Torrance, Los Angeles County, California*. Prepared for the City of Torrance and the U.S. Federal Highway Administration c/o California Department of Transportation, District 12.

The previous evaluation concluded that the Dow Chemical Plant, built beginning in 1952, was not significant and that it did not retain sufficient integrity to be eligible for the NRHP. The resource was revisited in the current study and identified several alterations to the property that have further diminished its historic integrity. In addition, the property has not acquired additional historical significance to make it eligible under any National Register of Historic Places (NRHP) or California Register of Historical Resources (CRHR) criteria.

The 2003 evaluation concluded that the property was not eligible for the NRHP. After review of the previous recordation and current field check and research, the present evaluation concludes that the property does not appear to meet the criteria for listing in the NRHP or the CRHR, nor does it appear to be an historical resource for purposes of the California Environmental Quality Act (CEQA).

*B14. Evaluator: M. Wilson, AECOM

*Date of Evaluation: November 2020

*B12. References:

Historic Aerials

2020 Aerial Photograph database, <https://www.historicaerials.com/>, accessed November 5, 2020.

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code _____
Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 3

*Resource Name or #: (Assigned by recorder) BNSF Harbor Subdivision

P1. Other Identifier:

- *P2. Location:** ☐ Not for Publication ☒ Unrestricted ***a. County** Los Angeles
and P2c, P2e, and P2b or P2d. (Attach Location Map as necessary.)
***b. USGS 7.5' Quad** Torrance, Calif. **Date** 1981 T 4S ; R 14W : Unsectioned; SB B.M.
c. Address _____ **City** _____ **Zip** _____
d. UTM: (Give more than one for large and/or linear resources) **Zone:** 11; A: 376230 mE/3745932 mN; B: 376367 mE/ 3745798 mN
***e. Other Locational Data:** (E.g., parcel #, directions to resource, elevation, etc., as appropriate.) The BNSF Harbor Subdivision Railroad is located northwest of downtown Torrance between Crenshaw Boulevard to the east and Madrona Avenue to the west. Located in an industrial area, this short segment of the railroad is bounded by the Mobile Oil Refinery to the northeast and the Dow Chemical Plant to the southeast.

- *P3a. Description:** (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)
This is a 190 foot segment of the Harbor Subdivision of the Burlington Northern Santa Fe (BNSF) established August 31, 1924 (Serpico 1988:151). The 12.5-mile main branch line runs northwest to southeast.

- *P3b. Resource Attributes:** (See attributes and codes) HP39. Railroad

- *P4. Resources Present:** ☐ Building ☒ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo:

(View, date, accession #) View to the West,
5/1/2003, 0H156-MS-3098

***P6. Date Constructed / Age and**

Sources: ☒ Historic
☐ Prehistoric ☐ Both
1924 factual

***P7. Owner and Address:**

Burlington Northern Santa Fe Corp.
P.O. Box 961057
Fort Worth, Texas 76161-0057

***P8. Recorded by:** (Name, affiliation, and address) M. Strauss

EDAW, Inc.
3780 Wilshire Blvd, Ste. 250
Los Angeles, CA 90010

***P9. Date Recorded:** 05/01/2003

***P10. Survey Type:** (Describe)
Reconnaissance

- *P11. Report Citation:** (Cite Survey report and other sources, or enter "none.") Dolan, Christy 2003. Historic Resources Evaluation Report, Del Amo Boulevard Extension Project, City of Torrance, Los Angeles County, California. Prepared for the City of Torrance and the U.S. Federal Highway Administration c/o California Department of Transportation, District 12.

- *Attachments: ☐ None ☒ Location Map ☐ Sketch Map ☐ Continuation Sheet ☒ Building, Structure, and Object Record
☐ Linear Resource Record ☐ Archaeological Record ☐ District Record ☐ Milling Station Record ☐ Rock Art Record
☐ Artifact Record ☐ Photograph Record ☐ Other (List)

LOCATION MAP

Primary #

19-186930

HRI #

Trinomial

Page 2 of 3

* Resource Name or # (Assigned by recorder) BNSF Harbor Subdivision

* Map Name: Torrance, Calif. 7.5' Quadrangle

* Scale: 1:24,000

* Date of Map: 1981



State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION

Primary # _____
HRI # _____

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 3 of 3

*NRHP Status Code

*Resource Name or # (Assigned by recorder) BNSF Harbor Subdivision

B1. **Historic Name:** Santa Fe and Los Angeles Harbor Railway / Atchison, Topeka & Santa Fe Railroad, San Pedro Branch
B2. **Common Name:** Burlington Northern Santa Fe (BNSF), Harbor Subdivision

B3. **Original Use:** railroad

B4. **Present Use:** railroad

*B5. **Architectural Style:**

*B6. **Construction History:** (Construction date, alterations, and date of alternations.)

The railway was established through Torrance on August 31, 1924.

*B7. **Moved?** ☒ No ☐ Yes ☐ Unknown **Date:**

Original Location:

*B8. **Related Features:**

B9a. **Architect:** unknown

B9b. **Builder:** unknown

*B10. **Significance: Theme** Industrial Development **Area** Southern California

Period of Significance 1920 - 1940

Property Type railroad

Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

In the early 1900s, serious development began on the San Pedro/Wilmington ports. In April 1922, the Santa Fe and Los Angeles Harbor (SF&LAH) Railway Company was incorporated to connect the Santa Fe Railway at El Segundo to the city of Wilmington. After years of being denied access to the Los Angeles ports, as they were territory of the Southern Pacific, Pacific Electric, and Los Angeles and Salt Lake Railways, the Santa Fe Railway built an extension toward the port area. Starting at El Segundo, construction of the new line began in June 1923 and was completed to Wilmington by the fall of 1924. The railway was leased to the Atchison, Topeka & Santa Fe in 1924, and in 1942, the line was acquired outright by the Santa Fe Railway. In 1995, the Santa Fe Pacific Corporation merged with Burlington Northern, Incorporated, forming the BNSF Railway. The 12.5-mile main branch line is now known as the Harbor Subdivision of the BNSF.

The railroad is a typical standard gauge that is currently under heavy use. Although it has been upgraded several times over the years, several of the railroad ties and tie plates are rusted. The area around Torrance has a network of railroads that serve the many industrial uses. The railroad that crosses the project APE was built long after the first railroads arrived in the area in the 1880s. It is a relatively small branch and does not appear to be associated with events or persons significant in our history. Further study of this railroad is unlikely to yield information important to our understanding of history. This property does not appear to be eligible for inclusion on the National Register.

B11. **Additional Resource Attributes:** (List attributes and codes):

*B12. **References:**

Infoplease 2002 "Burlington Northern Santa Fe Railway." The Columbia Electronic Encyclopedia. 21 April 2003

<<<http://www.infoplease.com/ce6/us/A0809523.html>>>

Serpico, Philip C. 1988 *Santa Fe Route to the Pacific*. Omni Publications, Palmdale, California.

Shanahan, Dennis F. and Charles Elliott, Jr. 1984 *Historic Torrance: A Pictorial History of Torrance, California*. Legends Press, Redondo Beach, California.

B13. **Remarks:**

*B14. **Evaluator:** C. Dolan

*Date of Evaluation: 05/06/2003

(Sketch Map with north arrow required.)

See attached Location Map.

(This space reserved for official comments.)

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
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Other Listings _____
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Page 1 of 3

*Resource Name or #: (Assigned by recorder) Alcoa Spur

P1. Other Identifier:

- *P2. Location: ☐ Not for Publication ☒ Unrestricted *a. County Los Angeles
and P2c, P2e, and P2b or P2d. (Attach Location Map as necessary.)
*b. USGS 7.5' Quad Torrance, Calif. Date 1981 T 4S ; R 14W : Unsectioned; SB B.M.
c. Address _____ City _____ Zip _____
d. UTM: (Give more than one for large and/or linear resources) Zone: 11; A: 376345 mE/3745823 mN; B: 377152 mE/ 3745822 mN
*e. Other Locational Data: (E.g., parcel #, directions to resource, elevation, etc., as appropriate.)
The Alcoa Spur is located northwest of downtown Torrance between Crenshaw Boulevard to the east and Madrona Avenue to the west. Located in an industrial area, this segment of the railroad is bounded by the Mobile Oil Refinery to the north and the Dow Chemical Plant to the south.
- *P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)
This is a 0.5 mile segment of the Alcoa spur line established on July 5, 1942 (Serpico 1988:151). Named for the Aluminum Company of America, this spur line extends 2 miles to the east off of the Harbor Subdivision of the Burlington Northern Santa Fe Railroad.
- *P3b. Resource Attributes: (See attributes and codes) HP39. Railroad
- *P4. Resources Present: ☐ Building ☒ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo:
(View, date, accession #) View to East,
5/1/2003, 0H156-MS-3106

*P6. Date Constructed / Age and Sources: ☒ Historic
☐ Prehistoric ☐ Both
1942 factual

*P7. Owner and Address:
Mobil Oil Corp.
P.O. Box 4973
Houston, Texas 77210

*P8. Recorded by: (Name, affiliation, and address) M. Strauss
EDAW, Inc.
3780 Wilshire Blvd, Ste. 250
Los Angeles, CA 90010

*P9. Date Recorded: 05/01/2003

*P10. Survey Type: (Describe)
Reconnaissance

*P11. Report Citation: (Cite Survey report and other sources, or enter "none.") Dolan, Christy 2003. Historic Resources Evaluation Report, Del Amo Boulevard Extension Project, City of Torrance, Los Angeles County, California. Prepared for the City of Torrance and the U.S. Federal Highway Administration c/o California Department of Transportation, District 12.

*Attachments: ☐ None ☒ Location Map ☐ Sketch Map ☐ Continuation Sheet ☒ Building, Structure, and Object Record
☐ Linear Resource Record ☐ Archaeological Record ☐ District Record ☐ Milling Station Record ☐ Rock Art Record
☐ Artifact Record ☐ Photograph Record ☐ Other (List)

State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
LOCATION MAP

Primary #

19-186931

HRI #

Trinomial

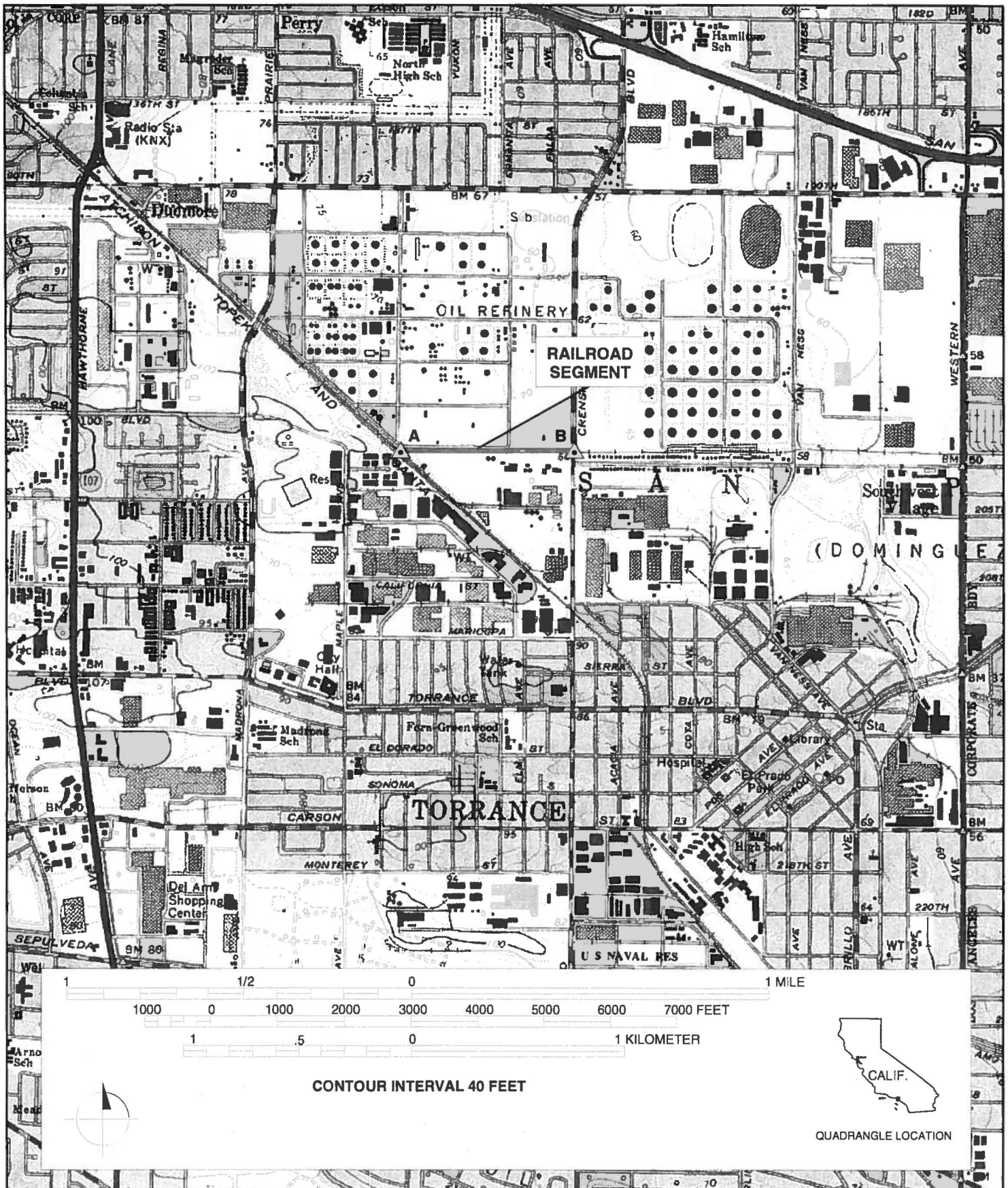
Page 2 of 3

* Resource Name or # (Assigned by recorder) **Alcoa Spur**

* Map Name: **Torrance, Calif. 7.5' Quadrangle**

* Scale: **1:24,000**

* Date of Map: **1981**



State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION

Primary # _____

HRI # _____

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 3 of 3

*NRHP Status Code 6Z

*Resource Name or # (Assigned by recorder) Alcoa Spur

B1. Historic Name: Alcoa spur line

B2. Common Name:

B3. Original Use: industrial railroad spur

B4. Present Use: industrial railroad spur

*B5. Architectural Style:

*B6. Construction History: (Construction date, alterations, and date of alterations.)
The Alcoa spur line was established on July 5, 1942.

*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date: Original Location:

*B8. Related Features:

B9a. Architect: unknown

B9b. Builder: unknown

*B10. Significance: Theme Industrial Development Area Southern California
Period of Significance World War II Property Type railroad Applicable Criteria N/A
(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)
Named for the Aluminum Company of America, the Alcoa spur line extends 2 miles to the east off of the Harbor Subdivision of the BNSF. At the time it was built, the Alcoa Station sat at the intersection, among several oil refineries. In 1988, this spur connected to the International Light Metals Company and the McDonnell Douglas Plant. At its end point, the Alcoa spur line can connect with the Southern Pacific Railway by way of industrial interchange trackage.

The Alcoa Railroad spur was established during World War II and has continued to be used as part of the interconnected rail lines throughout this part of Los Angeles County. It is a typical railroad and is not associated with any events or people important in our history. Further study of this railroad is unlikely to yield information important to our understanding of history. This property does not appear to be eligible for inclusion on the National Register.

B11. Additional Resource Attributes: (List attributes and codes):

*B12. References:

Serpico, Philip C. 1988 *Santa Fe Route to the Pacific*. Omni Publications, Palmdale, California.
Shanahan, Dennis F. and Charles Elliott, Jr. 1984 *Historic Torrance: A Pictorial History of Torrance, California*. Legends Press, Redondo Beach, California.

B13. Remarks:

*B14. Evaluator: C. Dolan

*Date of Evaluation: 05/06/2003

(Sketch Map with north arrow required.)

See attached Location Map.

(This space reserved for official comments.)

UPDATE SHEET

Primary # P-19-186931

HRI # _____

Trinomial _____

NRHP Status Code _____

Page 1 of 2

*Resource Name or #: (Assigned by recorder) Alcoa Spur

☐ Continuation ☒ Update

P1. Other Identifier:

***P2 e. Other Locational Data:** The Alcoa Spur is located northwest of downtown Torrance between Crenshaw Boulevard to the east and Madrona Avenue to the west. Located in an industrial area, this segment of the railroad is bounded by the Mobile Oil Refinery to the north and the Dow Chemical Plant to the south.

***P3a. Description:**

The Alcoa Spur (P-19-186931) was previously recorded in 2003 as a one-half-mile-long segment of a two-mile-long railroad spur off the BNSF Harbor Subdivision (**Photographs 1-2**). The Alcoa Spur was also connected with the Southern Pacific Railway via a large network of industrial interchanges. Although portions of the spur in the APE are still in use, the tracks that extend east beyond Crenshaw Avenue are disused and, in some locations, have been removed.

Established in 1942 to meet the rapid development of the military industrial complex in Torrance aligned along Del Amo Boulevard, the spur was named for the Aluminum Company of America (Alcoa). Originally, the Alcoa Station was located at the intersection of the spur and Del Amo Boulevard. The previous evaluation found the Alcoa Spur to be not eligible for the NRHP. The segment of the Alcoa Spur was revisited as part of the current study. It appeared unaltered and consistent with the previous evaluation. It has not acquired additional significance and, therefore, does not appear eligible for the NRHP or CRHR.

***P3b. Resource Attributes:** HP39. Railroad

P5a. Photograph:



Photograph 1. View of Alcoa Spur Tracks from W Del Amo Boulevard, view to north, November 5, 2020.

***P8. Recorded by:** M. Wilson, AECOM, 401 West A Street, Suite 1200, San Diego, CA 92101

***P9. Date Recorded:** November 2020

***P10. Survey Type:** Reconnaissance

***P11. Report Citation:** *Green Line Extension to Torrance Draft Environmental Impact Report: Cultural Resources Technical Study*, Prepared by AECOM for Los Angeles County Metropolitan Transportation Authority, 2020.

UPDATE SHEET

Primary # P-19-186931

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NRHP Status Code _____

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*Resource Name or #: (Assigned by recorder) Alcoa Spur

☐ Continuation ☒ Update

*B10. Significance: Theme Industrial Development

Area Southern California

Period of Significance World War II Property Type Railroad Applicable Criteria n/a

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope.
Also address integrity.)

M. Strauss of Edaw Inc. inventoried and evaluated this property in 2003 for the report titled *Historic Resources Evaluation Report, Del Amo Boulevard Extension Project, City of Torrance, Los Angeles County, California*. Prepared for the City of Torrance and the U.S. Federal Highway Administration c/o California Department of Transportation, District 12.

The previous evaluation concluded that the property was not eligible for the National Register of Historic Places (NRHP). After review of the previous recordation and current field check and research, the present evaluation concludes that the building does not appear to meet the criteria for listing in the NRHP or the California Register of Historical Resources (CRHR), nor does it appear to be an historical resource for purposes of the California Environmental Quality Act (CEQA).

*B14. Evaluator: M. Wilson, AECOM

*Date of Evaluation: November 2020

P5a. Photographs (Continued):



Photograph 2. The Alcoa Spur within the APE

State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
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Page 1 of 6*Resource Name or #: (Assigned by recorder) 3700 W. 190th Street – Administration Building

P1. Other Identifier:

*P2. Location: ☐ Not for Publication ☒ Unrestricted*a. County: Los Angeles and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)*b. USGS 7.5' Quad Torrance Date 1981 T 4S; R 14W; N/A ¼ of N/A ¼ of Sec 3; B.M. SBc. Address 3700 W. 190th Street City Torrance Zip 92376d. UTM: (Give more than one for large and/or linear resources) Zone 11, 376303 mE/ 3747150 mN

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, etc., as appropriate)

APN: 7352-002-029. Located on the south side of W. 190th Street, inside the main entrance to the ExxonMobil Torrance Refinery, second building on the east side of the driveway.

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

The Administration Building at 3700 W. 190th Street is located inside the main entrance of the ExxonMobil Torrance Refinery in Torrance, California. The large parcel on which the building is located contains the ExxonMobil Torrance Refinery, an industrial facility that contains numerous storage tanks and other refinery equipment as well as office buildings and parking lots. The refinery extends more than a half-mile south, east, and west from the subject building. The area to the north of the subject building is characterized by light industrial and service uses on the north side of W. 190th Street (a five to six-lane major arterial road), with mid-twentieth century single-family residential subdivisions, a high school, and Columbia Park beyond.

SEE CONTINUATION SHEET

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)

P3b. Resource Attributes: (List attributes and codes) Industrial Building – HP8*P4. Resources Present: ☒ Building☐ Structure ☐ Object ☐ Site ☐ District☐ Element of District☐ Other (Isolates, etc.)

P5b. Description of Photo: (view, date, accession

#) View to the North, March 2011

*P6. Date Constructed/Age and Source:

☒ Historic ☐ Prehistoric ☐ Both1952 (per 1952 copyrighted aerial imagery, NETR his toricaerials.com)

*P7. Owner and Address:

Private

*P8. Recorded by: (Name, affiliation, and address)

URS Corp.4225 Executive Square, Suite 1600San Diego, CA, 92108-4314*P9. Date Recorded: March 2011*P10. Survey Type: (Describe) Reconnaissance Survey

*P11. Report Citation:

(Cite survey report and other sources, or enter "none.") Verizon Cellular Communications Tower Site – Exxon Mobil IBR, 3700 W. 190th Street (APN: 7352-002-029) Torrance, CA 90504, March 2011.

*Attachments: ☐ NONE ☐ Location Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record ☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record ☐ Artifact Record ☐ Photograph Record ☐ Other (List):

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DEPARTMENT OF PARKS AND RECREATION
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*Resource Name or # (Assigned by recorder) 3700 W. 190th Street – Administration Building

*Recorded by: URS Corp

*Date March 2011

X Continuation Update

***P3a. (Continued)**

The subject property is the administration building of the Exxon Mobile Torrance Refinery, constructed in circa 1952, which is a mid-century-modern style office building. The two-story building has an irregular L-shaped plan and west-facing orientation. The low-pitch, cross-gable roof is covered with asphalt shingles and features deep boxed eaves. The walls are clad with narrow bricks set in a standard running bond. There are numerous windows on all the elevations and stories, which are uniform in materials and design: two-by-four metal-frame casement type. The windows do not have ledges. The windows are distributed evenly and regularly on all elevations. The main entry is at the center of the primary (west) façade. The recessed entry is filled with a large window wall with metal mullions and contains a double, aluminum frame, standard commercial style door. The door is reached by a broad set of brick steps and entry with a cantilevered flat concrete roof above. All of the building materials appear to be original, and no major additions or alterations were observed.

The area has a flat topography and the surrounding roads are set in a regular grid. Due to development, the subject parcel and the area surrounding has undergone extensive ground disturbance and is covered with pavement, streetscape improvements, and landscaping.

***B12. (Continued)**

City of Torrance. 2009. General Plan. <http://www.ci.torrance.ca.us/13530.htm>. Accessed January 25, 2011.

Exxon Mobil. N.d. (a). "History." http://www.exxonmobil.com/NA-English/PA/about_where_ref_torrance_history.aspx. Accessed January 25, 2011.

Exxon Mobil. N.d. (b). "About." http://www.exxonmobil.com/NA-English/PA/about_where_ref_torrance.aspx. Accessed January 25, 2011.

Los Angeles Times. Feb 12, 1928. Oct. 8, 1928. Nov. 6, 1928. Dec. 17, 1928. Dec. 31, 1928. Sep. 23, 1932. Oct. 30, 1932. Mar. 14, 1935. Dec. 26, 1937. Mar. 23, 1965. Mar. 28, 1965. Dec. 11, 1977.

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DEPARTMENT OF PARKS AND RECREATION
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*Resource Name or # (Assigned by recorder) 3700 W. 190th Street – Administration Building

*Recorded by: URS Corp

*Date March 2011

X Continuation Update

*P3a. (Continued)



Facing East



Facing South



Front of Torrance Refinery, ExxonMobil website, date unknown



Facing South

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BUILDING, STRUCTURE, AND OBJECT RECORD

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*NRHP Status Code 6Z

*Resource Name or # (Assigned by recorder) 3700 W. 190th Street – Administration Building

B1. Historic Name: Mobil Torrance Refinery Administration Building

B2. Common Name: ExxonMobil Torrance Refinery Administration Building

B3. Original Use: Office Building to support Industrial Facility

B4. Present Use: Office Building to support Industrial Facility

*B5. Architectural Style: Mid-Century Modern

*B6. Construction History: (Construction date, alterations, and date of alterations)

The Administration Building at 3700 W. 190th Street was constructed in 1952 (per 1952 copyrighted aerial imagery, NETR historicaerials) by the Mobil Oil Company. Based on a review of copyrighted historic aerials available at historicaerials.com (NETR Online), for the years 1952, 1954, 1972, 1980, 2003, 2004, and 2005, the building was under construction in 1952, and by 1954 had been completed with surrounding landscaping. Based on observation and review of the historic aerials, there have been no additions or major alterations since it was constructed. All building materials appear to be original.

*B7. Moved? X No Yes Unknown Date: Original Location:

*B8. Related Features:

Related features include low brick planters alongside the primary (west) façade and a parking lot that wraps around the north, south, and east elevations.

B9a. Architect: Unknown b. Builder: Unknown

*B10. Significance: Theme N/A Area Torrance, CA

Period of Significance N/A

Property Type Office Building to support Industrial Facility

Applicable Criteria N/A (Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The historical significance of the Administration Building at 3700 W. 190th Street was determined by applying the procedure and criteria for the National Register of Historic Places (NRHP).

Based on site investigations and historic research, Administration Building at 3700 W. 190th Street does not appear to possess the requisite significance to be eligible for listing on the NRHP.

SEE CONTINUATION SHEET

B11. Additional Resource Attributes: (List attributes and codes)

*B12. References:

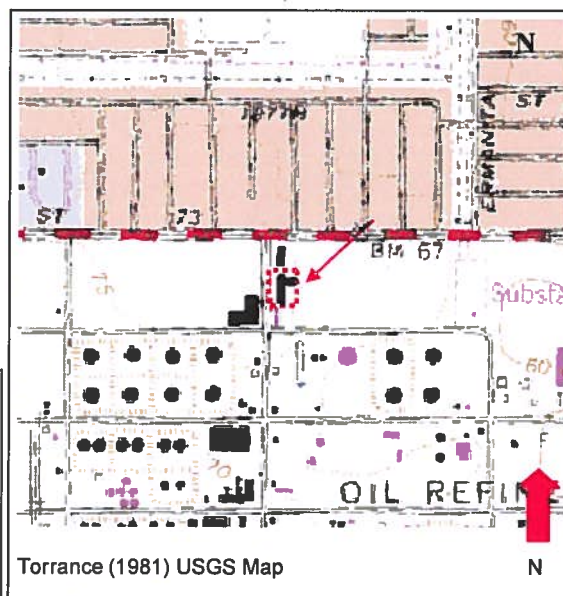
SEE CONTINUATION SHEET

B13. Remarks: None

*B14. Evaluator: URS Corporation

*Date of Evaluation: March 2011

(This space reserved for official comments.)



Torrance (1981) USGS Map

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*Recorded by: URS Corp *Date March 2011 X Continuation Update

***B10. (Continued)**

On May 31, 1911, the Dominguez Estate Company sold 2,791 acres of the rancho to Pasadena developer Jared Sidney Torrance. Torrance paid \$976,850 for the property. Susana Dominguez Del Amo sold an additional 730 acres to Torrance for \$350 per acre. The land was subdivided and founded as Torrance in 1912. Jared Sidney Torrance (1852- 1921), a real estate developer and vice president of Union Oil Company, envisioned a planned residential and industrial community (City of Torrance General Plan 2010).

Only a few years after the City's incorporation in 1921, petroleum was discovered in Torrance, and the discovery transformed the City from a small town to a major industrial area. Industry in Torrance continued to shape its development as a major employment center in the region. In the early 1900s, Torrance was an important hub site of the Pacific Electric Railway, also known as the Red Car system. The ease of access into the City by the Red Cars provided the impetus for industrial development along those lines. Today, Torrance continues to be a preferred location for companies focused on moving people, with two of the three largest Japanese auto makers – Toyota Motor Sales, U.S.A. and American Honda Motor Company – locating their U.S. headquarters here. California's aerospace industry began in Torrance and surrounding communities, and has played an important role in City history as well. As a major oil-producing region, Torrance was once dotted with thousands of oil wells and oil derricks. The ExxonMobil refinery in the north end of the City, established in 1929, is still responsible for much of Southern California's gasoline supply (City of Torrance General Plan 2010).

In late 1928, General Petroleum Corporation began construction of the Torrance Refinery. The refinery was situated on a 950-acre tract that the General Petroleum Corporation had purchased two years earlier for the purpose. The construction included two crude-distillation units with a combined capacity of 30,000 barrels per day together with the necessary auxiliary tanks, boilers, finishing apparatus, pump-houses, and office buildings. The new refinery, finished in early 1929, was to supplement the refineries at Vernon, Olinda, and Lebec, ultimately taking the place of the plant at Vernon (*Los Angeles Times*, Feb 12, 1928, Oct. 8, 1928, Nov. 6, 1928). The Torrance Chamber of Commerce used the plant's construction as an opportunity to promote development in the city, advertising an expected influx of 10,000 people (based on the 3,500 men that were to be on the refinery's payroll) and the need for buildings and investors to construct single-family homes (*Los Angeles Times*, Dec. 17, 1928, Dec. 31, 1928).

During the Great Depression, the refinery processed up to 30,000 barrels of crude oil daily lending a much-needed boost to the local economy (ExxonMobil N.d.[b]). In 1932, General Petroleum announced that they would be expanding the refinery by installing more machinery, pumps and cooling system, and additional pipe lines (*Los Angeles Times*, Sep. 23, 1932). It included expenditures for auxiliary tanks, pipe lines, pumps, steam boilers, and water treating and cooling systems (*Los Angeles Times* Oct. 30, 1932).

In 1935, General Petroleum announced that it would construct a direct pipe line from its production lands in the San Joaquin Valley to the refinery at Torrance. Up until that point, oil had arrived at the refinery by a more circuitous route established in 1912 by General Petroleum, but the new line would run directly from the refinery and pumping station at Lebec ninety miles to the Torrance refinery, at a rate of 40,000 barrels (top capacity of 50,000 barrels) per day without the use of any intermediate pumping stations or electric boosting stations (*Los Angeles Times*, Mar. 14, 1935). In 1937, General Petroleum invested another \$27 million toward further expansions of the refinery (*Los Angeles Times* Dec. 26, 1937). During World War II, the refinery refocused its energies on producing high-octane aviation fuel, helping American aviators to continually push the performance envelope (ExxonMobil N.d.[a]).

In 1965, the refinery, by that time owned by Mobil Oil Company, announced that it would begin work on an "ultra-modern computerized plant" to replace the 1929 refinery. The \$80 million project was heralded as "the finest, most modern refinery in the world utilizing computers and other electronic controls," requiring less staff to operate than the older facility. The new facility would have the capacity to refine 100,000 barrels per day (about the same as the older refinery by that time), but would produce higher grade products and an increased storage capacity of 600,000 barrels. The new refinery replaced the older refinery (*Los Angeles Times*, Mar. 23, 1965, Mar. 28, 1965).

By the 1970s, the refinery was being increasingly criticized for its environmental impact on the surrounding community, and there were numerous fires and explosions that resulted in fatalities and release of dangerous toxic gases. In an effort to provide a more attractive image of the refinery to the community, the refinery instituted a grand landscaping plan, planting 55 full-grown olive trees, 40,000 square feet of grass, and replacing chain link security fence with one interwoven with redwood strips. Keep America Beautiful Inc. awarded the refinery a

SEE CONTINUATION SHEET

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*Recorded by: URS Corp *Date March 2011 X Continuation Update

***B10. (Continued)**

distinguished service citation for community programs for having "demonstrated how a plan can blend, in a visually attractive way, into the neighboring community" (*Los Angeles Times*, Dec. 11, 1977).

Today, the Refinery's crude oil supply still comes primary from the San Joaquin Valley, and it is refined into gasoline, jet fuel, diesel fuel, liquified petroleum gases, coke, and sulfur. The ExxonMobil Refinery now employs nearly 800 people, covers an area of 750 acres, processes an average of 150,000 barrels of crude oil per day, and produces nearly ten percent of the gasoline that is refined in California (ExxonMobil N.d.[b]).

Upon review of the site survey and historical research, the subject property does not appear to meet the criteria of eligibility for inclusion on the NRHP. Initial research indicated the property does not appear to be associated with significant events in the United States, California, or Los Angeles County (Criterion A). Although the 1952 Administration Building has been historically used as a one of the office buildings supporting the ExxonMobil Torrance Refinery (the establishment being an important event in the development of Torrance and Los Angeles County and to the improvement of energy facilities in the region), the building, itself, does not appear to be significantly associated with the two major episodes of construction and expansion at the refinery in 1929 and in 1965 that characterize significant events. Research has yielded no information indicating the Administration Building is directly associated with the life and career of an individual who made important contributions to the history of the United States, California, or Los Angeles County, such as an important engineer or an administrator that was significantly associated with the establishment and development of energy facilities in the area or the general development of the region (Criterion B). Also, the Administration Building is a relatively mundane example of a mid-century modern building. Overall, the property lacks a distinguishing design and it does not appear to be representative of an important example of an administration building/office building that may illustrate the variation, evolution, or transition of construction types, technology, or materials, nor does it illustrate important design or construction for energy or oil refinery administration facilities. Overall, it does not illustrate the architectural development of the community. The architect and builder are unknown, and therefore the building does not appear to represent the work of a master (Criterion C). Finally, the Administration Building has not yielded, or may be likely to yield, information important in prehistory or history (Criterion D).

In addition, in order for a property to be eligible for listing in the National or State Register, besides meeting one of the above criterion, it must also retain its historic integrity. The National Register traditionally recognizes a property's historic integrity through seven aspects or qualities: location, design, setting, materials, workmanship, feeling, and association. In order for a property to be eligible, it must retain some, if not most, of the aspects. Location is defined as the place where the historic-period property was constructed or the place where the historic event took place. The subject property has not been moved; therefore, retains its integrity of location. However, no historic events are associated with the property. Design is defined as the composition of elements that constitute the form, plan, space, structure, and style of a property. The building does not appear to have been altered so it retains its integrity of design. Setting is defined as the physical environment of a historic-period property that illustrates the character of the place. The building was constructed at the entrance to the Mobil (now ExxonMobil) Torrance Refinery circa 1952, during the period when the 1929 refinery was still functioning and before the refinery was completely rebuilt in 1965. Although the refinery itself has undergone major modernization since construction of the Administration Building, the building still retains its integrity of setting within a major industrial refinery. Materials are defined as the physical elements combined in a particular pattern or configuration to form the aid during a period in the past. All the building materials appear to be original so the building has retained its integrity of materials. Workmanship is defined as the physical evidence of the crafts of a particular culture or people during any given period of history. The building does not demonstrate workmanship or crafts of a particular period, and therefore does not have integrity of workmanship. Feeling is defined as the quality that a historic-period property has in evoking the aesthetic or historic sense of a past period of time. Because the property has retained its original design, materials, it has retained its feeling for a mid-century administration-type building. Association is defined as the direct link between a property and the event or person for which the property is significant. No significant events or persons are associated with the building so integrity of association was not evaluated.

In conclusion, the Administration Building at 3700 W. 190th Street does not appear to be eligible for listing to the NRHP.

State of California
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____

HRI # _____

Trinomial _____

NRHP Status Code 6Z

Other Listings _____

Review Code _____ Reviewer _____ Date _____

*Page 1 of 13 *Resource Name or #: AT&T Mobility LLC LA0665-01 LTE/Storage & Extra/USID 48065

*P1. Other Identifier: Self-Storage*P2: Location: Not for publication Unrestricted ☒ a. County: Los Angeles

And (P2b and P2c or P2d. Attach a location map as necessary.)

*b. USGS Quad Torrance *Date: 1975 T; R; $\frac{1}{4}$ of $\frac{1}{4}$ of Sec. _____ B.M. _____c. Address: 2545 West 190th Street City: Torrance Zip: 90504d. UTM: (Give more than one large or linear resources) Zone: Me/ mN

e. Other Locational Data (e.g. parcel #, directions to resource, elevation, etc. as appropriate);

APN: #4090-021-045, Home Tract, Lots 29, 30, 31

*P3a. Description (Describe resource and its major elements, include design, materials, condition, alterations, size, setting and boundaries.)

The subject building is a multiple-level, asymmetrical, basically rectangular shaped, Modern style, commercial warehouse structure located on a major commercial artery in the city of Torrance. The building is sited within the immediate proximity of other large corporate buildings and parking lots for these structures in an area of the city that is devoted to large industrial complexes. The building has a concrete foundation, stucco, metal, and concrete exterior with a flat roof. Tower projects on the corners of the building rise above the roofline. The building has two major components – one comprised of 143,256 square feet of space with an additional 68,808 square foot of space. The building has multiple entrances but the primary office entrance is located on the south façade. The entrance consists of metal and glass doors with a large awning over the doors. A large concrete block wall curves around the area immediately to the west of the main entrance. A large concrete ramp is present on the east side of the building leading to a second story entrance area. The building's exterior has two pairs of single metal door entrances covered with a flat canopy roof on the north façade. Additional garage door style entrances are present at various points on the façade. Windows are large, metal framed, multi-lite, square shaped, windows set at regular intervals on the west and south facades. The property is landscaped with mature trees and bushes. A large parking lot is present immediately in front of the building. The building appears to be in good condition.

*P3b. Resource Attributes: (List attributes and codes) HP 6: 1-3 Story Commercial Building*P4. Resources Present: Building ☒ Structure

Object Site District Element of District

*P5b. Description of Photo: (View, date

Accessions #) View SE/ 06/10/2011

*P6. Date Constructed/Age and Source

Historic ☒ Prehistoric Both c. 1960/ Los Angeles County Assessor's Office

*P7. Address: Donya Warner, 2545 W. 190th Street, Torrance, CA 90504

*P8: Recorded by: (Name, Affiliation, Address)

K.A. Crawford/Crawford Historic Services
P.O. Box 634, La Mesa, CA 91944

*P9. Date Recorded: 06/10/2011*P10. Type of Survey: (Describe) Intensive

*P11: Report Citation (Cite Survey Report and other sources, or enter "None".) None



*Attachments: None Location Map Sketch Map Continuation Sheet ☒ Building, Structure and Object Record ☒ Archaeological Record District Record Liner Resource Record Milling Station Record Rock Art Record Artifact Record Photograph Record Other (List):

State of California – The Resources Agency
 DEPARTMENT OF PARKS AND RECREATION
 RESIDENCE, STRUCTURE, AND OBJECT RECORD

Primary #
 HRI#
 *NRHP Status Code 6Z

*Page 2 of 13 *Resource Name or # (Assigned by Recorder) AT&T Mobility LLC
LA0665-01 LTE/Storage & Extra/USID 48065

B1. Historic Name: None

B2. Common Name: Self-Storage

B3. Original Use: Commercial/Warehouse

B4. Present Use: Commercial/Warehouse

*B5. Architectural Style: Modern

*B6. Construction History: (Construction Date, alterations and dates of alterations)

The building was constructed in 1960. A large addition was constructed in 1962.

*B7. Moved? X No Yes Unknown Date: Original Location

*B8. Related Features: Parking lot

B9a. Architect: Unknown b. Builder: Unknown

*B10. Significance: Modern Architecture Area: Los Angeles Period of Significance: 1960
 - Present Property Type: Commercial Applicable Criteria: C

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The building was constructed in 1960 according to the Los Angeles County Assessor's Office and confirmed by historic aeriels. No original building permits were located in the City of Torrance Building Department permit files. The building was constructed as a large storage facility and was altered by the addition of over 68,000 square feet of space in 1962 to increase its storage capacity. The building has been used as a storage building since it's construction in 1960 and continues to operate in this capacity at the present time.

B11. Additional Resource Attributes: (List attributes and codes) None

*B12. References: McAlester and McAlester, A Guide to American Houses, 1991; City of Torrance Building Permits; County of Los Angeles Assessor's Office records; Historicaerials.com,

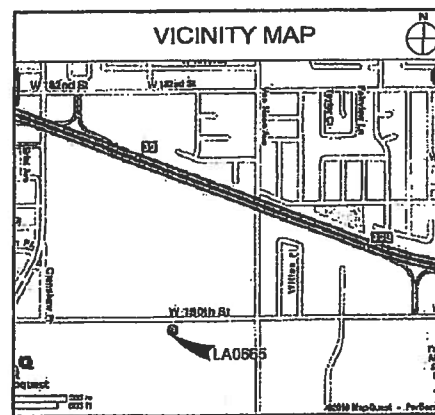
B13. Remarks: None

*B14. Evaluators: K.A. Crawford

*Date of Evaluation: 06/10/2011

(This space reserved for
 official comments.)

(Sketch Map with north arrow required.)



State of California – The Resource Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # _____

HRI# _____

Trinomial _____

Page 3 of *Resource Name or # (Assigned by recorder) AT&T Mobility LLC LA0665-01 LTE/Storage & Extra/USID 48065

*Recorded by K.A. Crawford/Crawford Historic Services

Date June 10, 2011

Continuation ☒ Update

(Continued from page 2)

Integrity Statement

In regard to the seven aspects of integrity – location, design, setting, materials, workmanship, feeling and association – the c. 1960 Modern style building on this property has retained its original location. The building has not been moved. The setting, feeling, and association have not remained intact as the commercial area surrounding the structure has changed substantially. Major changes from 1960 to the present have altered the original neighborhood setting, feeling and association. The design, materials and workmanship of the structure have basically remained intact as the various alterations and interior changes have not significantly reduced the building's integrity. The building has retained its integrity and the condition of the building is good.

National Register of Historic Places Eligibility Evaluation

The property was assessed under National Register of Historic Places **Criterion A** for its potential significance as part of any historic trends or events that may have made a significant contribution to the broad patterns of our history. The building was constructed as part of the overall continuing commercial development of the Torrance area which began in the 1880s. **Therefore, the property does not appear to meet the criteria for significance under Criterion A: Event.**

The property was assessed under National Register of Historic Places **Criterion B** for its potential significance and association with a person of importance in national history. There is no evidence to suggest that any of the persons associated with the construction or development of the building were considered important in the history of the property or nation. None of the persons associated with the property appear to be historically significant at the level necessary to meet the criteria for National Register of Historic Places. **Therefore, the property does not appear to meet the criteria for significance under Criterion B: Person.**

The property was assessed under National Register of Historic Places **Criterion C** for its potential significance as a property which embodies the distinctive characteristics of a type, period, method of construction or style of Modern architecture, represents the work of a master architect, builder or craftsman, possesses high artistic values, or represents a significant or distinguishable entity whose components lack individual distinction. The c. 1960 building is a standard example of the main characteristics of the Modern style and does not display any unusual features or elements that would raise it to a level of significance to qualify for the National Register of Historic Places. The building is not an example of the work of a master architect, builder or craftsman as no architect or contractor was identified. **Therefore, the building cannot be considered to represent the work of a master architect, builder or craftsman. Therefore, the property does not appear to meet the criteria for significance under Criterion C: Architecture as a good example of Modern style architecture.**

The property was assessed under National Register of Historic Places **Criterion D** for its potential significance and its ability to convey information. The property does not yield, or may not be likely to yield, information important in prehistory or history. In order for buildings, structures, or objects to be significant under Criterion D, they need to "be, or must have been, the principal source of information." This is not the case with this property. **Therefore, the property does not appear to meet the criteria for significance under Criterion D: Information Potential.**

In summary, the property does not appear to qualify for the National Register of Historic Places under any of the above criteria. Therefore, the building is not considered to be an historic resource for the purposes of the NHPA. The property was not accessed for eligibility under the California Register or local Torrance Register eligibility.

State of California – The Resource Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # _____

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Page 4 of 13 *Resource Name or # (Assigned by recorder) AT&T MOBILITY LLC LA0665-01
LTE/STORAGE & EXTRA/USID 48065

*Recorded by K.A. Crawford/Crawford Historic ServicesDate June 10, 2011Continuation ☒ Update

AT&T MOBILITY LLC LA0665-01 LTE/STORAGE & EXTRA/USID 48065

2545 WEST 190TH STREET, TORRANCE, CA 90504

View South/West Façade

June 10, 2011



State of California – The Resource Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # _____

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Page 5 of 13 *Resource Name or # (Assigned by recorder) AT&T MOBILITY LLC LA0665-01
LTE/STORAGE & EXTRA/USID 48065

*Recorded by K.A. Crawford/Crawford Historic ServicesDate June 10, 2011Continuation ☒ Update

AT&T MOBILITY LLC LA0665-01 LTE/STORAGE & EXTRA/USID 48065
2545 WEST 190TH STREET, TORRANCE, CA 90504
View Southeast/West Facade
June 10, 2011



State of California – The Resource Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # _____

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Page 6 of 13 *Resource Name or # (Assigned by recorder) AT&T MOBILITY LLC LA0665-01
LTE/STORAGE & EXTRA/USID 48065

*Recorded by K.A. Crawford/Crawford Historic ServicesDate June 10, 2011Continuation ☒ Update

AT&T MOBILITY LLC LA0665-01 LTE/STORAGE & EXTRA/USID 48065
2545 WEST 190TH STREET, TORRANCE, CA 90504
View Northeast/South and West Facades
June 10, 2011



State of California – The Resource Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

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Page 7 of 13 *Resource Name or # (Assigned by recorder) AT&T MOBILITY LLC LA0665-01
LTE/STORAGE & EXTRA/USID 48065

*Recorded by K.A. Crawford/Crawford Historic Services
Continuation ☒ Update

Date June 10, 2011

AT&T MOBILITY LLC LA0665-01 LTE/STORAGE & EXTRA/USID 48065
2545 WEST 190TH STREET, TORRANCE, CA 90504
View North/South Façade
June 10, 2011



State of California – The Resource Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

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Page 8 of 13 *Resource Name or # (Assigned by recorder) AT&T MOBILITY LLC LA0665-01
LTE/STORAGE & EXTRA/USID 48065

*Recorded by K.A. Crawford/Crawford Historic ServicesDate June 10, 2011Continuation ☒ Update

AT&T MOBILITY LLC LA0665-01 LTE/STORAGE & EXTRA/USID 48065
2545 WEST 190TH STREET, TORRANCE, CA 90504
View North/South Façade/Main Entrance
June 10, 2011



State of California – The Resource Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # _____

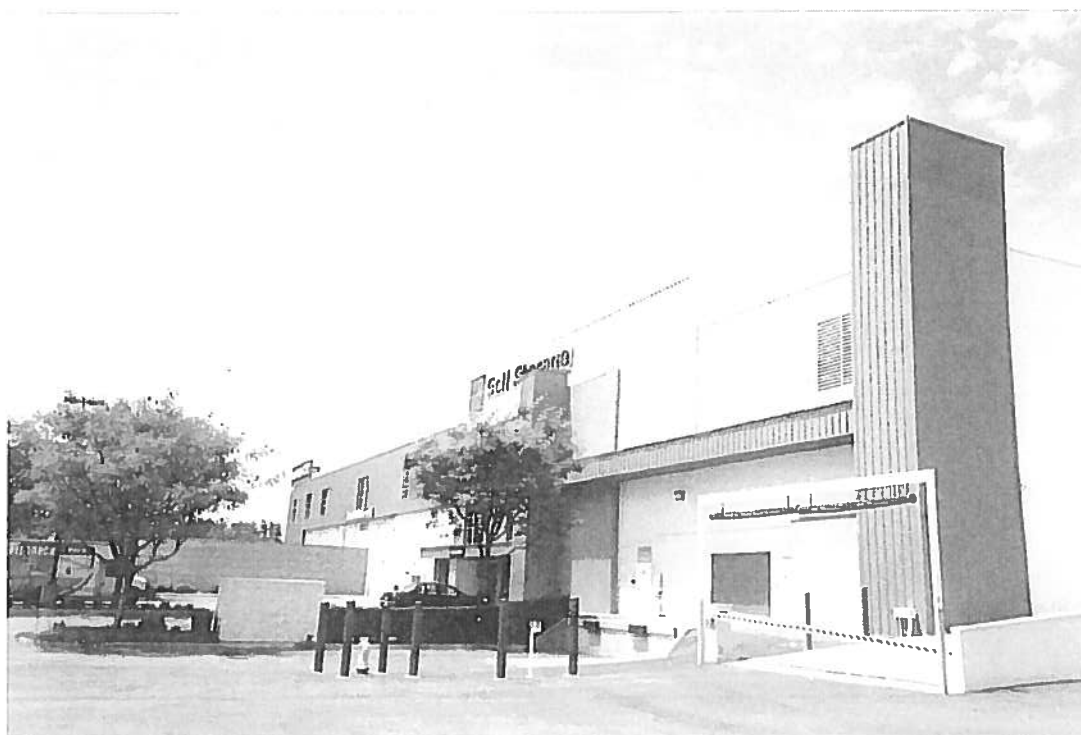
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Trinomial _____

Page 9 of 13 *Resource Name or # (Assigned by recorder) AT&T MOBILITY LLC LA0665-01
LTE/STORAGE & EXTRA/USID 48065

*Recorded by K.A. Crawford/Crawford Historic ServicesDate June 10, 2011Continuation ☒ Update

AT&T MOBILITY LLC LA0665-01 LTE/STORAGE & EXTRA/USID 48065
2545 WEST 190TH STREET, TORRANCE, CA 90504
View Northwest/South Façade
June 10, 2011



State of California – The Resource Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # _____

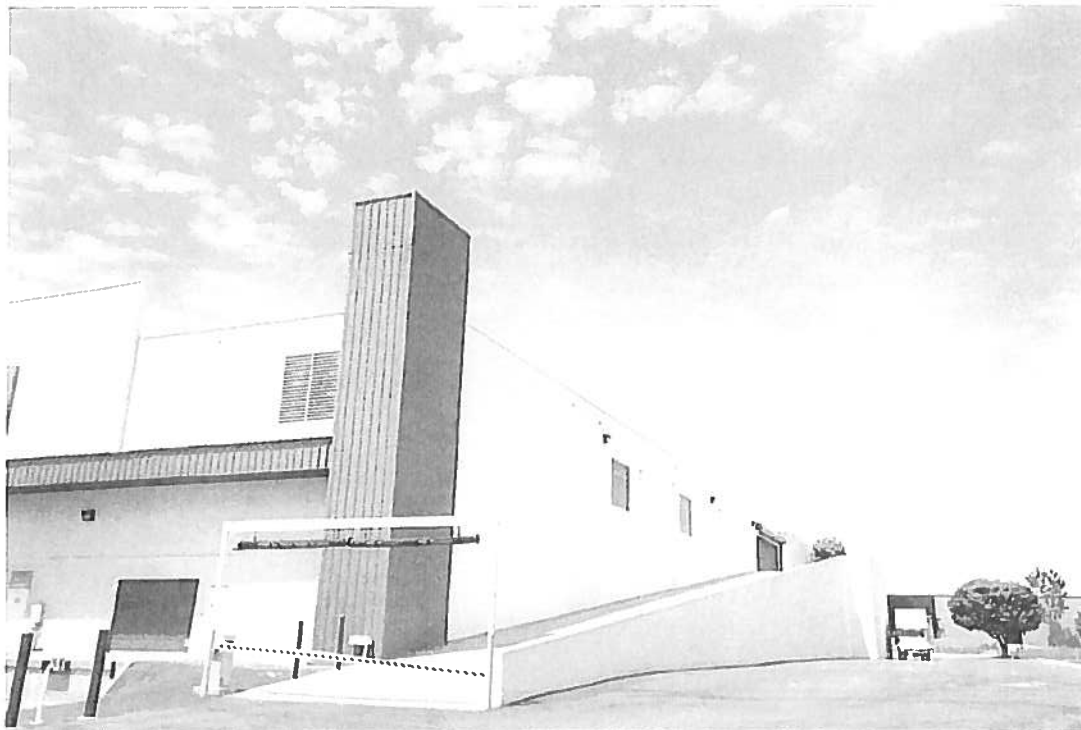
HRI# _____

Trinomial _____

Page 10 of 13 *Resource Name or # (Assigned by recorder) AT&T MOBILITY LLC LA0665-01
LTE/STORAGE & EXTRA/USID 48065

*Recorded by K.A. Crawford/Crawford Historic ServicesDate June 10, 2011Continuation ☒ Update

AT&T MOBILITY LLC LA0665-01 LTE/STORAGE & EXTRA/USID 48065
2545 WEST 190TH STREET, TORRANCE, CA 90504
View Northwest/South and East Façades
June 10, 2011



State of California – The Resource Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

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Page 11 of 13 *Resource Name or # (Assigned by recorder) AT&T MOBILITY LLC LA0665-01
LTE/STORAGE & EXTRA/USID 48065

*Recorded by K.A. Crawford/Crawford Historic ServicesDate June 10, 2011Continuation ☒ Update

AT&T MOBILITY LLC LA0665-01 LTE/STORAGE & EXTRA/USID 48065

2545 WEST 190TH STREET, TORRANCE, CA 90504

View Northwest/East Façade

June 10, 2011



State of California – The Resource Agency
DEPARTMENT OF PARKS AND RECREATION
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Page 12 of 13 *Resource Name or # (Assigned by recorder) AT&T MOBILITY LLC LA0665-01
LTE/STORAGE & EXTRA/USID 48065

*Recorded by K.A. Crawford/Crawford Historic ServicesDate June 10, 2011Continuation ☒ Update

AT&T MOBILITY LLC LA0665-01 LTE/STORAGE & EXTRA/USID 48065

2545 WEST 190TH STREET, TORRANCE, CA 90504

View Southeast/North Façade

June 10, 2011



State of California – The Resource Agency
DEPARTMENT OF PARKS AND RECREATION
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Page 13 of 13 *Resource Name or # (Assigned by recorder) AT&T MOBILITY LLC LA0665-01
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*Recorded by K.A. Crawford/Crawford Historic ServicesDate June 10, 2011Continuation ☒ Update

AT&T MOBILITY LLC LA0665-01 LTE/STORAGE & EXTRA/USID 48065
2545 WEST 190TH STREET, TORRANCE, CA 90504
View South/North Façade/Side Door Detail
June 10, 2011



- Project
- Half mile radius
- 1 mile radius

Del Amo/Crenshaw Boulevard Project

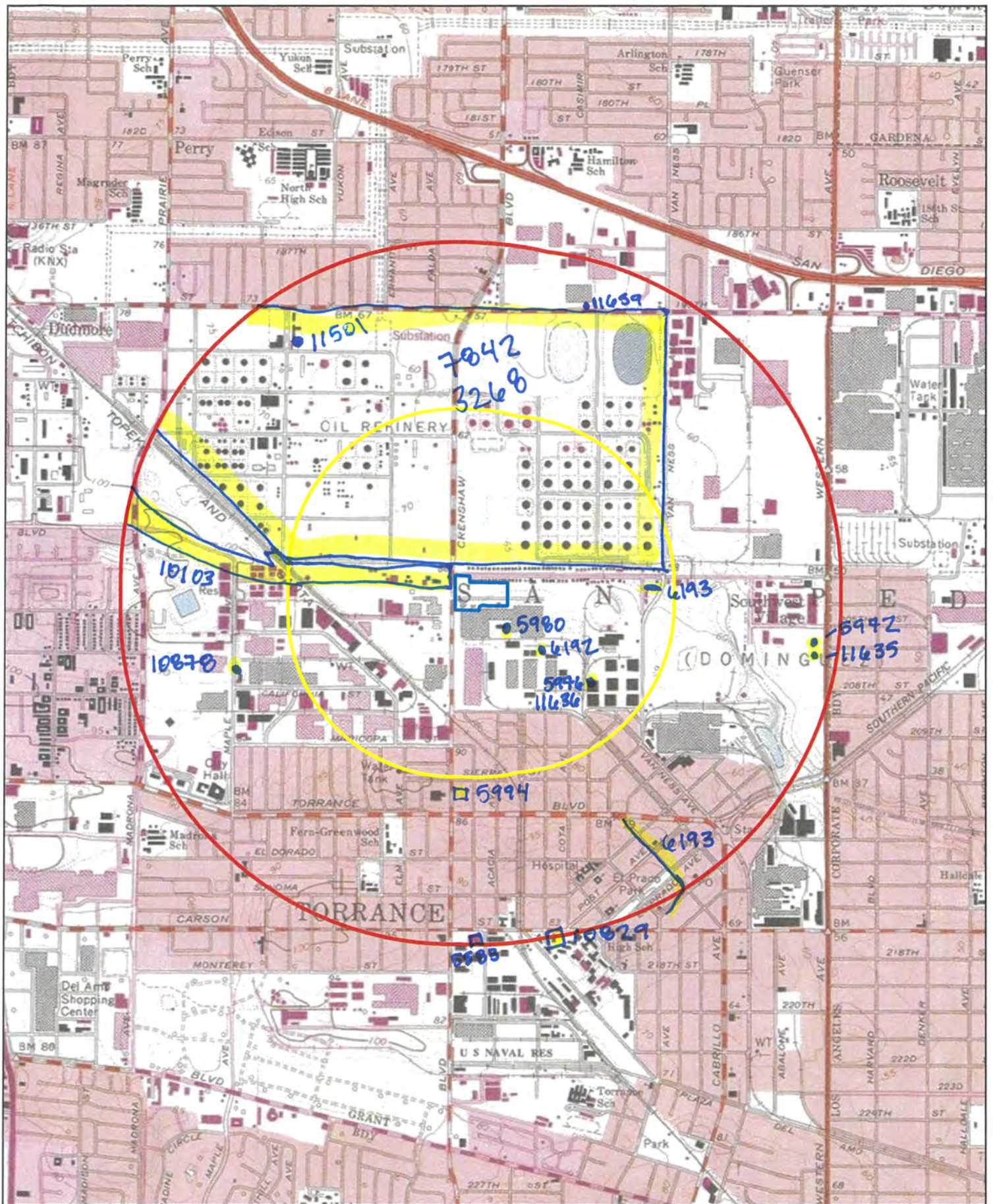
USGS *Torrance* Quadrangle
(7.5-minute series)

Reports

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1 of 3

ETS BFSA 8/14/2024

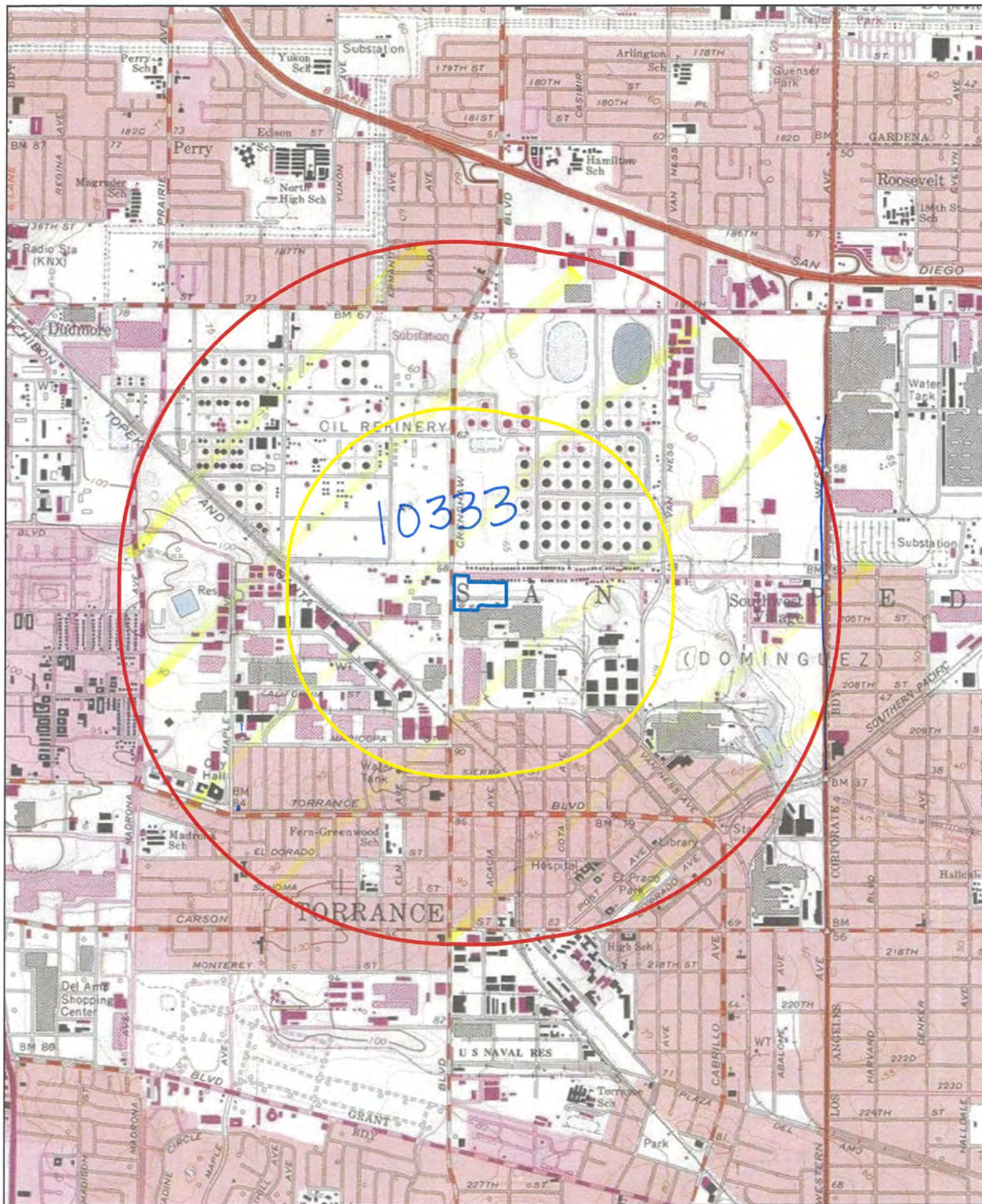


- Project
- Half mile radius
- 1 mile radius

Del Amo/Crenshaw Boulevard Project

USGS Torrance Quadrangle
(7.5-minute series)

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 Reports
 1:24,000
 2 of 3
 ETS BFSa 8/14/2024



- Project
- Half mile radius
- 1 mile radius

Del Amo/Crenshaw Boulevard Project

USGS Torrance Quadrangle
(7.5-minute series)



Reports
1:24,000
3 of 3

ETS BFSA 8/14/2024

Report List

Del Amo/Crenshaw Blvd

Report No.	Other IDs	Year	Author(s)	Title	Affiliation	Resources
LA-02904		1993	Stickel, Gary E.	Draft Report a Phase I Cultural Resources Literature Search for the West Basin Water Reclamation Project	Environmental Research Archaeologists: A Scientific Consortium	
LA-05583		2001	Wallock, Nicole	Cultural Resource Assessment: Cingular Wireless Facility No. Sm 076-01 Los Angeles County, California	LSA Associates, Inc.	
LA-05972		2002	Duke, Curt	Cultural Resource Assessment at & T Wireless Services Facility No. 05205a Los Angeles County, California	LSA Associates, Inc.	
LA-05976		2002	Duke, Curt	Cultural Resource Assessment at & T Wireless Services Facility No. 05044a-01 Los Angeles County, California	LSA Associates, Inc.	
LA-05980		2002	Duke, Curt	Cultural Resource Assessment at & T Wireless Services Facility No. 05127a Los Angeles County, California	LSA Associates, Inc.	
LA-05994		2002	Duke, Curt	Cultural Resource Assessment at & T Wireless Services Facility No. 05197 Los Angeles County, California	LSA Associates, Inc.	
LA-06192		2002	Duke, Curt	Cultural Reource Assessment at & T Wireless Services Facility No. 05127b Los Angeles County, California	LSA Associates, Inc.	
LA-06193		2002	Holson, John	Archaeological Survey and Record Search for Ospc-0036, La/torrance, Torrance and Los Angeles, Los Angeles County (800-39)	Pacific Legacy, Inc.	
LA-07842		2000	Maki, Mary K.	Phase I Archaeological Investigation of Limited Areas Within the Torrance Refinery and Atwood, Southwestern Marine and Vernon Terminals, Los Angeles and Orange Counties, California	Conejo Archaeological Consultants	
LA-09910		2009	Phil Fulton	Cultural Resource Assessment - Verizon Wireless Services, Prairie Facility, City of Torrance, Los Angeles County, California	LSA Associates, Inc.	19-186930
LA-10103		2003	Dolan, Christy and Monica Strauss	Historic Property Survey Report Del Amo Boulevard Extension Project City of Torrance, Los Angeles County, California	EDAW, Inc.	19-186929, 19-186931
LA-10333		2009	McKenna, Jeanette M.	A Brief Historic Context Statement Prepared for the General Plan Update: The City of Torrance, Los Angeles County, California	McKenna et al.	19-000100, 19-000110, 19-000127, 19-000137, 19-000138, 19-000191, 19-000276, 19-000277, 19-000278, 19-000279, 19-000280, 19-000281, 19-000344, 19-002378

Report List

Del Amo/Crenshaw Blvd

Report No.	Other IDs	Year	Author(s)	Title	Affiliation	Resources
LA-10567		2005	Hogan, Michael, Bai "Tom" Tang, Josh Smallwood, Laura Hensley Shaker, and Casey Tibbitt	Identification and Evaluation of Historic Properties - West Basin Municipal Water District Harbor- South Bay Water Recycling Project Proposed Project Laterals	CRM Tech	
LA-10829		2003	Fader, Steven	Torrance Health Center, Treatment of Character-Defining Elements	Steven Fader Architect	19-178514
LA-10878		2007	Bonner, Wayne	Cutural Resources Records Search and Site Visit Results for T-Mobile Candidate LA03369C (SCE Maple), 505 Maple Avenue, Torrance, Los Angeles County, California	Michael Brandman Associates	19-186929, 19-186930, 19-186931
LA-11051		2011	Martorama, Dean	Verizon Cellular Communications Tower Site - Exxon Mobil IBR 3700 W. 190th Street, (APN: 7352-002-029) Torrance, CA 90504: Results of Architectural History Survey for Verizon Cellular Communications Tower Site	URS	19-189420
LA-11150		2003	Maxwell, Pamela	West Basin Municipal Water District Harbor/ South Bay Water Recycling Project	U.S. Army Corps of Engineers	19-000094, 19-000098, 19-000103, 19-000140, 19-000276, 19-000277, 19-000278, 19-000279, 19-000280, 19-000281, 19-000282, 19-000389, 19-000390, 19-000709, 19-000794, 19-000822, 19-000844, 19-000845, 19-000847, 19-000848, 19-000999, 19-001735
LA-11635		2011	Bonner, Wayne H.	Cultural Resources Records Search and Site Visit Results for AT&T Mobility, LLC Candidate LA0152-01, USID 25673 (Hanjin Worldwide Express), 20435 South Western Avenue, Torrance, Los Angeles County, California	Environmental Assessment Specialists	
LA-11636		2011	Bonner, Wayne H.	Cultural Resources Records Search and Site Visit Results for AT&T Mobility, LLC Candidate LA0096-01, USID 24319 (Central and Van Ness), 2201 Dominguez Street, Torrance, Los Angeles County, California	Environmental Assessment Specialists	
LA-11659		2011	Bonner, Wayne H.	Cultural Resources Records Search and Site Visit Results for AT&T Mobility, LLC Candidate LA0665-01, USID 48065 (Storage & Extra), 2545 West 190th Street, Torrance, Los Angeles County, California	Environmental Assessment Specialists	19-189959

Report List

Del Amo/Crenshaw Blvd

Report No.	Other IDs	Year	Author(s)	Title	Affiliation	Resources
LA-13019		2013	Bonner, Wayne H. and Kathleen A. Crawford	Cultural Resources Records Search and SHE Visit Results for T Mobile West, LLC Candidate LA33694A (ATT Switch - Torrance), 1307 Cravens Avenue, Torrance, Los Angeles County, California	Environmental Assessment Specialists, Inc.	
OR-03268		2000	Maki, Mary K.	Phase I Archaeological Investigation of Limited Areas Within the Torrance Refinery and Atwood, Southwestern Marine and Vernon Terminals, Los Angeles and Orange Counties, California	Conejo Archaeological Consultants	