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# BAYLANDS SPECIFIC PLAN

## Cultural Resources Technical Report

Prepared for  
City of Brisbane

March 2025





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# STATEMENT OF CONFIDENTIALITY

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This report identifies the locations of cultural resources in the vicinity of the Bayland Specific Plan in the City of Brisbane, San Mateo County, California. Disclosure of this information to the public may be in violation of both federal and State laws. Federal regulations applicable to the Project include, but may not be limited to, Section 304 of the National Historic Preservation Act (54 U.S. Code [USC] Section 307103) and the Archaeological Resources Protection Act (16 USC Section 470h). The applicable State regulations include, but may not be limited to, California Government Code Section 6250 et seq. and Section 6254 et seq. Disclosure of site location information to individuals other than those meeting the U.S. Secretary of the Interior's Professional Qualifications Standards (36 Code of Federal Regulations Part 61) or the California State Personnel Board criteria for Associate State Archaeologist or State Historian II violates the California Office of Historic Preservation records access policy.

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# SUMMARY OF FINDINGS

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## Baylands Specific Plan Cultural Resources Technical Report

Environmental Science Associates (ESA) has prepared this 15B16BCultural Resources Technical Report for the Bayland Specific Plan (Specific Plan). The Project Area is located in the City of Brisbane in San Mateo County and includes the Baylands Specific Plan Area along with offsite areas intended for Baylands-related infrastructure and public facilities. This study has been completed to comply with the California Environmental Quality Act (CEQA) of 1970, as amended. The Specific Plan includes a comprehensive plan for development of the 684.3-acre Baylands site, including goals, policies, and development standards and plans to guide future development actions. The Specific Plan proposes the development of 2,200 residential units and 6.5 million square feet of retail, commercial, office, conference, and research and development, and campus uses; 500,000 square feet of hotel use; and a middle school, open space, and parks and trails.

To determine whether cultural resources are in the Project Area, ESA conducted a cultural resources records search, pedestrian survey, architectural history review, and geoarchaeological analysis. ESA identified 20 cultural resources within the Project Area. These resources include: one pre-contact habitation site with historic-era artifact scatter (P-38-005131); one historic-era artifact scatter (P-41-002160); and 18 historic-era architectural resources (SPRR Roundhouse, the Machinery & Equipment Building (P-41-002720; 3401 Bayshore Blvd), the South San Francisco Overhead (P-41-002721), 10 Industrial Way, 131 Industrial Way, 140–190 Industrial Way, 151–171 Industrial Way, 200 Industrial Way, 21–27 Industrial Way, 250–256 Industrial Way, 36 Industrial Way, 40 Industrial Way, 60–130 Industrial Way, 290–296, 300–312, 340–374, and 380 Industrial Way, 2629–2635 Bayshore Boulevard, 595 Tunnel Avenue, 601 Tunnel Avenue, and 950 Tunnel Avenue). The Lazzari Charcoal Building was previously documented within the project site, but was destroyed in a fire in April 2024. As a result of this study, one archaeological resource, P-38-005131 and three architectural resources, SPRR Bayshore Roundhouse and Turntable, the Machinery and Equipment Building (P-41-002720), and the South San Francisco Overhead (P-41-002721) have been recommended or determined eligible for the California Register of Historical Resources or listed in the City of Brisbane General Plan (1994, as amended) and are considered historical resources for the purposes of CEQA.

This report does not identify or analyze the potential for tribal cultural resources, as defined by CEQA for the Project Area; the identification and evaluation of potential impacts on such resources should be part of the tribal consultation processes conducted by the CEQA lead agency, the City of Brisbane.

The Specific Plan has the potential to impact architectural and archaeological resources that are also considered historical resources. To address these impacts, ESA prepared mitigation measures to reduce impacts to historical resources to a less-than-significant level.

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# CHAPTER 1

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## Introduction

Environmental Science Associates (ESA) has prepared this 15B16BCultural Resources Technical Report for Baylands Specific Plan (Specific Plan), which would consist of a comprehensive plan for development of the 684.3-acre Baylands Specific Plan site (Specific Plan site), including goals, policies, and development standards and plans to guide future development actions. The Specific Plan site includes 563.5 acres of land area and 120.8 acres of lagoon.

The Specific Plan proposes the development of 2,200 residential units and 6.5 million square feet of retail, commercial, office, conference, and research and development, and campus uses; 500,000 square feet of hotel use; a school, open space, and parks and trails and various infrastructure improvements within the Specific Plan site. In addition to the Specific Plan Area, offsite improvements are also proposed as part of the Project, consisting of the construction of a new fire station at 140 Valley Drive; the use of the existing fire station located at 3445 Bayshore Boulevard for training purposes; the installation of waterlines through Bayshore Boulevard and Guadalupe Canyon Drive, and installation of approximately recycled waterlines to serve South San Francisco. The Specific Plan site and the offsite improvement areas are analyzed in this report as the Project Area.

The Project Area is shown on the U.S. Geological Survey (USGS) *South San Francisco*, California 7.5-minute topographic quadrangle (**Figures 1 and 2**; USGS 1981).

The Specific Plan must comply with the California Environmental Quality Act (CEQA), as amended. The City of Brisbane (City) is the lead agency for CEQA. The purpose of this report, in accordance with CEQA, is to:

- Identify cultural resources, including pre-contact and historic-era archaeological resources, buildings, structures, and places of importance to Native Americans within the Project Area;
- Preliminarily evaluate cultural resources according to the criteria set forth by the California Register of Historical Resources (California Register);
- Determine whether the Specific Plan would have an impact on California Register-listed or eligible resources (i.e., historical resources as defined by the CEQA); and
- If needed, recommend procedures for avoidance or mitigation of significant impacts to historical resources and/or unique archaeological resources.

Ashleigh Sims, MA, RPA, completed the archaeological portions of this report. Sims meets the Secretary of the Interior's Professional Qualifications Standards (SOI PQS) for Archeology.

Kathy Cleveland, MA, and Amber Grady, MA, completed the architectural history portions of this report. Cleveland and Grady meet the SOI PQS for Architectural History. Heidi Koenig, MA, RPA, reviewed the archaeological portions of this report and meets the SOI PQS for Archaeology. Becky Urbano, MA, reviewed the architectural history portions of this report and meets the SOI PQS for History and Architectural History.

## **1.1 Plan Location and Description**

The Project Area and the City of Brisbane are within the nine-county San Francisco Bay region and are located in the northeastern corner of San Mateo County immediately south of the City and County of San Francisco (see Figure 1). Municipalities adjoining Brisbane include San Francisco to the north, Daly City, and an unincorporated portion of San Mateo County to the west, and South San Francisco to the south.

The Project Area is bounded on the east by U.S. Highway 101 (U.S. 101) and on the west and south by Bayshore Boulevard (see Figure 2). The Project Area's northern boundary is formed by the San Francisco County line and the portion of the existing Recology waste management facilities within the City of Brisbane.

The applicant, Sunquest Properties Inc. ("Sunquest") and its development manager, Baylands Development Inc. ("BDI"), collectively referred to as the "applicant," are proposing development of 2,200 dwelling units, 6.5 million square feet of commercial office development, and an additional 500,000 square feet of hotel use; acquisition of a water supply; and construction of associated onsite and offsite infrastructure and public facilities. To accomplish this development, the applicant is requesting approval of a General Plan Amendment and the Baylands Specific Plan.

Currently, the Specific Plan site is split between the Baylands and Beatty General Plan Subareas. An amendment to the Brisbane General Plan is proposed to modify the General Plan land use map to include the entirety of the Baylands Specific Plan within the Bayland Subarea. The land use designation for the portion of the Baylands Specific Plan currently within the Beatty Subarea would be modified from Heavy Commercial to Baylands Planned Development, Residential Prohibited. In addition, the Brisbane General Plan Circulation Element is proposed to be amended to realign Lagoon Road to directly access the southbound U.S. 101 freeway ramps at Sierra Point Parkway and extend Sierra Point Parkway from its current terminus at the southbound U.S. 101 freeway ramps north to Geneva Avenue.

### **1.1.1 Restoration of Historic Roundhouse**

The Specific Plan provides for rehabilitation of the SPRR Bayshore Roundhouse and its adaptive reuse. The Specific Plan proposes that the restored Roundhouse become the "focal point for community activity and interactions for the community and serve as an anchor point for connecting (the Baylands) to the existing Brisbane cultural heritage." The Specific Plan's land use plan places the Roundhouse within a 3.5-acre circular park echoing the building's form and provides for adaptive reuse of the Roundhouse and adjacent site as a public amenity with an

emphasis on preserving the unique character of this railroad building typology. The proposed adaptive reuse program provides for a community center, railroad museum, café, and open-air theater in conformance with the Secretary of Interior's Standards for Rehabilitation. In addition, the Specific Plan proposes interpretive design and choice of landscape materials and features that help tell the story of how the turntable pit, "whisker tracks" emanating from the turntable, and other features contributed to the historic use of the Roundhouse.

Based on the recommendations of the Brisbane Bayshore Roundhouse Stabilization and Rehabilitation Plan, the Specific Plan proposes severing the Roundhouse structure from its existing concrete slab, lifting the building, and setting it down upon a new concrete slab supported by new stem walls and compacted fill following site grading to achieve a pad elevation protected from sea level rise and flooding (Page & Turnbull 2020). The stabilization and restoration of the Roundhouse is proposed to be completed in five increments:

1. **Safety and Security:** The initial increment of Roundhouse stabilization and rehabilitation would include fencing the site, installing security measures to prevent unwanted access, mitigating imminent hazards, and removal of pests and plants. This increment is proposed to be initiated prior to Specific Plan approval.
2. **Initial Stabilization:** Once security measures have been installed and imminent hazards have been mitigated, the applicant proposes temporarily "mothballing" the Roundhouse to prevent further damage and deterioration. This would include protecting the structure from further moisture penetration and plant and pest infestation, as well as stabilizing the structural components against wind and seismic forces. These stabilization measures are expected to be in place for two to three years.
3. **Raising the Building and Re-Grading the Site:** In response to expected sea level rise, the Roundhouse site would be raised, and the structure would be lifted or deconstructed piece-by-piece and reconstructed at a higher elevation on a new foundation. Comprehensive materials testing would be undertaken prior to lifting or deconstructing the Roundhouse to inform the strategy for subsequent restoration and retrofit.
4. **Second Stabilization:** After the site has been raised, a new foundation would be constructed, and the Roundhouse structure would be connected to or re-built atop the new foundation. Some portion of the final retrofit would be installed at this time to stabilize the structure, such as columns that need to be attached to the new foundation, steel framing to brace the brick walls, and repairs to wood roof framing would be installed at this time to stabilize the structure. Protection against moisture and infestation would then be reinstated (similar to the initial stabilization) while awaiting final restoration.
5. **Final Restoration and Retrofit:** The final restoration of the Roundhouse is proposed to include a seismic retrofit and strengthening of the gravity system using the California Historic Building Code to extend the life of the Roundhouse and provide appropriate seismic safety for use and occupancy of the Roundhouse.

The SOI Standards for Rehabilitation include ten criteria, including use, maintenance of historic materials, preventing the creation of a false sense of history, retention of changes that are

significant in their own right, preservation of distinctive materials, repair of damaged historic features, prevention of damage resulting from chemical or physical treatments, preservation of archaeological resources (not applicable), complimentary additions or alterations, and potential to remove any new additions while maintaining the building's essential form and integrity. The proposed rehabilitation of the Roundhouse, as currently designed, is in full compliance with eight of the ten Standards for Rehabilitation and is in partial compliance with two of the Standards. **Table 1** describes the standards and the proposed project's compliance (with an explanation of non-compliance). Full details of the Brisbane Bayshore Roundhouse Stabilization and Rehabilitation Plan, including SOI Standards compliance, can be found in **Appendix A**.

**TABLE 1**  
**PROJECT COMPLIANCE WITH THE SOI STANDARDS FOR REHABILITATION**

Standard No.	Standard Description	Project Compliance
1	The property will be used as it was historically or be given a new use that requires minimal change to its distinctive materials, features, spaces, and spatial relationships.	Yes
2	The historic character of a property will be retained and preserved. The removal of distinctive materials or alteration of features, spaces, and spatial relationships that characterize the property will be avoided	Yes
3	Each property will be recognized as a physical record of its time, place and use. Changes that create a false sense of historical development, such as adding conjectural features or elements from other historical properties, will not be undertaken.	Yes
4	Changes to a property that have acquired significance in their own right will be retained and preserved.	Partial (removal of non-original wood partition wall)
5	Distinctive materials, features, finishes and construction techniques or examples of craftsmanship that characterize a property will be preserved.	Partial (removal of interior tracks)
6	Deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature will match the old in design, color, texture, and, where possible, materials. Replacement of missing features will be substantiated by documentary and physical evidence.	Yes
7	Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.	Yes
8	Archeological resources will be protected and preserved in place. If such resources must be disturbed, mitigation measures will be undertaken.	Yes
9	New additions, exterior alterations, or related new construction will not destroy historic materials, features, and spatial relationships that characterize the property. The new work shall be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing to protect the integrity of the property and environment.	Yes
10	New additions and adjacent or related new construction will be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.	Yes

Overall, the proposed Project is in compliance with the Standards for Rehabilitation and would not result in the loss of historic integrity or negatively affect the building's eligibility for listing on the National Register.

## 1.2 Project Area

As previously stated, for this study, the *Project Area* is defined as the maximum extent, both horizontally and vertically, of both direct and indirect potential impacts resulting from the Specific Plan site and the offsite improvements. The Project Area encompasses the Specific Plan footprint, including areas of new construction and operations-related activities (e.g., construction staging areas) associated with the Specific Plan and the offsite improvements for the two fire stations and the waterlines. The vertical extent of the Project Area consists of the maximum depth of ground disturbance proposed by the Specific Plan and the offside improvement activities. Because detailed design of building foundations within the Specific Plan site is not available, exact depths of ground disturbance cannot be determined. The Project Area is depicted in **Figure 3**.

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# CHAPTER 2

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## Regulatory Context

### 2.1 California Regulations

The Specific Plan is subject to review under CEQA, with the City of Brisbane as lead reviewing agency for CEQA purposes. The State of California (State), through the State Historic Preservation Officer (SHPO), consults on implementation of the National Historic Preservation Act (NHPA) and also oversees statewide comprehensive cultural resource surveys and preservation programs. The California Office of Historic Preservation (OHP), as an office of the California Department of Parks and Recreation, implements these policies and also maintains the California Historical Resources Inventory. The SHPO is an appointed official who implements historic preservation programs within the State's jurisdiction.

#### 2.1.1 California Environmental Quality Act

CEQA (codified at Public Resources Code [PRC] Section 21000 et seq.) is the principal statute governing environmental review of projects occurring in the State. CEQA requires lead agencies to determine if a project would have a significant effect on historical resources, unique archaeological resources, or tribal cultural resources (TCR[s]).

The State implements provisions in CEQA through its statewide comprehensive cultural resources surveys and preservation programs. Typically, a resource must be more than 50 years old to be considered as a potential historical resource. The OHP advises recordation of any resource 45 years or older, since there is commonly a five-year lag between resource identification and the date that planning decisions are made.

#### Historical Resources

CEQA Guidelines (codified at California Code of Regulations [CCR] Section 1500 et seq.) recognize that a *historical resource* includes: (1) a resource in the California Register of Historical Resources (California Register); (2) a resource included in a local register of historical resources, as defined in Public Resources Code (PRC) Section 5020.1(k) or identified as significant in a historical resource survey meeting the requirements of PRC Section 5024.1(g); and (3) any object, building, structure, site, area, place, record, or manuscript which a lead agency determines to be historically significant or significant in the architectural, engineering, scientific, economic, agricultural, educational, social, political, military, or cultural annals of California by the lead agency, provided the lead agency's determination is supported by substantial evidence in light of the whole record.

If a lead agency determines that an archaeological site is a historical resource, the provisions of PRC Section 21084.1 and CCR Section 15064.5 apply. If an archaeological site does not meet the criteria for a historical resource contained in the CEQA Guidelines, then the site may be treated in accordance with the provisions of PRC Section 21083, pertaining to unique archaeological resources.

## Unique Archaeological Resources

As defined in PRC Section 21083.2 a *unique archaeological resource* is an archaeological artifact, object, or site, about which it can be clearly demonstrated that without merely adding to the current body of knowledge, there is a high probability that it meets any of the following criteria:

- Contains information needed to answer important scientific research questions and there is a demonstrable public interest in that information;
- Has a special and particular quality such as being the oldest of its type or the best available example of its type; or,
- Is directly associated with a scientifically recognized important pre-contact or historic event or person.

CEQA Guidelines note that if an archaeological resource is not a unique archaeological, historical resource, or TCR, the effects of the Project on those cultural resources shall not be considered a significant effect on the environment (CCR Section 15064.5[c][4]).

### 2.1.2 California Register of Historical Resources

The California Register is “an authoritative listing and guide to be used by State and local agencies, private groups, and citizens in identifying the existing historical resources of the State and to indicate which resources deserve to be protected, to the extent prudent and feasible, from substantial adverse change” (PRC Section 5024.1[a]). The criteria for eligibility for the California Register are based upon the criteria for listing in the National Register of Historic Places (National Register) (PRC Section 5024.1[b]), as defined above. Certain resources are determined by the statute to be automatically included in the California Register, including California properties formally determined eligible for, or listed in, the National Register.

To be eligible for the California Register, a cultural resource must be significant at the local, State, and/or federal level under one or more of the following four criteria:

1. Is associated with events that have made a significant contribution to the broad patterns of California’s history and cultural heritage;
2. Is associated with the lives of persons important in our past;
3. Embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of an important creative individual, or possesses high artistic values; or
4. Has yielded, or may be likely to yield, information important in prehistory or history.



A resource eligible for the California Register must be of sufficient age and retain enough of its historic character or appearance (integrity) to convey the reason for its significance. Additionally, the California Register consists of resources that are listed automatically and those that must be nominated through an application and public hearing process. The California Register automatically includes the following:

- California properties listed on the National Register and those formally determined eligible for the National Register;
- California Registered Historical Landmarks from No. 770 onward; and
- Those California Points of Historical Interest that have been evaluated by the OHP and have been recommended to the State Historical Commission for inclusion on the California Register.

Other resources that may be nominated to the California Register include:

- Historical resources with a significance rating of Category 3 through 5 (those properties identified as eligible for listing in the National Register, the California Register, and/or a local jurisdiction register);
- Individual historic resources;
- Historic resources contributing to historic districts; and
- Historic resources designated or listed as local landmarks, or designated under any local ordinance, such as an historic preservation overlay zone; and
- TCRs.

### 2.1.3 California Public Resources Code Section 5097.99

PRC Section 5097.99, as amended, states that no person shall obtain or possess any Native American artifacts or human remains which are taken from a Native American grave or cairn. Any person who knowingly or willfully obtains or possesses any such artifacts or human remains is guilty of a felony which is punishable by imprisonment. Any person who removes, without authority of law, any such items with an intent to sell or dissect or with malice or wantonness is also guilty of a felony which is punishable by imprisonment.

### 2.1.4 California Native American Historic Resource Protection Act

This California Native American Historic Resources Protection Act of 2002 imposes civil penalties, including imprisonment and fines up to \$50,000 per violation, for persons who unlawfully and maliciously excavates upon, removes, destroys, injures, or defaces a Native American historic, cultural, or sacred site that is listed or may be listed in the California Register.

## 2.1.5 California Health and Safety Code Section 7050.5

Section 7050.5 of the California Health and Safety Code (HSC) protects human remains by prohibiting the disinterring, disturbing, or removing of human remains from any location other than a dedicated cemetery. PRC Section 5097.98 (and reiterated in CCR Section 15064.59 [e]) also identifies steps to follow in the event of the accidental discovery or recognition of any human remains in any location other than a dedicated cemetery.

## 2.2 Local Regulation

### 2.2.1 City of Brisbane General Plan

The City has several policies pertaining to the conservation and protection of cultural resources as part of its general plan. These policies pertain to both historic-era buildings and archaeological resources. While the City of Brisbane identifies significant local historic resources within the General Plan, it does not maintain a process for nomination or a list of qualifying criteria.

**Policy 136:** Entourage [sic] the maintenance and rehabilitation of structures important to the history of Brisbane.

**Program 136a:** Provide assistance to owners of historic property in planning rehabilitation projects.

**Program 136b:** Provide information to property owners on loan and grant funds and tax incentives.

**Program 136c:** Provide local incentives, such as the Brisbane Star awards, to maintain historic places.

**Policy 137:** Conserve pre-historic resources in accordance with State and Federal requirements.

**Program 137a:** Consider amendments to the Zoning Ordinance to require resource surveys in conjunction with land use development applications and to establish procedures in the event of discovery to protect Native American Cultural Resources consistent with the standardized procedures given in Appendix K of CEQA.

# CHAPTER 3

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## Environmental Context

### 3.1 Physiography

The Baylands was a tidal flat prior to modern development of the San Francisco Bay Area (Bonilla 1998; USDA 2023). Historic-era and modern development over the last 180 years has completely transformed the landscape and landform of the Project Area. By 1890, rail traffic from San Francisco to San Jose had increased to four trains daily to San Jose and points beyond, as well as three trains to Menlo Park and back each day. A new level route that more closely followed the Bayshore was needed between San Bruno and San Francisco to eliminate the steep grade through Bernal Cut. In October 1904, construction of the Bayshore Cutoff began, and the work was done under the name of Bayshore Railway, a SPRR-held company. This line was one of the most expensive segments of railroad that had been built up to that time, costing almost a million dollars per mile for its 9.81 miles between San Francisco and San Bruno. The SPRR Bayshore Yard occupied the western portion of what is now the Specific Plan area from the early 1900s through the 1950s, and portions continued to be used until the 1980s. In the 1990s, the railroad features, including the many tracks between the current alignments of Bayshore Boulevard and the Caltrain alignment were removed from the area, and it has been largely unused since.

### 3.2 Geology and Soils

The surficial geology of the majority of the Project Area is Holocene-age artificial fill, with artificial fill over tidal flats along the northwest edge, and Bay mud at the southern end of the lagoon. Icehouse Hill, which is generally located east of the intersection of Bayshore Boulevard and Guadalupe Canyon Parkway, consists of surficial geology including Cretaceous and Jurassic period sandstone and shale associated with the Franciscan Complex, with small areas of Pleistocene-age slope debris and ravine fill at the edge of the point where the weathered bedrock has collected. At the northeastern corner of the Project Area is a small area with Cretaceous and Jurassic period greenstone (Bonilla 1998). Mapped soils in the Project Area consist mainly of reclaimed complex Urban land-Orthents which is artificial fill placed on top of a tidal flat. Along the northern and western edge of the Project Area, west of the filled-in tidal flat are Urban land and Orthents artificial fill soils. At Icehouse Hill, Candlestick-Kron-Buriburi complex soils are present, which is a complex of sandy loams over bedrock with bedrock being identified between 14 and 34 inches below ground surface (USDA 2023). The Orthent and Urban land soils date between the historic and modern period as the area was reclaimed by dumping building remains from the 1906 San Francisco earthquake and other fill material over time.

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# CHAPTER 4

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## Cultural Context

### 4.1 Pre-Contact Setting

Categorizing the pre-contact period into broad cultural stages allows researchers to describe a broad range of archaeological resources with similar cultural patterns and components during a given time frame, thereby creating a regional chronology. This section provides a brief discussion of the pre-contact chronology for the Project Area.

The natural marshland communities along the edges of bays and channels were the principal source for subsistence and other activities during the prehistory of the San Francisco Bay region. Many of the original surveys of archaeological sites in the Bay region were conducted between 1906 and 1908 by Stanford (and, later, U.C. Berkeley) archaeologist N. C. Nelson. Such surveys yielded the initial documentation of nearly 425 “earth mounds and shell heaps” along the littoral zone of the bay (Nelson 1909). From these beginnings, the most notable sites in the Bay region were excavated scientifically, such as the Emeryville shellmound (CA-ALA-309), the Ellis Landing Site (CA-CCO-295) in Richmond, and the Fernandez Site (CA-CCO-259) in Rodeo Valley (Moratto 1984). These dense midden sites, such as CA-ALA-309, have been carbon-14 dated to  $2,310 \pm 220$  BP, but other evidence from around the bay suggests that human occupation in the region is of greater antiquity, perhaps as early as 9,000 BP (Davis and Treganza 1959, as cited in Moratto 1984).

Archaeologists developed individual cultural chronological sequences tailored to the archaeology and material culture of each subregion of California. Each of these sequences is based principally on the presence of distinctive cultural traits and stratigraphic separation of deposits. Milliken et al. (2007) provide a framework for the interpretation of the San Francisco Bay Area. The authors divided human history in California into three periods: the *Early Period*, the *Middle Period*, and the *Late Period*. In many parts of California four periods are defined; the fourth being the *Paleoindian Period* (11500–8000 B.C.), characterized by big-game hunters occupying broad geographic areas. Evidence of human habitation during the Paleoindian Period has not yet been discovered in the San Francisco Bay Area. Economic patterns, stylistic aspects, and regional phases further subdivide cultural periods into shorter phases. This scheme uses economic and technological types, socio-politics, trade networks, population density, and variations of artifact types to differentiate between cultural periods.

During the Early Period (Lower Archaic, 8000–3500 B.C.), geographic mobility continued from the Paleoindian Period and is characterized by the millingslab and handstone as well as large wide-stemmed and leaf-shaped projectile points. The first cut shell beads and the mortar and pestle are first documented in burials during the Early Period (Middle Archaic, 3500–500 B.C.),

indicating the beginning of a shift to sedentism. During the Middle Period, which includes the Lower Middle Period (Initial Upper Archaic, 500 B.C.–A.D. 430), and Upper Middle Period (Late Upper Archaic, A.D. 430–1050), geographic mobility may have continued, although groups began to establish longer term base camps in localities from which a more diverse range of resources could be exploited. The first rich black middens are recorded from this period. The addition of milling tools, obsidian, and chert concave-base projectile points, as well as the occurrence of sites in a wider range of environments, suggest that the economic base was more diverse. By the Upper Middle Period, mobility was being replaced by the development of numerous small villages. Around A.D. 430, a dramatic cultural disruption occurred as evidenced by the sudden collapse of the *Olivella* saucer bead trade network. During the Initial Late Period (Lower Emergent, A.D. 1050–1550), social complexity developed toward lifeways of large, central villages with resident political leaders and specialized activity sites. Artifacts associated with the period include the bow and arrow, small corner-notched projectile points, and a diversity of beads and ornaments.

## 4.2 Ethnographic Setting

A compilation of ethnohistorical, historical, and archeological data indicates that the San Francisco Peninsula was inhabited by a cultural group known as the Ohlone before the arrival of Europeans (Milliken 1995). While traditional anthropological literature portrayed the Ohlone peoples as having a static culture, today it is better understood that many variations of culture and ideology existed within and between villages. While these “static” descriptions of separations between native cultures of California make it an easier task for ethnographers to describe past behaviors, this approach masks Native adaptability and self-identity. California’s Native Americans never saw themselves as members of larger “cultural groups”, as described by anthropologists. Instead, they saw themselves as members of specific village communities, perhaps related to others by marriage or kinship ties, but viewing the village as the primary identifier of their origins.

Levy (1978) describes the language group spoken by the Ohlone (often referred to as “Costanoan” in the literature). This term is originally derived from a Spanish word designating the coastal peoples of Central California. Today Costanoan is used as a linguistic term that refers to a larger language family that included distinct sociopolitical groups that spoke at least eight languages of the Penutian language group. The Ohlone once occupied a large territory from San Francisco Bay in the north to the Big Sur and Salinas Rivers in the south.

In 1770, the Ohlone lived in approximately 50 separate and politically autonomous nations, with the Yelamu people of Visitacion Valley being most closely associated with the Project Area. During the Mission Period (1770 to 1835), native populations, especially along the California coast, were brought—usually by force—to the missions by the Spanish missionaries to provide labor. The missionization caused the Ohlone people to experience cataclysmic changes in almost all areas of their life, particularly a massive decline in population caused by introduced diseases and declining birth rate, resulting in large part from colonization by the Spanish missionaries. Following the secularization of the missions by the Mexican government in the 1830s, most

Native Americans gradually left the missions and established rancherias in the surrounding areas (Levy 1978).

Economically, the Ohlone engaged in hunting and gathering. Their territory encompassed both coastal and open valley environments that contained a wide variety of resources, including grass seeds, acorns, bulbs and tubers, bear, deer, elk, antelope, a variety of bird species, and rabbit and other small mammals. The Ohlone acknowledged private ownership of goods and songs, and village ownership of rights to land and/or natural resources; they appear to have aggressively protected their village territories, requiring monetary payment for access rights in the form of clam shell beads, and even shooting trespassers if caught.

After European contact, Ohlone life ways were severely disrupted by missionization, disease, and displacement. Today the Ohlone still have a strong presence in the San Francisco Bay Area and are very interested in their historic and contemporary land use practices and lifeways.

## 4.3 Historic-Era Setting

### 4.3.1 Brisbane

Brisbane is located at the northern end of San Mateo County, north of South San Francisco, east of Daly City, and south of the City and County of San Francisco. Non-native representation in the San Francisco Bay Area dates back to the Spanish exploration of the region in 1769. Early Spanish involvement throughout California centered on missionary and military interests set on securing Spain's territories. Mission San Francisco de Asis, also known as Mission Dolores, was established in 1776, and local native people were taken in as laborers for the mission. Spanish control of the area lasted until 1821 when the newly independent Mexican government took control of California and distributed the newly secularized land holdings as land grants to various individuals.

Brisbane is part of the area that encompassed the 9,594 acres granted to Jacob Lesse in 1841 in the Rancho Canada de Guadalupe la Visitacion y Rodeo Viejo Mexican land grant. In 1843, Lesse traded his grant to Robert Ridley, and in 1884 Charles Crocker bought the land, christening it Visitacion Ranch. Following the 1906 San Francisco earthquake, real estate entrepreneurs attempting to develop the area and named their new town the City of Visitacion, but lack of funding for necessary civil services halted plans for a developed town. Instead, the area was inhabited by a small population of rural families during the first quarter of the 20th century (Oral History Associates 1986).

It was in 1929 that the community adopted the name "Brisbane" and experienced its first major growth phase. Throughout the 1930s, the residential area boomed due to its affordability, with 400 homes built between 1929 and 1933. By 1940, the town had grown to nearly 2,500 inhabitants, from a population of 28 in 1929. The City of Brisbane was incorporated in 1961 and the Baylands area was annexed into the City in 1962 (Oral History Associates 1986).

The Specific Plan Area is located on filled land reclaimed from tidal marshlands along San Francisco Bay. Bayshore Boulevard traces the approximate path of the original Bay shoreline. In the early 1900s, the Southern Pacific Railroad (SPRR) constructed railroad tracks across the Bay. After the 1906 San Francisco earthquake, the area west of the rail corridor was filled in primarily with demolition rubble.

The area east of the rail corridor was used as a municipal landfill site beginning in the 1930s. Starting from the north, dumping continued southward until it was finally stopped in the 1960s at the edge of what is now Brisbane Lagoon. The construction of US Highway 101 in the mid-1950s established the easternmost boundary of the Bay fill and created the Brisbane Lagoon south of the former landfill. The former Brisbane Landfill site is bounded by the Union Pacific/Joint Powers Board railroad corridor (Caltrain tracks) to the west, US Highway 101 to the east, and Brisbane Lagoon to the south. After the landfill operation was discontinued in 1967, a soil cover approximately 20 to 30 feet deep was placed over the site. Since the 1940s, a variety of uses have developed along Tunnel Avenue.

## **Southern Pacific Railroad**

The western portion of the Specific Plan Area is primarily associated with early-20th century railroad development. The San Francisco & San Jose Railroad (SF&SJRR) Company incorporated in 1861, and the railroad connecting the two cities was completed in 1864. SPRR bought out the SF&SJRR in 1868, around the same time that the owners of the Central Pacific Railroad, the “Big Four”—Leland Stanford, Collis Huntington, Charles Crocker, and Mark Hopkins—purchased the SPRR. The SF&SJRR was consolidated into the new SPRR in October 1870. The railroad expanded the agricultural economy of California and led to more innovative ways of shipping and preserving food supplies, such as transporting fruit and meat in refrigerator cars developed in 1880.

By 1890, rail traffic from San Francisco to San Jose had increased to four trains daily to San Jose and points beyond, as well as three trains to Menlo Park and back each day. E. H. Harriman became president of the SPRR in 1901 and initiated extensive improvements to the rail line, including the construction of the Bayshore Cutoff in 1904. A new level route that more closely followed the Bayshore was needed between San Bruno and San Francisco to eliminate the steep grade through Bernal Cut. In October 1904, construction of the Bayshore Cutoff began, and the work was done under the name of Bayshore Railway, a SPRR-held company. This line was one of the most expensive segments of railroad that had been built up to that time, costing almost a million dollars per mile for its 9.81 miles between San Francisco and San Bruno. The construction of this line was a difficult undertaking, as 20 percent of the route consisted of tunnels. The cut at Icehouse Hill, some 95 feet in depth, required removal of 750,000 cubic yards of material, which was used to fill in the inlet known as Visitacion Bay, north of the cut. Harriman directed his engineers to take the project several steps further than what earlier railroad planners had originally envisioned, as he was planning for future growth of the San Francisco Bay Area. The entire line was constructed with two main tracks, although it was designed to accommodate up to four tracks throughout the entire line, with the exception of four of the five tunnels. The line, which officially opened for service on December 8, 1907, shortened the distance between



San Bruno and San Francisco by four miles and eliminated Bernal Cut as a mainline, saving 17 minutes for commuters heading from San Jose to San Francisco. The western portion of the Project Area is located on land that was previously owned and occupied by the SPRR.

As part of the Bayshore Cutoff project, a modern freight terminal designed to replace the old machine shops at 16th and Harrison Streets and the car repair and roundhouse facilities at Mariposa Street on the old line in San Francisco was constructed on some 200 acres of fill at Visitacion Bay (see Figure 4.D-3 of the 2015 Final Program EIR). The new “Bayshore Yard,” some 8,400 feet long, included a roundhouse, machine and car shops, and a hump,<sup>1</sup> the second hump to be built on the West Coast. The Roundhouse at the Bayshore Yard was built circa 19072 to service freight locomotives.

The former Tank and Boiler Shop at the Bayshore Yard was built in 1920 to maintain and repair the iron boilers on steam locomotives (Southern Pacific 1920). Locomotives would originate from the Roundhouse, leaving from one of the rear doorways located in the northwestern portion of this building, stop at a transfer pit where the locomotive would slide horizontally, and align with one of approximately 15 tracks leading into the large, brick Machine and Erecting Building located approximately 300 feet north of the Tank and Boiler Shop. From there, the approximately 6,000-pound boilers would be hoisted off the locomotive(s) by an overhead gantry crane, placed on smaller rail cars, and transferred to the Tank and Boiler Shop through one of eight tracks leading into the building. In the Boiler Shop, the boiler’s internal flues would be dismantled and washed to eliminate the scale and mineral build-up that would accumulate on them and reduce their efficiency (Hart 2007). The nearby transfer pit, the hump, and the Machine and Erecting Building no longer exist. The former Tank and Boiler Shop operated until the 1950s, and in 1963 it was leased to the Lazzari Fuel Company for use as a charcoal warehouse, a use that continued until April 2024 when the building was destroyed in a fire.

The Visitacion Ice Manufacturing Plant, located at the southern end of the railroad yard, was constructed in 1924 as a Pacific Fruit Express Ice Manufacturing Plant to supply ice to the trains of the Pacific Fruit Express going in and out of San Francisco. The plant had a 90-ton daily production capacity and 2,300 tons of storage capacity, as well as an island platform of 10 car lengths for loading and unloading ice onto waiting trains when it was in use. The plant at Visitacion was constructed to replace the retired San Francisco-Mission Bay Ice Transfer Plant and served the Bay Peninsula north of Santa Clara. It was constructed using the standard layout plan and design for a Pacific Fruit Express ice manufacturing plant. The Visitacion Ice Manufacturing Plant was in operation between 1924 and 1955, coinciding with the heyday of steam locomotion and train transport of California agriculture. Ice manufacturing plants were central aspects of the support system necessary for the shipment of perishable produce. Refrigeration with ice allowed for transportation of perishable goods beyond the local market and was very important to the development of California as an agricultural supplier throughout the continent (Thompson 1992). The building was discontinued as an ice plant in 1955 and was bought by the Market Street Van & Storage Company by 1962. It currently houses Machinery & Equipment, Inc.

By 1950, approximately 75 other railroad maintenance shops and smaller structures had been constructed along the western edge of the railroad yard and clustered along Bayshore Boulevard. These buildings included a machine shop, a powerhouse, a coach repair shop, a freight car repair shop, a lumber shed, a storage shed, loading platforms, a tower at the north end of the yard, and thousands of linear feet of rail spurs. A station plan from 1950 identifies the Bayshore Yard and many of its associated structures, including the Roundhouse (see Appendix F.5 of the 2015 Final Program EIR).

By 1954, the SPRR had nearly completed the change from steam-powered locomotives to diesel power. In May 1954, heavy repair of steam locomotives ceased at the Bayshore Yard, and its shops were closed. Following its official closing, the Bayshore Yard remained busy for several years dismantling the now-defunct steam engines, and the yard became the major classification yard for the San Francisco Terminal. Upon arriving at Bayshore, freight trains had their cars switched into smaller segments, or “cuts,” destined to various industrial areas. From the Bayshore Yard, switch engines hauled the cuts of cars to the small yard in South San Francisco and to the Mission Bay yard at 16th and Bryant Streets in downtown San Francisco. After dropping off these inbound cars, engines would return to the Bayshore Yard with cuts of outbound cars that would eventually be made up into outbound trains.

Use of the freight yard ceased in the 1960s, and the yard was predominantly idle at the time of its purchase in the late 1980s by Tuntex, now Universal Paragon Corporation. Caltrain took over the Union Pacific rail line in the 1980s, and by 1989 nearly all of the railroad spur tracks and numerous other maintenance shops and smaller support structures had been removed.

## **Bayshore Boulevard**

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. “According to the 1913–1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse” (Carey & Company 2008).

## **Crocker Land Company**

The Crocker Land Company, a subsidiary of Crocker Estate Company, owned properties throughout California (Oral History Associates, Inc. 1989). The owner of the Crocker Estate Company was Charles Crocker, founder of Crocker Bank (Oral History Associates, Inc. 1989). “Its San Mateo County holdings included all of the undeveloped property on San Bruno Mountain and a large tract of tidelands on the east side of the Bayshore Freeway” (Oral History Associates, Inc. 1989). “The town of Brisbane was almost completely surrounded by Crocker-owned property” (Oral History Associates, Inc. 1989).

## **Incorporation**

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. Development pressure was also felt from the SPRR, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a subsidiary of the Crocker Estate Company (Oral History Associates, Inc. 1989).

Prior to incorporation, the area was under the control of the County of San Mateo (Oral History Associates, Inc. 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased the threat of annexation as the development would need to be annexed into one of the surrounding cities in order to provide services such as police, fire, water, and sewer (Oral History Associates, Inc. 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc. 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted to incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc. 1989). The subject property is east of the originally incorporated city and was annexed to Brisbane shortly after its incorporation.

## **Industrial Development**

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both SPRR and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History Associates, Inc. 1989). Some of SPRR's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc. 1989). The subject property is located on what was, in the early 1960s, lands owned by SPRR (Oral History Associates, Inc. 1989). Nearby Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the adjacent SPRR ceased operations.

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## CHAPTER 5

# Research Methods and Results

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### 5.1 Records Search

ESA received the results of a records search from the Northwest Information Center (NWIC) of the California Historical Resources Information System (CHRIS) on January 25, 2023 (File No. 22-1116). The purpose of the records search was to (1) determine whether known cultural resources have been recorded within or adjacent to the Project Area; (2) assess the likelihood for unrecorded cultural resources to be present based on historical references and the distribution of nearby sites; and (3) develop a context for the identification and preliminary evaluation of cultural resources. The records search consisted of an examination of the following documents:

- NWIC digitized base maps (USGS *South San Francisco, California* 7.5-minute topographic map), to identify recorded archaeological sites and studies within 0.5 mile of the Project Area.
- NWIC digitized base maps (USGS *South San Francisco, California* 7.5-minute topographic map), to identify recorded architectural resources (building, structures, and objects) within 0.5 mile of the Project Area.
- Resource Inventories: *California Inventory of Historical Resources*, *California Historical Landmarks*, *Built Environment Resource Directory* (BERD; OHP 2023) (through March 2020), and *Archaeological Determinations of Eligibility* (ADOE, as of April 2012) for San Francisco and San Mateo County.

#### 5.1.1 Previous Studies of the Project Area

The NWIC records search results identified thirty previous cultural resources studies that have included portions of the Project Area. The most relevant reports to this study, which provide details on subsurface sensitivity, and resources documented in the Project Area, and have been completed in the last 20 years, are identified in **Table 2**.

**TABLE 2**  
**PREVIOUS CULTURAL RESOURCES STUDIES THAT INCLUDE PORTIONS OF THE PROJECT AREA**

Study #	Title	Author	Date
S-029657	Archaeological Inventory for the Caltrain Electrification Program Alternative in San Francisco, San Mateo, and Santa Clara Counties, California	Wendy J. Nelson, Tammara Norton, Larry Chiea, and Reinhard Pribish	2002
S-035247	Historic Property Survey Report, Tunnel Avenue OH Bridge (Bridge Number 35C-0124), City of Brisbane, San Mateo County, California	Colin I. Busby	2004
S-030760	Archaeological Inventory for the South San Francisco Four-Tracking and New Station Project, Caltrain Peninsula Corridor Line, San Mateo County, California; From North of Tunnel Avenue in Brisbane, MP 6.1, to Colma Creek in San Bruno, MP 9.72	Laura Leach-Palm and Brian F. Byrd	2005
S-031824	Peninsula Corridor Joint Powers Board, South San Francisco Station and Track Work Project; Brisbane and South San Francisco, San Mateo County, California; Caltrans Mile Posts: 06.10 to 10.60	Rand F. Herbert, Christopher McMorris, Amanda Blosser, Julia Cheney, Shaine Klima, and Stephen Melvin	2006
S-036747	Finding of No Adverse Effect for the Peninsula Corridor Joint Powers Board, South San Francisco Station and Track Work Project, Brisbane, South San Francisco and San Bruno, San Mateo County, California, Caltrain Mile Posts: 06.10 to 10.60	Rand Herbert, Christopher Morris, and Cynthia Toffelier	2006
S-043111	Visitacion Valley Redevelopment EIR, San Francisco, California: Historic Resources Technical Report	Carey & Co., Inc.	2008
S-035899	Archaeological Investigation Report and Mitigation Plan for the Sunnydale Auxiliary Sewer Project, Visitacion Valley, San Francisco, California	Anthropological Studies Center, Sonoma State University	2009
S-048738	California High-Speed Train Project, Environmental Impact Report/Environmental Impact Statement, San Francisco to San Jose Section, Archaeological Survey Report, Technical Report [Draft]	Denise Jurich and Amber Grady	2011
S-045790	Final Archaeological Data Recovery Plan for Phase III Hazardous Materials Remediation, Visitacion Valley Redevelopment Program, Schlage Lock Property, City and County of San Francisco, California	William Self Associates, Inc.	2014
S-053226	Results of Archaeological Monitoring of the Data Gap Investigation of the San Mateo County portion of the Universal Paragon Corporation, Inc. Operable Unit-Brisbane, California (letter report)	Heather Price	2019
S-053227	Results of Archaeological Coring Plan, in conjunction, with the Data Gap Investigation of the Universal Paragon Corporation, Inc. Operable Unit 2-Brisbane, California (letter report)	Megan Watson	2019
N/A	Cultural Resources Technical Report for the Brisbane Data Gap Investigation Project in the City of Brisbane, San Mateo County, California	Alonso, Zingerella, and Johnson	2022

SOURCE: NWIC, 2023

## Previous Cultural Resources Analysis for Baylands Development

The 2015 Specific Plan for the Baylands site proposed to create a development of generally the same horizontal extent as the 2023 Specific Plan (Metis and ESA 2015). The 2015 Program EIR found that the previously proposed Project and the alternatives would have a significant impact on historical resources, particularly the SPRR Roundhouse. To mitigate potential impacts to a less-than-significant level, mitigation included: preparation of a plan to protect and stabilize the Roundhouse (Mitigation Measure 4.D-1a) and that the plans for any new buildings within 50 feet of the Roundhouse be reviewed by the City for architectural compatibility with the Roundhouse (Mitigation Measure 4.D-1b). As described above in the Project Description section, in 2020 Page & Turnbull drafted a Stabilization and Rehabilitation Plan for treatment of the Roundhouse, which fulfilled this original mitigation. Full implementation of the Plan would allow the Project to conform with the SOI Standards for Rehabilitation and would not result in the loss of historic integrity or negatively affect the building's eligibility for listing on the National Register.

The 2015 Program EIR identified the Machinery & Equipment Building (P-41-002720; former SPRR Ice Manufacturing Plant, located at 3401 Bayshore Blvd) as an existing historical resource, per the City of Brisbane General Plan. The previous EIR determined that project development could potentially cause an adverse effect to the building through changes to its historic setting, and recommended implementation of design guidelines to ensure that new development be compatible with the historic structure (including 50-foot setbacks, and appropriate height/volume/and material). The current Project includes open space in the area surrounding the Machinery & Equipment Building (P-41-002720), and as such would not result in any potential impacts to the historical setting through introduction of new development in proximity to the building. No other eligible historic structures or landscapes were identified within the current Project Area as a part of the 2015 Program EIR analysis.

The 2015 Program EIR also identified the South San Francisco Overhead (P-41-002721) as an existing historical resource, per the City of Brisbane General Plan. The 2015 Program EIR determined that the project's development of pedestrian and bicycle access through the former railroad tunnel would not physically alter the tunnel structure and would have no significant direct or indirect impact on the railroad tunnel as a historical resource.

The 2015 Program EIR also found that the Project and the alternatives would have a significant impact on archaeological resources and human remains. However, all of these potential impacts were able to be mitigated to a less-than-significant level with implementation of inadvertent discovery protocol (Mitigation Measures 4.D-2 and 4.D-3).

### 5.1.2 Previously Recorded Resources

The NWIC records search indicated that five previously recorded cultural resources are within or adjacent to the Project Area. The San Mateo BERD also has record of two resources in the Project Area that are not in the CHRIS database: the SPRR Bayshore Roundhouse and the SPRR Bayshore Turntable. The site records and National Register documentation for these resources are included in **Appendix B**. **Table 3** includes the name, brief description, and location in relation to the Specific Plan for all additional previously recorded resources within 0.5 mile of the Project

Area. Additional information about each resource is provided in the Cultural Resources Identified in Project Area section below. Twenty-eight additional cultural resources have been previously recorded within 0.5 mile of the Project Area. These are included as **Table A-1** in Appendix B.

**TABLE 3**  
**CULTURAL RESOURCES RECORDED WITHIN OR ADJACENT TO THE PROJECT AREA**

Primary #	Name	Resource Description	Latest Recorded By and Date	Eligibility	Location and Proximity to Project Area
P-38-005131	Schlage Lock / Ralston Mound	Multicomponent resource with pre-contact habitation and burial site and the remains of the historic-era Union Pacific Silk Manufacturing Company.  While unlikely the historic component of the resource extends into the Project Area, the pre-contact component of this [REDACTED] and the subsurface of this area has not been explored to confirm its exact boundary at depth.	Alonso, 2015	Not Evaluated for National Register or California Register	[REDACTED]
P-41-002160	FT-2	Historic-era artifact scatter	Martinez, 2009	Not eligible for National Register or California Register	[REDACTED]
P-41-002395	HST-33H	Historic-era artifact scatter	Martinez et al., 2009	Not Previously Evaluated	[REDACTED]
P-41-002706	AHC-178	Dense surface scatter of shell	Shoup, 2019	Not Previously Evaluated	[REDACTED]
P-41-002721	South San Francisco Overhead / Bayshore/Crocker Tunnel	Historic-era concrete arch tunnel	McMorris, Klima, and Cheney, 2005	Not eligible for National Register; Locally listed and considered a historical resource	Adjacent to Icehouse Hill Development District of Specific Plan Area.
N/A	SPRR Bayshore Roundhouse and Turntable Pit	Historic-era railroad roundhouse remains	Brandi, 2009	Listed on the National Register and Eligible for the California Register	Within Roundhouse Development District of Specific Plan Area.
N/A	Lazzari Charcoal Building / SPRR Tank and Boiler Shop*	Historic-era SPRR building	Informally documented in Metis and ESA, 2015	Not eligible for National Register of California Register	Within Roundhouse Development District of Specific Plan.

SOURCE: NWIC, 2023; OHP, 2023; Metis and ESA, 2015

\* The Lazzari Charcoal Building was destroyed in a fire in April 2024, and is not further considered as a cultural resource within this analysis



## 5.2 Native American Correspondence

ESA contacted the California Native American Heritage Commission (NAHC) on February 9, 2023 to request a search of their sacred land file and a list of Native American tribes in the vicinity who may have an interest in the proposed Specific Plan. On February 20, 2023, the NAHC responded stating that the file search was negative for sacred sites. The NAHC also provided contact information for eight tribal representatives from six Tribes for additional information.

This report does not evaluate tribal cultural resources within the Project Area. Tribal consultation for the Specific Plan was undertaken by the City of Brisbane and eight Tribes culturally affiliated with the Baylands Project Area. On August 3, 2022, a request for consultation was sent to the eight Tribes identified the NAHC. No Tribe requested formal consultation, nor did any Tribe identify any tribal cultural resources within the Project Area. **Appendix C** provides copies of the NAHC and Tribal correspondence.

## 5.3 Historic Map and Aerial Imagery Review

ESA conducted a review of the following sources of historic maps and aerial photography: USGS topographic quadrangles (USGS 1892, 1915, 1939, 1943, 1947, 1956, 1969, 1975, 1981, 1993, 1999), U.S. Bureau of Land Management General Land Office (GLO) plat maps (GLO 1866), National Oceanic and Atmospheric administration T-Sheets (NOAA 1854, 1932, 1944, 1953), and historic aerial photographs from [historicaerials.com](http://historicaerials.com) (NETR 1946, 1956, 1968, 1980, 1982). The review focused on the Project Area and vicinity. The Project Area had been nearly completely inundated by the San Francisco Bay, with potentially the western and northern edge being within the marshy tideland. In the late 1800s and early 1900s the western portion of the Project Area was filled in and the SPRR Bayshore yard was established. Between the 1930s and the 1960s the eastern portion of the Project Area was filled in and the James Lick Freeway, which is now part of U.S. 101, was constructed at the eastern edge of the Project Area. Over this time period, the western and northeastern portions of the Project Area were increasingly developed with industrial buildings. **Table 4** summarizes the results of the historic map and aerial photography review.

**TABLE 4**  
**HISTORIC MAP AND AERIAL IMAGERY REVIEW RESULTS**

Date	Type	Scale	Name	Features Depicted/General Notes
1854	T-Sheet	1:10,000	T-00460N	The Project Area is mostly inundated with the western edge potentially within the marshy tideland at the edge of the bay. The land to the west of the Project Area is labeled as 'Cañada del Visitacion'.
1866	GLO Plat	N/A	Township 3 South, Range 5 West, MDBM	Project Area is labeled as 'Lot No. 43. Part of Rancho Cañada de Guadalupe y Rodeo Viejo' to the north and the southern half is part of 'Lt No. 41. Part of Rancho Cañada, La Visitacion, y Rodeo Viejo'. No features are identified on the map.
1892	Topo	1:62,500	San Mateo, CA	Majority of Project Area is underwater with just the portion of Icehouse Hill not submerged. The eastern edge of the Project Area appears to be in marshy tidelands that may be periodically dry. A couple of structures may be visible in the vicinity of Industrial Way within Project Area.
1915	Topo	1:62,500	San Mateo, CA	Area between Bayshore Boulevard and the Caltrain alignment has been filled in. The Roundhouse is visible and there are several train track lines around the alignment of the Caltrain ROW. The tracks also cut through Icehouse Hill. Three buildings appear near alignment of Industrial Way within the Project Area.
1932	T-Sheet	1:10,000	T-04640	SPRR yard and the Bayshore Roundhouse are depicted. The Project Area is inundated east of the Caltrain ROW. The edge of the filled portion of the Project Area has a 'broken stone wall' along the shore.
1939	Topo	1:62,500	San Mateo, CA	Western portion of Project Area has been developed with several buildings and rail tracks as part of the SPRR Bayshore yard. Alignment of Industrial Way appears to match modern alignment. Northeast portion of Project Area (east of the Caltrain ROW) has been filled in and has two loops of track.
1943	Topo	1:62,500	San Mateo, CA	Same as 1939 topo map.
1944	T-Sheet	1:10,000	T-05920	SPRR Bayshore yard is depicted on map. Northeast portion of Project Area is filled in and labeled as 'Garbage Disposal Area'.
1946	Aerial Photo	n/a	n/a	Project Area is dominated by the SPRR Bayshore yard, which encompasses the western portion of the Project Area. The alignment of Industrial Way matches the modern alignment and there are several large warehouses along this road. The northern third of the area east of the Caltrain alignment has been filled in, with only roads visible in this area.
1947	Topo	1:24,000	San Francisco South, CA	The SPRR Bayshore yard is completely developed with a large swath of land completely covered in rail tracks. The SPRR Roundhouse and Turntable and the SPRR Tank and Boiler Shop are depicted on the map. Additional area east of the Caltrain ROW has been filled in, almost reaching Icehouse Hill. This area is not built out much, but there are a few roads and rail lines in this area.
1953	T-Sheet	1:10,000	T-11064	Portion of Project Area north and east of Icehouse Hill has been filled in and is labeled 'Garbage dump'. Northern edge of U.S. 101 Freeways appears to be under construction.
1956	Topo	1:24,000	San Francisco South, CA	Fill has been placed in the bay from Sierra Point up to Candlestick Cove, to construct the U.S. 101 Freeway, which is depicted on this map as, "(Under Construction)". The freeway construction forms the enclosed lagoon from the northern end of Icehouse Hill to the southern end of the Project Area.
1956	Aerial Photo	n/a	n/a	The western portion of the Project Area is the same as the 1946 aerial. The eastern portion of the Project Area has been filled in down to Icehouse Hill and the U.S. 101 Freeway appears to be under construction on the eastern edge of the Project Area.
1968	Aerial Photo	n/a	n/a	The lagoon has been filled up to its current size and the U.S. 101 Freeway appears completed. New buildings are visible in the northeast portion of the Project Area. A circular track is visible in the northeastern portion of the Project Area

Date	Type	Scale	Name	Features Depicted/General Notes
1969	Topo	1:24,00	San Francisco South, CA	The U.S. 101 Freeway has been completed and the lagoon has been filled in around Icehouse Hill, shrinking the lagoon to its current size. Additional buildings have been constructed in the northeast portion of the Project Area, including a circular 'Speedway' (or automobile racetrack).
1975	Topo	1:24,000	San Francisco South, CA	Additional buildings have been constructed in the northeast portion of the Project Area and around Industrial Way and at the end of Icehouse Hill.
1980	Aerial Photo	n/a	n/a	Buildings within the SPRR Bayshore yard have been removed as have some of the train tracks. The Speedway track in the northeastern portion of the Project Area has been removed.
1981	Topo	1:24,000	San Francisco South, CA	Project Area is increasingly developed with additional buildings along Bayshore Boulevard and additional roads in the eastern half of the Project Area. The auto racing facility has been removed.
1982	Aerial Photo	n/a	n/a	Same as 1980 aerial photo.
1993	Topo	1:24,000	San Francisco South, CA	Most of the SPRR features have been removed. Only the Roundhouse and Tank and Boiler Shop are depicted on the map in the area of the SPRR Bayshore yard. Buildings are still extant along Industrial Way and at the end of Icehouse Hill. All other previously depicted buildings in the Project Area are not present.
1999	Topo	1:24,000	San Francisco South, CA	Same as 1993 topo map.

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# CHAPTER 6

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## Pedestrian Survey

### 6.1 Architectural History Survey

#### 6.1.1 Methods

ESA architectural historians Kathy Cleveland and Becky Urbano completed a pedestrian survey on February 8, 2023. The purpose of this survey was to document those historic-age buildings and structures within the Project Area that had not been previously evaluated as potential historic resources. Eight parcels, consisting of 11 buildings (**Figure 4**), were surveyed and documented:

- 21–27 Industrial Way (APN 005-310-120)
- 60–130 Industrial Way (APN 005-311-070)
- 131 Industrial Way (APN 005-312-100)
- 140–190 Industrial Way (APN 005-312-110)
- 151–171 Industrial Way (APN 005-311-100)
- 200 Industrial Way (APN 005-311-020)
- 250–256 Industrial Way (APN 005-311-010)
- 290–296, 300–312, 340–374, and 380 Industrial Way (APN 005-320-020)

ESA architectural historian Amy Langford completed a pedestrian survey on June 13, 2024. The purpose of this survey was to document additional historic-age buildings and structures within the Project Area that had not been previously evaluated as potential historic resources. Eight parcels, consisting of 7 buildings (**Figure 4**), were surveyed and documented:

- 10 Industrial Way (APN 005-311-090)
- 36 Industrial Way (APN 005-311-120)
- 40 Industrial Way (APN 005-311-110)
- 2629–2635 Bayshore Boulevard (APN 005-340-110; 005-340-120)
- 595 Tunnel Avenue (APN 005-152-270)
- 601 Tunnel Avenue (APN 005-250-020)
- 950 Tunnel Avenue (APN 005-162-370)

## Results

ESA determined that none of the 15 newly identified potential historic resources (consisting of 18 historic-age buildings) listed qualify as historical resources under CEQA. Appendix B includes the Department of Parks and Recreation (DPR) 523 site records and evaluation forms for these resources. ESA also revisited the recorded location of the South San Francisco Overhead (P-41-002721), the SPRR Bayshore Roundhouse and Turntable Pit, which is listed on the National Register and eligible for the California Register, and the Machinery & Equipment Building / Visitation Ice Manufacturing Plant (P-41-002720), which are all considered locally eligible and as such are historical resources under CEQA. A summary of these resources and the findings of the pedestrian survey are provided below.

## 6.2 Archaeological Survey

### 6.2.1 Methods

ESA archaeologists Ashleigh Sims and Doug Alexander conducted spot checks of the four previously recorded archaeological resources within and adjacent to the Project Area on February 7, 2023. Since the surface of the Project Area is either fill or has been completely disturbed with the deconstruction of the SPRR Bayshore yard, the landfill, and the construction of other buildings, the pedestrian survey focused on relocating known sites and getting a general idea of the surficial soils of the Project Area. Specifically, the locations of P-38-005131, P-41-002160, P-41-002395, and P-41-002706 were inspected to determine if there had been any change to these resources from their last recording and to confirm that the resources were recorded and described in the correct locations. The results of each of the site-specific inspections are described in detail below.

## Results

P-41-002160 was generally found to be as previously recorded. No evidence of P-38-005131 was identified in the Project Area, however this area has been significantly disturbed on the surface and the resource has been the subject of extensive excavation that may have resulted in the removal or relocation of surficial cultural material. The recorded locations of P-41-002395 and P-41-002706 were revisited and neither of these resources were found to extend into the Project Area. A summary of the survey findings of P-41-002395 and P-41-002706 is below.

Figure 4 shows the locations of cultural resources identified within the Project Area as a result of the pedestrian survey. **Appendix D** provides photographs of the survey area.

### **P-41-002395**

This resource was recorded in 2009 by Martinez, Zelazo, and Moore as a historic-era artifact scatter

P-41-002395 was treated as eligible for the National Register as part of the San Francisco to San Jose California High-Speed Rail

Project (California High-Speed Rail Authority 2022). No effort was made to evaluate the resource and it was assumed eligible under Criterion D due to the potential for the resource to have a subsurface historic-era deposit that could yield information important in local and state history (Martinez, Zelazo, and Moore 2009).

ESA revisited the general location of the previously recorded resource [REDACTED]. No cultural material was identified in the vicinity [REDACTED]. P-41-002395 is [REDACTED] outside of the Project Area. The edge of the Project Area in the vicinity of the recorded location of P-41-002395 was not obscured by gravel and an intensive survey of this area identified no evidence of the resource. Therefore, this resource is not within the Project Area.

#### ***P-41-002706***

This resource is the reported location of a surface scatter of shell midden [REDACTED]. The resource is described as a loose loam with abundant snail, mussel, oyster, and clam shell and a single fish bone. The resource borders [REDACTED] the Project Area.

ESA revisited the recorded location of the resource during the current survey and relocated the shell material. During the current survey, ESA found that the shell scatter was at the location previously recorded. However, the soil does not appear to be midden and instead the shell appears to be natural shell deposited at the edge of the bay, where this location would have been prior to land reclamation efforts. In addition, the shell is intermixed with modern gravel and debris. The shell scatter is also highly disturbed [REDACTED]. The shell does not appear to be a cultural deposit.

The shell also does not appear to extend into the Project Area. The shell material is [REDACTED] not within the Project Area.

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## CHAPTER 7

# Cultural Resources Identified in Project Area

Through background research, six previously recorded cultural resources were identified within the Project Area: one pre-contact habitation site with historic-era artifact scatter (P-38-005131); one historic-era artifact scatter (P-41-002160); and four historic-era architectural resources (South San Francisco Overhead [P-41-002721], SPRR Bayshore Roundhouse and Turntable Pit, the Machinery & Equipment Building/Visitacion Ice Manufacturing Plant [P-41-002720], and the South San Francisco Overhead [P-41-002721]). During the field surveys conducted for the Specific Plan, P-41-002395 and P-41-002706, were relocated and found to have been previously mapped incorrectly and are not within the Project Area.

An additional ineligible resource, the Lazzari Charcoal Building / SPRR Tank and Boiler Shop, had been previously documented within the project area; however, the Lazzari Charcoal Building was destroyed by a fire in April 2024. It is not analyzed further as a historical resource in this report.

Additionally, during the architectural history survey, fifteen previously unrecorded historic-era architectural resources (consisting of 18 historic-age buildings) were recorded: 10 Industrial Way, 131 Industrial Way, 140–190 Industrial Way, 151–171 Industrial Way, 200 Industrial Way, 21–27 Industrial Way, 250–256 Industrial Way 36 Industrial Way, 40 Industrial Way, 60–130 Industrial Way, 290–296, 300–312, 340–374, and 380 Industrial Way, 2629–2635 Bayshore Boulevard, 595 Tunnel Avenue, 601 Tunnel Avenue, and 950 Tunnel Avenue. Previously unevaluated architectural resources are described and evaluated below. The locations of the 20 cultural resources identified in the Project Area as a result of the records search and pedestrian survey, including their previous California Register-eligibility status and recommendations of California Register-eligibility as a result of this study, are summarized in **Table 5**. **Table 5** also includes reference to the destroyed Lazarri Charcoal Building. While the entire Project Area was surveyed, the cultural resources identified in Table 5 all occur within the Specific Plan Area.

**TABLE 5**  
**CULTURAL RESOURCES IDENTIFIED IN THE PROJECT AREA**

<b>Primary</b>	<b>Type<sup>^</sup></b>	<b>Age/ Affiliation</b>	<b>Name: Description</b>	<b>Previous CR- Eligibility<sup>§</sup></b>	<b>Updated CR- Eligibility</b>
P-38-005131	Archae	Pre-contact and Historic	Schlage Lock / Ralston Mound: Multicomponent resource with pre-contact habitation and burial site and the remains of the historic-era Union Pacific Silk Manufacturing Company	UE	RE
P-41-002160	Archae	Historic	FT-2: Historic-era artifact scatter	DNE	DNE
N/A	Archit	Historic	SPRR Bayshore Roundhouse and Turntable Pit: Historic-era railroad roundhouse remains	DE	DE
N/A	Archit	Historic	Lazzari Charcoal Building / SPRR Tank and Boiler Shop: Historic-era SPRR building*	DNE	DNE
P-41-002720	Archit	Historic	Machinery & Equipment Building / Visitacion Ice Manufacturing Plant / Pacific Fruit Express Ice Mechanization Plan (3401 Bayshore Blvd): Historic-era industrial building	DE	DE
P-41-002721	Archit	Historic	South San Francisco Overhead / Bayshore/Crocker Tunnel	DE	DE
N/A	Archit	Historic	21–27 Industrial Way: Industrial building	UE	RNE
N/A	Archit	Historic	60–130 Industrial Way: Industrial building	UE	RNE
N/A	Archit	Historic	131 Industrial Way: Industrial building	UE	RNE
N/A	Archit	Historic	140–190 Industrial Way: Industrial building	UE	RNE
N/A	Archit	Historic	151–171 Industrial Way: Industrial building	UE	RNE
N/A	Archit	Historic	200 Industrial Way: Industrial building	UE	RNE
N/A	Archit	Historic	250–256 Industrial Way: Industrial building	UE	RNE
N/A	Archit	Historic	290–296, 300–312, 340–374, and 380 Industrial Way: Industrial building	UE	RNE
N/A	Archit	Historic	2629–2635 Bayshore Boulevard: Commercial building	UE	RNE
N/A	Archit	Historic	10 Industrial Way: Industrial building	UE	RNE
N/A	Archit	Historic	36 Industrial Way: Public utility building	UE	RNE
N/A	Archit	Historic	40 Industrial Way: Industrial building	UE	RNE
N/A	Archit	Historic	595 Tunnel Avenue: Industrial building	UE	RNE
N/A	Archit	Historic	601 Tunnel Avenue: Industrial building	UE	RNE
N/A	Archit	Historic	950 Tunnel Avenue: Industrial building	UE	RNE

<sup>^</sup> Archae-Archaeological, Archit-Architectural

<sup>§</sup> CR-California Register, UE-Unevaluated, DE-Determined Eligible, DNE-Determined Not Eligible, RE-Recommended Eligible, RNE-Recommended Not Eligible

\* The Lazzari Charcoal Building was destroyed in a fire in April 2024, and is not further considered as a cultural resource within this analysis.

## 7.1 Architectural Resources

### 7.1.1 SPRR Bayshore Roundhouse and Turntable Pit

The SPRR Bayshore Roundhouse and Turntable Pit consists of the remains of a semi-circular shaped, one-story brick building and turntable pit. The Roundhouse once contained 17 stalls for SPRR train engines and cars. A National Register nomination was prepared by Brandi (2009), and the property was listed on the National Register in 2010. ESA revisited the resource and found that it has been damaged by fire and vandalism and is currently fenced off from access. This report's cover photo exemplifies the current condition of the Roundhouse. The turntable was covered in dense vegetation and fenced off.

### 7.1.2 P-41-002720 – Machinery & Equipment Building / Visitacion Ice Manufacturing Plant

Constructed in 1924, this L-shaped brick building consists of three sections: two storage areas in the two-story square northern portion and the single-story rectangular southern section that was used as the tank and compressor room. The main rail line that served the ice manufacturing plant still exists and is located immediately east of the property boundary. The associated rail spur used by the ice manufacturing plant is still present, but it is no longer attached to the main line and is covered by concrete. The island platform for loading and unloading of ice onto waiting trains is also no longer extant. The building now provides storage space for Machinery & Equipment, Inc.

The main building is a hollow and pressed brick structure with wooden beams supporting the two-story portion and steel trusses supporting the single-story section. The exterior brick piers occur approximately every 15 feet, are about two feet wide, and project at least 18 inches from the main facade. The second-story portion of the building has a yellow brick cornice with decorative brick detailing and cast stone parapet caps. Along the southern portion of the building, these piers frame large rectangular windows made up of small panes of glass. The building's interior is still covered with the cork insulation that was installed during its use as an ice manufacturing plant and has undergone only minimal structural alteration.

### 7.1.3 P-41-002721 – South San Francisco Overhead / Bayshore/Crocker Tunnel

This resource is a reinforced concrete arch tunnel under Bayshore Boulevard that connects Crocker Industrial Park with the area east of Bayshore Boulevard. The tunnel was built in 1928 and was extended in 1951. The South San Francisco Overhead has been previously determined not eligible for the National Register (OHP 2023). However, the City of Brisbane General Plan identifies this resource as a locally eligible historical resource (Metis and ESA 2015).

ESA revisited the eastern entrance of the tunnel and found that the resource is as it was previously recorded, although surrounding vegetation is dense. NWIC mapping of the resource appears to greatly exaggerate its extent.

### 7.1.4 21–27 Industrial Way

The subject property is a 0.881-acre lot that includes a large warehouse building that was constructed in 1965. The building has an irregular footprint, a concrete slab foundation, and a low pitched, side gabled roof. The standing seam, metal clad walls sit atop concrete stem walls. The areas surrounding the building are paved with the area between the building and Industrial Way to the west stripped for parking.

#### Evaluation

The subject property at 21–27 Industrial Way had not been previously evaluated. In 2023, ESA staff evaluated the subject property for its potential historical significance under California Register Criteria 1 through 4. The subject property at 21–27 Industrial Way includes a light industrial building that has housed several businesses since it was constructed. The subject property and surrounding land were originally occupied by the SPRR. Industrial Way was subsequently redeveloped with a variety of light industrial buildings after SPRR no longer occupied the site in the 1960s. The building is a standard industrial building. Research does not support any association between the occupants of the building and any historically significant events or trends in the development of Brisbane. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. At 21–27 Industrial Way is a utilitarian building with no distinct architectural style. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property at 21–27 Industrial Way would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

#### Integrity Considerations

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. The subject property at 21–27 Industrial Way does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

#### Conclusions

As the property at 21–27 Industrial Way does not meet any of the California Register criteria, ESA recommends it ineligible for listing in the California Register.

### 7.1.5 60–130 Industrial Way

The subject property is a 1.912-acre lot that includes a large warehouse building that was constructed in 1968. The building has a rectangular footprint, a concrete slab foundation, and a

low pitched, side gabled roof. The standing seam, metal clad walls sit atop concrete stem walls. A loading dock is present along the full length of the east (primary) façade. The areas that surround the building are paved with the area between the building and Industrial Way stripped for parking.

## Evaluation

The subject property at 60–130 Industrial Way had not been previously evaluated. In 2023, ESA staff evaluated the subject property for its potential historical significance under California Register Criteria 1 through 4. The subject property at 60–130 Industrial Way is a light industrial building that has housed several businesses since it was constructed. The subject property and surrounding land were originally occupied by the SPRR. Industrial Way was subsequently redeveloped with a variety of light industrial buildings after SPRR no longer occupied the site in the 1960s. The building is a standard industrial building. Research does not support any association between the occupants of the building and any historically significant events or trends in the development of Brisbane. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. At 60–130 Industrial Way is a utilitarian building with no distinct architectural style. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property at 60–130 Industrial Way would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

## Integrity Considerations

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. The subject property at 60–130 Industrial Way does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

## Conclusions

As the property at 60–130 Industrial Way does not meet any of the California Register criteria, ESA recommends it ineligible for listing in the California Register.

### 7.1.6 131 Industrial Way

The subject property is a 0.43-acre lot that includes a large warehouse building that was constructed in 1969. The building has a rectangular footprint, a concrete slab foundation, and a low pitched, front gabled roof. The walls are clad in standing seam metal sheeting. The areas that surround the building are paved with the area between the building and Industrial Way to the west stripped for parking.

## Evaluation

131 Industrial Way had not been previously evaluated. In 2023, ESA staff evaluated the subject property for its potential historical significance under California Register Criteria 1 through 4. The subject property at 131 Industrial Way is a light industrial building that has housed several businesses since it was constructed. The subject property and surrounding land were originally occupied by the SPRR. Industrial Way was subsequently redeveloped with a variety of light industrial buildings after SPRR no longer occupied the site in the 1960s. The building is a standard industrial building. Research does not support any association between the occupants of the building and any historically significant events or trends in the development of Brisbane. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. 131 Industrial Way is a utilitarian building with no distinct architectural style. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property at 131 Industrial Way would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

## Integrity Considerations

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. 131 Industrial Way does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

## Conclusions

As the property at 131 Industrial Way does not meet any of the California Register criteria, ESA recommends it ineligible for listing in the California Register.

### 7.1.7 151–171 Industrial Way

The subject property is a 0.771-acre lot that includes a large warehouse building constructed in 1971. The building has an irregular footprint, a concrete slab foundation, and a low pitched, side gabled roof. The walls are clad in standing seam metal sheeting. The areas that surround the building to the north, west, and south are paved with the area between the building and Industrial Way to the east stripped for parking. The area to the east is a combination of paved area and vegetation.

## Evaluation

The subject property at 151–171 Industrial Way had not been previously evaluated. In 2023, ESA staff evaluated the subject property for its potential historical significance under California Register Criteria 1 through 4. The subject property at 151–171 Industrial Way is a light industrial

building that has housed several businesses since it was constructed. The subject property and surrounding land were originally occupied by the SPRR. Industrial Way was subsequently redeveloped with a variety of light industrial buildings after SPRR no longer occupied the site in the 1960s. The building is a standard industrial building. Research does not support any association between the occupants of the building and any historically significant events or trends in the development of Brisbane. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. At 151–171 Industrial Way is a utilitarian building with no distinct architectural style. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property at 151–171 Industrial Way would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

## Integrity Considerations

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. The subject property at 151–171 Industrial Way does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

## Conclusions

As the property at 151–171 Industrial Way does not meet any of the California Register criteria, ESA recommends it ineligible for listing in the California Register.

### 7.1.8 140–190 Industrial Way

The subject property is a 1.033-acre lot that includes a large warehouse building that was constructed in 1971. The building has an irregular footprint, a concrete slab foundation, and a low pitched, side gabled roof. The roof extends out of the east (primary) façade creating a wide eave. The walls are clad in standing seam metal sheets. The areas surrounding the building are paved with the area between the building and Industrial way stripped for parking.

## Evaluation

The subject property at 140–190 Industrial Way had not been previously evaluated. In 2023, ESA staff evaluated the subject property for its potential historical significance under California Register Criteria 1 through 4. The subject property at 140–190 Industrial Way is a light industrial building that has housed several businesses since it was constructed. The subject property and surrounding land were originally occupied by the SPRR. Industrial Way was subsequently redeveloped with a variety of light industrial buildings after SPRR no longer occupied the site in the 1960s. The building is a standard industrial building. Research does not support any

association between the occupants of the building and any historically significant events or trends in the development of Brisbane. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. At 140–190 Industrial Way is a utilitarian building with no distinct architectural style. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property at 140–190 Industrial Way would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

### **Integrity Considerations**

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. The subject property at 140–190 Industrial Way does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

### **Conclusions**

As the property at 140–190 Industrial Way does not meet any of the California Register criteria, ESA recommends it ineligible for listing in the California Register.

## **7.1.9 200 Industrial Way**

The subject property is a 0.619-acre lot that includes a large warehouse building constructed in 1965. The building is of concrete tilt-up construction with a rectangular footprint. The building has a bow truss roof behind a parapet wall. The areas surrounding the building to the north, west, and south are paved. There is a landscape strip between the building and the sidewalk nearly the full length of the east façade.

### **Evaluation**

ESA staff evaluated the subject property for its potential historical significance under California Register Criteria 1 through 4. The subject property at 200 Industrial Way is a light industrial building that has housed several businesses since it was constructed. The subject property and surrounding land were originally occupied by the SPRR. Industrial Way was subsequently redeveloped with a variety of light industrial buildings after SPRR no longer occupied the site in the 1960s. The building is a standard industrial building. Research does not support any association between the occupants of the building and any historically significant events or trends in the development of Brisbane. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal



associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. 200 Industrial Way is a utilitarian building with no distinct architectural style. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property at 200 Industrial Way would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

## **Integrity Considerations**

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. 200 Industrial Way does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

## **Conclusions**

As the property at 200 Industrial Way does not meet any of the California Register criteria, ESA recommends it ineligible for listing in the California Register.

### **7.1.10 250–256 Industrial Way**

The subject property is a 1.227-acre lot that includes a large (16,400 sf) warehouse building that was constructed in 1920. The building has a rectangular footprint and a side gambled roof. The walls are clad in corrugated metal sheets on approximately the top ¼ of the walls with stucco cladding below. The primary (east) façade consists of 10 bays with five of the bays including large, metal roll-up doors.

## **Evaluation**

The subject property at 250–256 Industrial Way had not been previously evaluated. In 2023, ESA staff evaluated the subject property for its potential historical significance under California Register Criteria 1 through 4. The subject property at 250–256 Industrial Way is a large industrial building, constructed in 1920, that was originally part of the SPRR complex of buildings. The warehouse was not part of the earliest 1907 construction of the freight yard but was constructed around the same time as the former Tank and Boiler Shop. The building has housed several businesses since SPRR vacated the property in the mid-twentieth century. Following the decline and eventual closure of the freight yard, Industrial Way was subsequently developed with a variety of light industrial buildings and the context of the area has changed significantly. Unlike the nearby SPRR Roundhouse, the warehouse at 250–256 Industrial Way was one of many buildings that supported, but would not be considered individually integral to, the workings of the freight yard. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. At 250–256 Industrial Way is a utilitarian building with no distinct architectural style.

The warehouse is a more common industrial building style and does not reflect distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property at 250–256 Industrial Way would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

## **Integrity Considerations**

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. The subject property at 250–256 Industrial Way does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

## **Conclusions**

As the property at 250–256 Industrial Way does not meet any of the California Register criteria, ESA recommends it ineligible for listing in the California Register.

### **7.1.11 290–296, 300–312, 340–374, and 380 Industrial Way**

The subject property is a 5.662-acre lot that includes four warehouse buildings that were constructed in 1971. All buildings consist of single-story warehouses with standing seam metal sheet exteriors and low-pitched side gabled roofs, arranged along the west side of Industrial Way.

## **Evaluation**

290–296, 300–312, 340–374, and 380 Industrial Way had not been previously evaluated. In 2023, ESA staff evaluated the subject property for its potential historical significance under California Register Criteria 1 through 4. The subject property includes four light industrial buildings that have housed several businesses since all four were constructed in 1971. This construction led to the extension and realignment of the south end of Industrial Way. The subject property and surrounding land were originally occupied by the SPRR. Industrial Way was subsequently redeveloped with a variety of light industrial buildings after SPRR no longer occupied the site. The building is a standard industrial building. Research does not support any association between the occupants of the building and any historically significant events or trends in the development of Brisbane. The buildings do not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. The buildings on the subject property are all utilitarian buildings with no distinct architectural style. They do not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

## Integrity Considerations

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. The subject property does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

## Conclusions

As the property does not meet any of the California Register criteria, ESA recommends it ineligible for listing in the California Register.

### 7.1.12 2629–2635 Bayshore Boulevard

The subject property is located on two lots (APN 005-340-110 and APN 005-340-120) measuring a combined 11,468 square feet that includes a commercial building constructed ca. 1930s – 1947 (2635 Bayshore) and a wood-frame addition constructed ca. 1950 – 1965 (2629 Bayshore). 2635 Bayshore Boulevard (APN 005-340-120) is a one-story building of wood-frame construction with a horizontal log veneer and a double gabled roof. 2629 Bayshore Boulevard (APN 005-340-110) is a wood-frame addition with wood board and stucco siding and a flat roof with an overhang.

## Evaluation

The property at 2629 and 2635 Bayshore Boulevard had been previously recorded in 2006 but had not been evaluated. In 2024, ESA staff evaluated the subject property for its potential historical significance under California Register Criteria 1 through 4. The subject property includes a commercial building and addition that have housed multiple businesses since their original construction. The commercial building appears to be an expansion of a contiguous office and saloon originally located on the site by at least 1914. It is a standard commercial building that has functioned as the site of various local commercial enterprises—as a saloon, office, restaurant, cocktail lounge, and statuary and garden center. The building was owned and occupied for the greatest length of time by members of the Silvestri family, who have continuously owned and occupied a portion of the subject building since 1964. However, there is insufficient evidence to suggest that the A. Silvestri Company is a particularly noteworthy local business enterprise or that any individual member of the Silvestri family achieved historical acclaim (beyond a prolonged stewardship of the property) while occupying the building. The buildings do not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. The buildings on the subject property are all vernacular buildings with several distinct but unexceptional architectural details. They do not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

## Integrity Considerations

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. The subject property does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

## Conclusions

As the property does not meet any of the California Register criteria, ESA recommends it ineligible for listing in the California Register.

### 7.1.13 10 Industrial Way

The subject property is a 0.528-acre lot that includes a small (8,400 sf) warehouse building that was constructed between 1960 and 1980. The building has a rectangular footprint and a low-pitched roof. The walls are clad in standing seam metal sheets and wood board siding. The primary (east) façade features a roll-up, metal, vehicular entrance and two unglazed pedestrian doors located beneath a wood slat overhang supported by brick posts.

## Evaluation

The property at 10 Industrial Way had not been previously evaluated. In 2024, ESA staff evaluated the subject property for its potential historical significance under California Register Criteria 1 through 4. The subject property includes a light industrial warehouse building that was constructed between 1968 and 1980 and appears to have continuously functioned as a commercial storage facility since its construction date. It is a standard warehouse property type that has likely been occupied by multiple businesses and commercial enterprises, although research failed to identify any longtime owners or occupants of the property. As a commercial property, 10 Industrial Way reflects an important period of economic and commercial growth in Brisbane after the city's incorporation in 1961, but not in an individually significant way. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. The building on the subject property is a common light industrial building with few distinct but unexceptional architectural details. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and is not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

## Integrity Considerations

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. The subject property does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

## Conclusions

As the property does not meet any of the California Register criteria, ESA recommends it ineligible for listing in the California Register.

### 7.1.14 36 Industrial Way

The subject property is a 0.193-acre lot that includes a one-story public utility building that was constructed in 1972. The building has a rectangular footprint and a flat roof. The walls are clad in brick siding that features a diamond-like design on the building's primary (west) façade. The primary (east) façade features a recessed entry enclosed by a metal gate. A poured concrete driveway leads to a roll-up, metal, garage entrance located at the building's north façade.

## Evaluation

The property at 36 Industrial Way had not been previously evaluated. In 2024, ESA staff evaluated the subject property for its potential historical significance under California Register Criteria 1 through 4. The subject property includes a public utility building that was constructed in 1972 as the Bayshore Sanitary District's Eric H. Carlyle Pump Station. It is a standard public utility property type that currently functions as an operational pump station as well as the District Headquarters of the Bayshore Sanitary District, which was established as a special district in 1925 to serve the wastewater processing needs of what would become the City of Brisbane and Daly City. As a public utility, 36 Industrial Way has historically been a key component of a larger regional wastewater infrastructure managed by the Bayshore Sanitary District, but not in an individually significant way. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. The Station bears the name of Eric H. Carlyle, who was presumably a longtime employee or associate of the Bayshore Sanitary District. However, preliminary research failed to uncover Carlyle's professional contributions to the District or San Mateo County government. As the building was constructed a year after Carlyle's death and therefore not associated with his productive life, it therefore is not recommended eligible for listing under Criterion 2. The building on the subject property is a standard public utility building with few distinct but unexceptional architectural details. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and is not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

## Integrity Considerations

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. The subject property does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

## Conclusions

As the property does not meet any of the California Register criteria, ESA recommends it ineligible for listing in the California Register.

### 7.1.15 40 Industrial Way

The subject property is a 0.289-acre lot that includes a one-story light industrial building that was constructed ca. 1956 – 1958. The building has a rectangular footprint and a tiled hipped roof. The walls are clad in brick siding and typical fenestration appears to consist of metal sash, sliding windows with transom lights. Partially visible from the public right-of-way are a pair of metal pedestrian doors on the building's primary (east) façade.

## Evaluation

The property at 40 Industrial Way had not been previously evaluated. In 2024, ESA staff evaluated the subject property for its potential historical significance under California Register Criteria 1 through 4. The subject property includes a light industrial building that was constructed ca. 1956 – 1958 and appears to have continuously functioned as a commercial and industrial space since that time. It is a standard light industrial property type that has likely been occupied by multiple businesses and commercial enterprises, although research failed to identify any longtime owners or occupants of the property. As a commercial property, 40 Industrial Way reflects an important period of economic and commercial growth in Brisbane after the city's incorporation in 1961, but not in an individually significant way. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. The building on the subject property is a common light industrial building with few distinct but unexceptional architectural details. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and is not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

## Integrity Considerations

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. The subject property does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

## Conclusions

As the property does not meet any of the California Register criteria, ESA recommends it ineligible for listing in the California Register.

### 7.1.16 595 Tunnel Avenue

The subject property is a 3.515-acre lot that includes two warehouse buildings that were constructed between 1956 and 1963. The buildings are of identical size and massing and feature a rectangular footprint. The warehouses are clad with corrugated metal siding and capped with front gable roofs covered with corrugated metal. The primary (west) façades feature large entrances with two corrugated metal sliding doors.

#### Evaluation

The property at 595 Tunnel Avenue had not been previously evaluated. In 2024, ESA staff evaluated the subject property for its potential historical significance under California Register Criteria 1 through 4. The subject property includes two light industrial warehouse buildings that were constructed between 1956 and 1963 by the Van Arsdale-Harris Lumber Company. The buildings are standard warehouse property types that appear to have continuously functioned as a commercial storage facility since their construction date. The Van Arsdale-Harris Lumber Company is associated with the development of the regional lumber industry in the years of rebuilding that followed the 1906 San Francisco earthquake. As a lumber warehouse, the subject property reflects an important period of economic and commercial growth in Brisbane after the city's incorporation in 1961, but not in an individually significant way. The buildings do not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. They are therefore not recommended eligible for listing under Criterion 2. The buildings on the subject property are common light industrial warehouse buildings with no architectural style. They do not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

#### Integrity Considerations

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. The subject property does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

#### Conclusions

As the property does not meet any of the California Register criteria, ESA recommends it ineligible for listing in the California Register.

### 7.1.17 601 Tunnel Avenue

The subject property is a 5.28-acre lot that includes a one-story light industrial building that was constructed ca. 1956 – 1962. The building is of wood-frame construction and features an irregular

footprint. The original volume is primarily clad with wood board and corrugated metal siding and capped by a double gabled roof. The building's west façade is dominated by a large addition clad with corrugated metal siding and capped with a flat roof.

## Evaluation

The property at 601 Tunnel Avenue had not been previously evaluated. In 2024, ESA staff evaluated the subject property for its potential historical significance under California Register Criteria 1 through 4. The subject property includes a light industrial building that was constructed ca. 1956–1962 and appears to have originally functioned as a commercial space for an auto sales company until 1968 and appears to have continuously functioned as a commercial and industrial space since that time. It is a standard light industrial property type that has been occupied by multiple businesses and commercial enterprises, including a flea market, the Sierra Point Lumber & Plywood Company and the Golden State Lumber Company. As a wholesale auto warehouse and, later, as a lumber sales warehouse, the subject property reflects an important period of economic and commercial growth in Brisbane after the city's incorporation in 1961, but not in an individually significant way. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. The building on the subject property is a common light industrial building with few distinct but unexceptional architectural details. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and is not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

## Integrity Considerations

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. The subject property does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

## Conclusions

As the property does not meet any of the California Register criteria, ESA recommends it ineligible for listing in the California Register.

### 7.1.18 950 Tunnel Avenue

The subject property is a 23.48-acre lot that includes a multiple-component petroleum pipeline facility constructed ca. 1969 known as the Kinder Morgan Brisbane Terminal (Terminal). The western portion of the Terminal contains 20 refined petroleum products tanks that individually hold a storage capacity ranging from 10,000 to 75,000 barrels. The eastern portion of the Terminal is comprised of a cluster of light industrial buildings, office buildings, sheds, and truck loading



stations. The following is based on the historical record and comparative analysis of extant regional terminals owned and operated by Kinder Morgan.

## Evaluation

The property at 950 Tunnel Avenue had not been previously evaluated. In 2024, ESA staff evaluated the subject property for its potential historical significance under California Register Criteria 1 through 4. The subject property includes an assemblage of refined petroleum product tanks, ancillary buildings, sheds, and truck loading stations constructed ca. 1969 by the Southern Pacific Pipe Line, Inc. When first constructed, the subject property served as one of 20 terminal facilities owned and operated by Southern Pacific Pipe Line, Inc. At the time, the “Southern Pacific Tank Farm” was one component of the Southern Pacific Railroad’s first foray into the domestic transportation of refined products between Arizona and California. By the 1970s, the Southern Pacific pipeline system was comprised of approximately 2,500 miles of product pipeline and 50 associated stations and facilities. As such, the Brisbane Terminal has historically been a key component of a larger domestic petroleum product transportation infrastructure managed by Southern Pacific Railroad, but not in an individually significant way. The Terminal does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. The Terminal on the subject property is a standard petroleum pipeline facility with no unifying architectural style. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and is not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

## Integrity Considerations

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. The subject property does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

## Conclusions

As the property does not meet any of the California Register criteria, ESA recommends it ineligible for listing in the California Register.

## 7.2 Archaeological Resources

### 7.2.1 P-38-005131

This resource has two components: a pre-contact habitation site with midden and burials, known as the Ralston Shellmound, and the historic-era remains of the Union Pacific Silk Manufacturing Company. [REDACTED] The Department of

Parks and Recreation (DPR) documentation for this resource does not show that it extends into the Project Area, however recent coring by PaleoWest (Alonso, Zingerella, and Johnson 2022) found midden in the Project Area that likely is associated with the pre-contact component of P-38-005131. The pre-contact component of the resource consists of what was likely two shellmounds [REDACTED]

[REDACTED]. The historic-era component consists of the remains of a silk ribbon factory that employed Chinese workers in the late 19th century.

## Evaluation

Based on the previous work on the resource [REDACTED] and due to the presence of human remains, this resource is recommended eligible for listing in the California Register under Criterion 1 (for its association with historical events) and 4 (for its data potential) and is therefore considered a historical resource for the purposes of CEQA (WSA 2011).

It is unclear from the results of the geotechnical study of a portion of the Plan Area [REDACTED] if the midden identified is an intact deposit, redeposited material from tidal wave action, or part of historic-era reclamation and landscaping activities (Alonso, Zingerella, and Johnson 2022). The extent and depth of the resource within the Project Area has not yet been tested because Project design has not been finalized and, therefore, the depth and extent of potential disturbance to this resource by the Specific Plan is currently unknown. Mitigation Measures **MM-CR-3, Archaeological Testing, Evaluation, and Treatment**, and **MM-CR-4, Site-Specific Mitigation for P-38-005131** (see Archaeological Resource Mitigation Measures) establishes a procedure to identify, evaluate, and treat cultural resources within the Project Area once more details regarding the depth and extent of Project-related ground disturbance is known.

### 7.2.2 P-41-002160

This resource is a scatter of historic-era artifacts identified by Leach-Palm and Thorpe in [REDACTED]  
[REDACTED]  
[REDACTED]. The resource includes glass and ceramic fragments dating to the late 19th and early 20th centuries. During the current pedestrian survey, ESA relocated the artifact scatter. The [REDACTED] scatter has been disturbed with the installation of riprap [REDACTED]  
[REDACTED]. Historic-era material was identified around the vegetation in the northern portion of the site within what appears to be a push pile.

## Evaluation

Previous evaluation and concurrence from the SHPO identified the resource as not eligible for the National Register or the California Register (OHP 2012:FTA040913A, Metis and ESA 2015:4.D-5). ESA concurs with the previous evaluation and determinations of this resource. Therefore, this resource is not considered a historical resource or unique archaeological resource for the purposes of CEQA.

## CHAPTER 8

# Assessment of Impacts to Identified Historical Resources and/or Unique Archaeological Resources

As a result of the records search, pedestrian survey, and resource evaluations, there are four historical resources in the Project Area: P-38-005131 (pre-contact habitation site and historic-era artifact scatter), P-41-002720 (Machinery & Equipment Building), P-41-002721 (South San Francisco Overhead), and the SPRR Bayshore Roundhouse and Turntable. **Figure 5** depicts the historical resources within the Project Area. Ground disturbing activities associated with the Specific Plan may result in significant impacts to the archaeological site, P-38-005131. Impacts to this resource can be mitigated to a less-than-significant level with implementation of the Archaeological Resource Mitigation Measures (below).

The Specific Plan as proposed is not anticipated to result in significant impacts to architectural resources. As described above, the 2015 Program EIR identified the Machinery & Equipment Building (P-41-002720) as an existing historical resource per the City of Brisbane General Plan. The 2015 Program EIR determined that project development could potentially cause an adverse effect to the building through changes to its historic setting, and recommended implementation of design guidelines to ensure that new development be compatible with the historic structure (including 50-foot setbacks, and appropriate height/volume/and material). The current Project includes open space in the area surrounding the Machinery & Equipment Building (P-41-002720), and as such would not result in any potential impacts to the historical setting through introduction of new development in proximity to the building.

The South San Francisco Overhead (P-41-002721) is a former SPRR tunnel located beneath Bayshore Boulevard that once connected the freight yard to today's Crocker Business Park with a single-track railroad spur. This resource is a reinforced concrete arch tunnel under Bayshore Boulevard that connects Crocker Industrial Park with the area east of Bayshore Boulevard. The tunnel was built in 1928 and was extended in 1951. The South San Francisco Overhead has been previously determined not eligible for the National Register (OHP 2023). However, the City of Brisbane General Plan identifies this resource as a locally eligible historical resource (Metis and ESA 2015). The 2015 Program EIR determined that project development of pedestrian and bicycle access through this former railroad tunnel would not physically alter the tunnel structure and would have no significant direct or indirect impact on the railroad tunnel as a historical resource. Similarly, the current Project does not propose any physical alterations of the tunnel, and as such would also have no significant direct or indirect impact on the railroad tunnel as a historical resource.

In 2020, Page & Turnbull (P&T) prepared a Stabilization and Rehabilitation Plan (S&R Plan) for the SPRR Bayshore Roundhouse. The report provided: diagrams of the structure, identified its character defining features, an assessment of its current condition, and recommendations for stabilization and treatment. In this report, P&T determined that lifting the SPRR Bayshore Roundhouse six to eight feet to accommodate sea level rise is physically feasible and will not compromise its status as a National Register-listed property. P&T recommended severing the building from its existing concrete slab, lifting the building, and setting it down upon a new concrete slab supported by new stem walls and compacted fill. P&T recommended this method over dismantling and reconstruction, both from a cost and preservation standpoint. Following relocation of the Roundhouse, at the developer's cost, the building would be adaptively reused as a community space (such as a community center, museum, or open-air theater) (Page & Turnbull 2020).

As described, the proposed plan would comply with the Secretary of the Interior's Standards for Rehabilitation and prevent any significant impact to the SPRR Bayshore Roundhouse and Turntable as a historical resource. As part of this analysis, digital photographs meeting the National Register of Historic Places Nomination requirements were taken to provide a baseline contemporary record of the resource (**Appendix E**). Implementation of these mitigations would result in a less than significant impact. Impacts to this resource may be reduced to a less-than-significant level with implementation of the Architectural Resource Mitigations Measures (below). Therefore, the Specific Plan may significantly impact significant architectural and archaeological resources. However, implementation of all recommended mitigation measures would potentially reduce the impact to a less-than-significant level.

## CHAPTER 9

# Archaeological Sensitivity Assessment

This report uses the term “sensitivity” to discuss this relationship, whereby an area with high sensitivity would be an area with both a high likelihood of encountering archaeological deposits and a high likelihood of any such deposits being significant (i.e., qualifying as an historical resource, for CEQA purposes). Overall, the Project Area has a high sensitivity for buried pre-contact cultural material below the fill within native soil, and at the surface along the northern and western edge of the Project Area, which was on the edge of the marshland prior to the placement of artificial fill. The depth of fill varies considerably within the Project Area due to slope of the soils and bedrock prior to land reclamation efforts, which required more fill to be placed in the eastern portion of the Project Area, which had a historical lower elevation (Geosyntec 2021a; Geosyntec 2021b). The western portion of the Project Area also has a high sensitivity for surficial or shallow historic-era cultural deposits. The area east of the Caltrain ROW has, in general, lower sensitivity as the artificial fill is very thick here due to the slope of the natural topography in this area. Additionally, the landfill and the lagoon are unlikely to contain intact historic-era archaeological resources.

## 9.1 Recent Geotechnical Studies of the Specific Plan Area

Since development of the 2015 Program EIR, PaleoWest (2022) participated in monitoring geotechnical coring work within [REDACTED] the Specific Plan Area where 688 cores were excavated [REDACTED]. Historic-era cultural materials (such as glass, metal, and ceramic fragments) were identified in 173 of the cores. Shell (possibly culturally modified deposits) was identified in 23 cores, 12 of which showed evidence of being disturbed by modern or historic-era material. Overall, PaleoWest recommended that additional archaeological testing be conducted to determine the extent, thickness, and consistency of the cultural deposits identified during the coring, particularly [REDACTED] near P-38-005131 [REDACTED] (Alonso, Zingerella, and Johnson 2022).

Two geotechnical reports have also been prepared for the Specific Plan Area: one for the area between Bayshore Boulevard, the Caltrain Right-of-Way (ROW), the San Mateo County border, and Icehouse Hill (the work monitored by PaleoWest, referenced above) and one for the area east of the Caltrain ROW and north of the lagoon. Both of these reports provide several cross-sections of various portions of Specific Plan Area based on the results of the geotechnical work and provide descriptions of the stratigraphy (ENGEO 2022a; ENGEO 2022b).

## 9.2 Pre-Contact Archaeological Resources

Landforms that predate the earliest estimated periods for human occupation of the region are considered to have very low potential for the presence of buried archaeological sites, while those that postdate human occupation are considered to have a higher potential for buried archaeological sites. Currently, archaeological research indicates that the earliest evidence for human occupation of California dates to the Late Pleistocene, which ended approximately 11500 BP. Therefore, the potential for buried archaeological deposits in landforms from or predating the Late Pleistocene is very low (Meyer and Rosenthal 2008:160–161).

As previously mentioned, the Project Area is mostly situated on a modern artificial fill which was placed in the Baylands between the late 1800s and the mid-1900s (Bonilla 1998; USDA 2022). Along the northwestern edge of the Project Area the surficial geology consists of artificial fill over tidal flats and Bay Mud is present at the southern end of the lagoon. At Icehouse Hill, the surficial geology consists of Cretaceous and Jurassic period sandstone and shale associated with the Franciscan Complex, with small areas of Pleistocene-age slope debris and ravine fill at the edge of the point where the weathered bedrock has collected. A small area at the northeastern corner of the Project Area nearest to Candlestick Point features Cretaceous and Jurassic-period greenstone at the surface (Bonilla 1998). Mapped surficial soils mirror surficial geology with artificial fill (Urban land-Orthents) covering much of the Project Area with Candlestick-Kron-Buriburi loams at Icehouse Hill.

Based on the soil type and bedrock present at the surface of the Project Area, the potential for previously unrecorded surficial pre-contact archaeological deposits is low. Any surficial pre-contact cultural material would likely be encountered in a redeposited, disturbed context due to the Project Area being primarily artificial fill at the surface. However, the context for buried pre-contact archaeological deposits is more complex. The natural topography and bathymetry (i.e., elevations above and below sea level) in the Project Area, prior to historic-era land reclamation efforts, sloped downward toward the San Francisco Bay. Sea levels, and therefore the level of San Francisco Bay, were dramatically lower when humans first arrived in the area. Human occupation may have occurred deep below the artificial fill of the Project Area 10,000 years ago when the Bay was much shallower, and therefore there is the potential that there are intact sites at this depth below the fill (**Figure 6**). Archaeologically sensitive buried landforms include land surfaces that lay exposed after the postulated date of arrival of humans in this area during the terminal Pleistocene (possibly as much as 10,000 to 12,000 years ago) and were subsequently inundated by rising seas during the Early (11,550 to 7,650 years before present [BP]) and Middle Holocene (7,650 to 3,750 BP) and buried by sediments, such as Young Bay Mud, as the sea level rose. Approximately 12,000 years BP, a warming climate trend caused global sea levels to rise rapidly with the melting of ice associated with the Wisconsin glacialiation; by 10,000 years BP, rising sea levels had connected the bay with the Pacific Ocean and established estuary conditions. Approximately 8,000 years BP, the Bay shoreline was approximately where the modern built shoreline is now, just east of the Highway 101 alignment. Combined with rising water levels, increased sedimentation into streams and rivers (Helley et al. 1979) resulted in the formation of marshes and tidal flats during the mid-Holocene. By 2,000 years BP, the Bay had inundated the eastern half of the Project Area, much of which had formed into tidal marshlands (Meyer and

Brandy 2019). The main high points above the water in the Project Area would have been at Icehouse Hill. As sea level rose, any potential archaeological deposits that formed when the area was previously dry land and accessible for human occupation would have been covered with tidal and bay deposits, both submerging and burying the deposits. Land reclamation efforts in the late 1800s and 1900s are unlikely to have disturbed these deposits if they had been sufficiently buried by estuarine and tidal deposits.

Because human populations have grown since the arrival of the area's first inhabitants, areas that have been exposed on the surface longer are more likely to yield archaeological resources than areas that had been inundated earlier in time. Based on the timing of sea level rise and shoreline models, the potential for significant buried pre-contact archaeological resources is highest in areas closest to the historic-era shoreline, with decreasing sensitivity towards the bay.

This analysis is corroborated by the pre-contact sensitivity model developed by Far Western which covers parts of the Project Area and indicated low to very high sensitivity for buried resources, with increasing sensitivity in a landward direction (Meyer and Brandy 2019). Midden soils identified by PaleoWest during coring of [REDACTED] the Project Area (Alonso, Zingerella, and Johnson 2022) may represent intact, buried archaeological deposits that confirm this sensitivity assessment; alternatively, however, this material could represent redeposited midden [REDACTED] that had eroded into the tidal marsh or was disturbed as part of land reclamation in the Project Area.

While the slope of the pre-contact shoreline is sensitive for pre-contact resources, it is unlikely to be impacted by the Specific Plan on the eastern side of the Caltrain ROW, where the pre-contact



**Photo 1. Overview of Specific Plan Area from Icehouse Hill, 1905. View north. The in-progress railroad grade through the middle of the photo is the Bayshore Cutoff and would become the western edge of the SPRR Bayshore yard. The hill in the back right is Candlestick Point. (OpenSFHistory.org 2023)**

shoreline would be significantly deeper than the landfill and below Bay Mud sediments that were deposited as the shoreline rose with sea level. The exact depth of native soils sensitive for pre-contact resources varies throughout the Project Area as the exact slope of the pre-contact shoreline is unknown. This uncertainty and variation in pre-contact sensitivity of the Plan Area are addressed in the Archaeological Resource Mitigation Measures (below).

### 9.3 Historic-era Archaeological Resources

As previously stated, the Project Area was subject to intensive land reclamation efforts between the late 1800s and the mid-1900s. During this time, artificial fill was placed in the Project Area to turn the tidal marshland into useable land. This effort was conducted in order to build the SPRR Bayshore yard and the Brisbane Landfill, which dominated the landscape of the Project Area in the first half of the 20th century (see **Photo 1** and **Photo 2**). The alignment of the Bayshore Freeway, which is also Highway 101 in the Project Area, was moved west, into the bay in the 1950s. Fill was dumped directly into the bay to raise the elevation so the freeway could be built (Travis 1957). Since the majority of the Project Area was reclaimed with fill, these fill deposits may contain cultural material from its original location. However, this material would not be in-



**Photo 2. Overview of the SPRR Bayshore yard from Icehouse Hill, 1947.**  
Similar viewpoint of Photo 1. View north. (OpenSFHistory.org 2023)

situ and would not have sufficient integrity to be a significant deposit. Significant historic-era cultural deposits in the Project Area are likely to be focused in the area of the SPRR Bayshore yard and the industrial area on the eastern edge of the Project Area, which was the first part of the Project Area to be raised. Cultural deposits associated with the SPRR Bayshore yard would be on top of the fill near the surface.



# CHAPTER 10

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## Conclusions

Based on the results of the records search, background research, pedestrian survey, resource significance evaluations, and assessment of impacts, 21 cultural resources were identified within the Project Area: one pre-contact habitation site with historic-era artifact scatter P-38-005131, one historic-era artifact scatter P-41-002160, and 19 historic-era architectural resources: SPRR Roundhouse, SPRR Tank and Boiler Shop, Machinery & Equipment Building P-41-002720, South San Francisco Overhead P-41-002721, 10 Industrial Way, 21–27 Industrial Way, 36 Industrial Way, 40 Industrial Way, 60–130 Industrial Way, 131 Industrial Way, 140–190 Industrial Way, 151–171 Industrial Way, 200 Industrial Way, 250–256 Industrial Way, 290–296, 300–312, 340–374, and 380 Industrial Way, 2629–2635 Bayshore Boulevard, 595 Tunnel Avenue, 601 Tunnel Avenue, and 950 Tunnel Avenue.

As a result of the study, four resources: P-38-005131, P-41-2720 Machinery & Equipment Building, P-41-002721 South San Francisco Overhead, and the SPRR Bayshore Roundhouse and Turntable are recommended or determined eligible for the California Register and are treated as historical resources for the purposes of the Specific Plan. All other resources in the Project Area (P-41-002160, SPRR Tank and Boiler Shop, 10 Industrial Way, 21–27 Industrial Way, 36 Industrial Way, 40 Industrial Way, 60–130 Industrial Way, 131 Industrial Way, 140–190 Industrial Way, 151–171 Industrial Way, 200 Industrial Way, 250–256 Industrial Way, 290–296, 300–312, 340–374, and 380 Industrial Way, 2629–2635 Bayshore Boulevard, 595 Tunnel Avenue, 601 Tunnel Avenue, and 950 Tunnel Avenue) are recommended or determined not eligible for the California Register and are not historical resources.

This study also concludes that the Project Area has a high sensitivity for buried pre-contact cultural deposits in native soils within the Project Area and a high sensitivity for surficial or shallow historic-era cultural deposits, particularly west of the Caltrain ROW. Pre-contact sensitivity at the surface is also high along the northern and eastern edge of the Project Area which was on the edge of the marshland prior to the placement of artificial fill. The historical resources identified in the Project Area may be impacted by the Specific Plan.

Impacts to architectural resources as a result of the Specific Plan are anticipated to be less than significant with the implementation of the S&R Plan, which conforms to the mitigation provided in the 2015 Program FEIR regarding potential impacts to historical resources (Metis and ESA 2015).

To address impacts to archaeological resources, the following mitigation has been included: cultural resources awareness training of construction personnel, site-specific mitigation for P-38-005131, and archaeological testing, evaluation, and treatment for activities that may result in

impacts to currently unknown archaeological resources, in addition to mitigation regarding the protocol for inadvertent discovery of archaeological resources and human remains. While the inadvertent discovery protocol is included in the 2015 Program EIR, the results of new documentation and reporting on P-38-005131, as well as the results of geotechnical investigations, and archaeological monitoring of those investigations, have provided additional information about the archaeological sensitivity of the Project Area. This additional information required a re-evaluation of the Project Area's archaeological sensitivity (which is included above) and to add additional mitigation measures from the 2015 Program EIR.

## 10.1 Architectural Resource Mitigation Measures

The Specific Plan is not anticipated to cause significant impacts to architectural historical resources. The SPRR Bayshore Roundhouse and Turntable, a historical resource, may be impacted by the construction of buildings and infrastructure within the Specific Plan site. Vibration related to construction activities is not anticipated to result in a significant impact to the Roundhouse, as the nearest potential for pile driving to the roundhouse location is 300 feet to the south of the northern boundary of the Icehouse District. At this anticipated distance, conservative estimates using the upper end of vibration generation for pile driving is 1.518 PPV at 25 feet, which would result in a peak vibration level of 0.04 PPV at the roundhouse location. Caltrans' threshold for extremely fragile historic buildings requires a maximum of 0.12 PPV for transient vibration sources and 0.08 PPV for frequent sources. As such, construction related vibration is not anticipated to result in an impact to the Roundhouse as a historical resource. Additionally, the 2020 Page & Turnbull Stabilization and Rehabilitation Plan includes protective language regarding initial mothballing/stabilization of the Roundhouse, and seismic retrofit according to current CA Historic Building Code after the relocation is completed.

As part of the proposed project, Page & Turnbull's 2020 Stabilization and Rehabilitation Plan (S&R Plan) shall be implemented. As described above, this five-stage plan includes securing the site from vandalism and hazards prior to implementation of remaining steps; initial stabilization or "mothballing" of the site to prevent further damage and deterioration; raising and regrading the site; secondary stabilization including initial retrofitting; and final restoration and retrofit to extend the life of the Roundhouse and provide appropriate seismic safety for use and occupancy.

The SOI Standards for Rehabilitation include ten criteria, including use, maintenance of historic materials, preventing the creation of a false sense of history, retention of changes that are significant in their own right, preservation of distinctive materials, repair of damaged historic features, prevention of damage resulting from chemical or physical treatments, preservation of archaeological resources (not applicable), complimentary additions or alterations, and potential to remove any new additions while maintaining the building's essential form and integrity. As described above, the proposed rehabilitation of the Roundhouse, as currently designed, is in full compliance with eight of the ten Standards for Rehabilitation and is in partial compliance with two of the Standards. Overall, the proposed Project is in compliance with the Standards for Rehabilitation and would not result in the loss of historic integrity or negatively affect the building's eligibility for listing on the National Register. Full implementation of this plan would result in a less-than-significant impact on historic built resources.

## 10.2 Archaeological Resource Mitigation Measures

The Specific Plan may cause a significant impact to archaeological historical resources and may disturb human remains. There is a known archaeological historical resource, P-38-005131, which may be significantly impacted by the Specific Plan and there is the potential for currently unknown archaeological historical resources to be impacted by the Specific Plan. To address these potential impacts, **MM-CR-2, Cultural Resources Awareness Training; MM-CR-3, Archaeological Testing, Evaluation, and Treatment; MM-CR-4, Site-Specific Mitigation for P-38-005131; and MM-CR-5, Inadvertent Discovery of Cultural Resources**, have been developed. These measures require that, prior to Project construction, an Archaeological Testing Plan be established to clarify the depth of fill and the sensitivity of the Project Area for archaeological resources, and to determine if P-38-005131 has a subsurface component within the Project Area. Mitigation measures have also been included that require a cultural resources awareness training be provided for all construction personnel involved in ground-disturbing work and that archaeological monitoring be conducted in all areas identified as sensitive as a result of the archaeological testing. While highly unlikely that archaeological resources would be found in the landfill or disturbed portions of the Project Area, it has become an industry standard practice that an awareness training be conducted for all construction personnel who are involved in ground disturbance. A mitigation measure has also been included to establish a procedure to follow in the event of an unanticipated discovery of archaeological resources. These mitigation measures ensure that impacts to archaeological historical resources are less than significant.

The Specific Plan may also disturb human remains. While there are no known human remains within the Project Area. There have been human remains identified in nearby archaeological resources and there is the potential for currently unknown human remains within the Project Area. To address this impact, **MM-CR-6, Inadvertent Discovery of Human Remains**, establishes a protocol to follow on the discovery of human remains or potential human remains. Mitigation measures are included in the Project EIR.

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# CHAPTER 11

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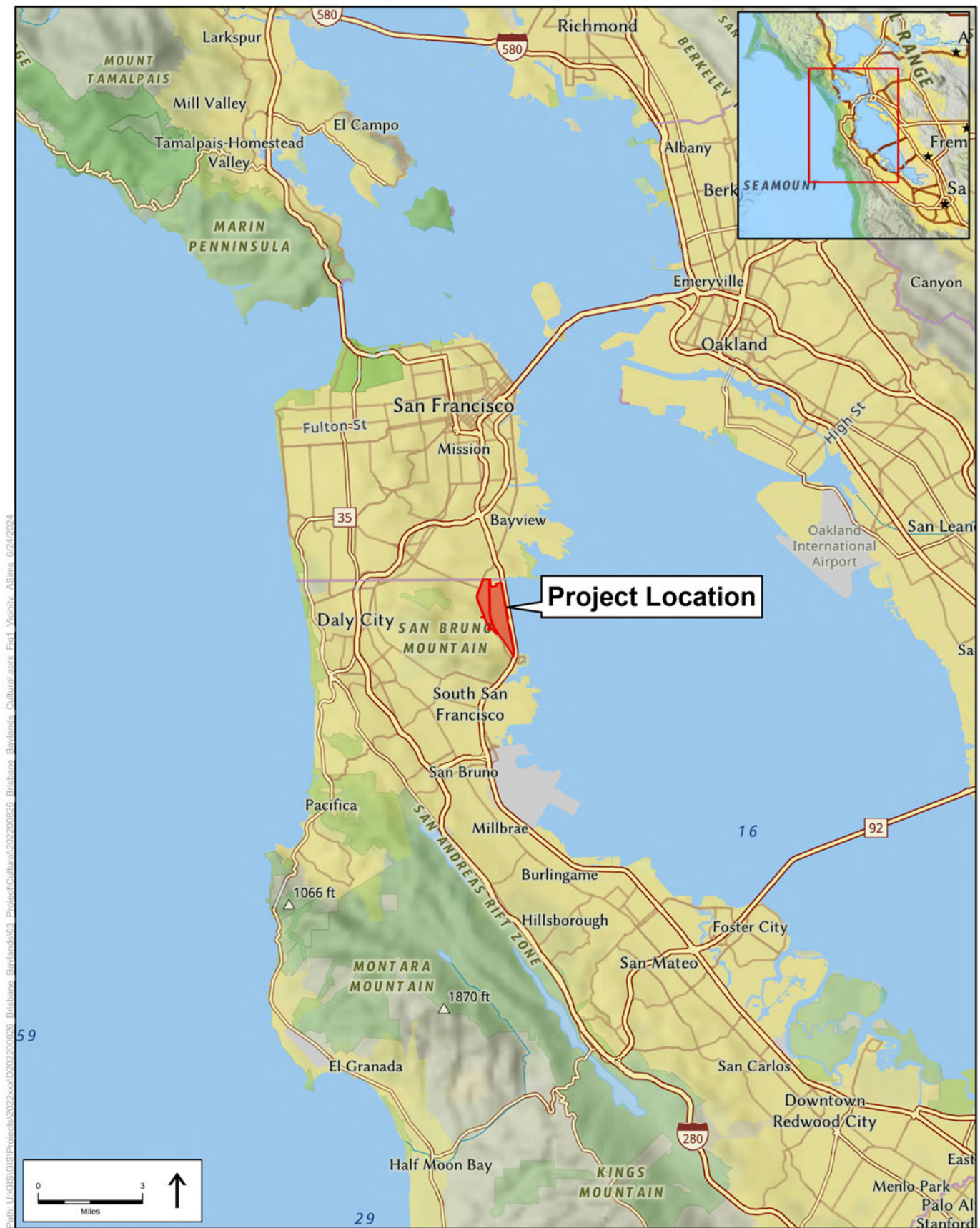
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# FIGURES

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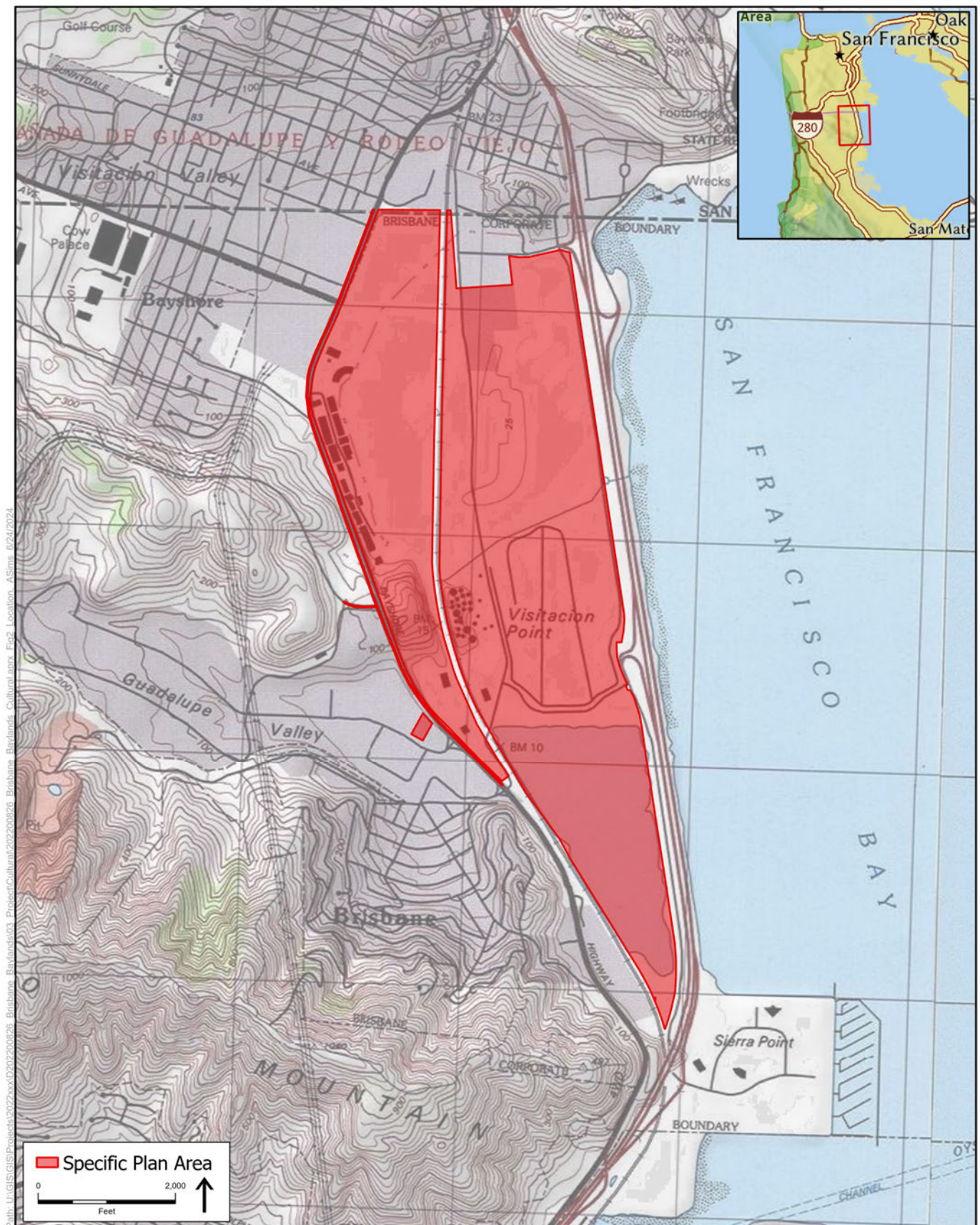


SOURCE: ESA, 2024; ESRI, 2024

D202200826. Baylands Specific Plan

**Figure 1**  
Project Vicinity





SOURCE: ESA, 2024; USGS 7.5' Topographic Quadrangle (San Francisco South, CA)

D202200826. Baylands Specific Plan

**Figure 2**  
Project Location





SOURCE: ESA, 2024; ESRI, 2024

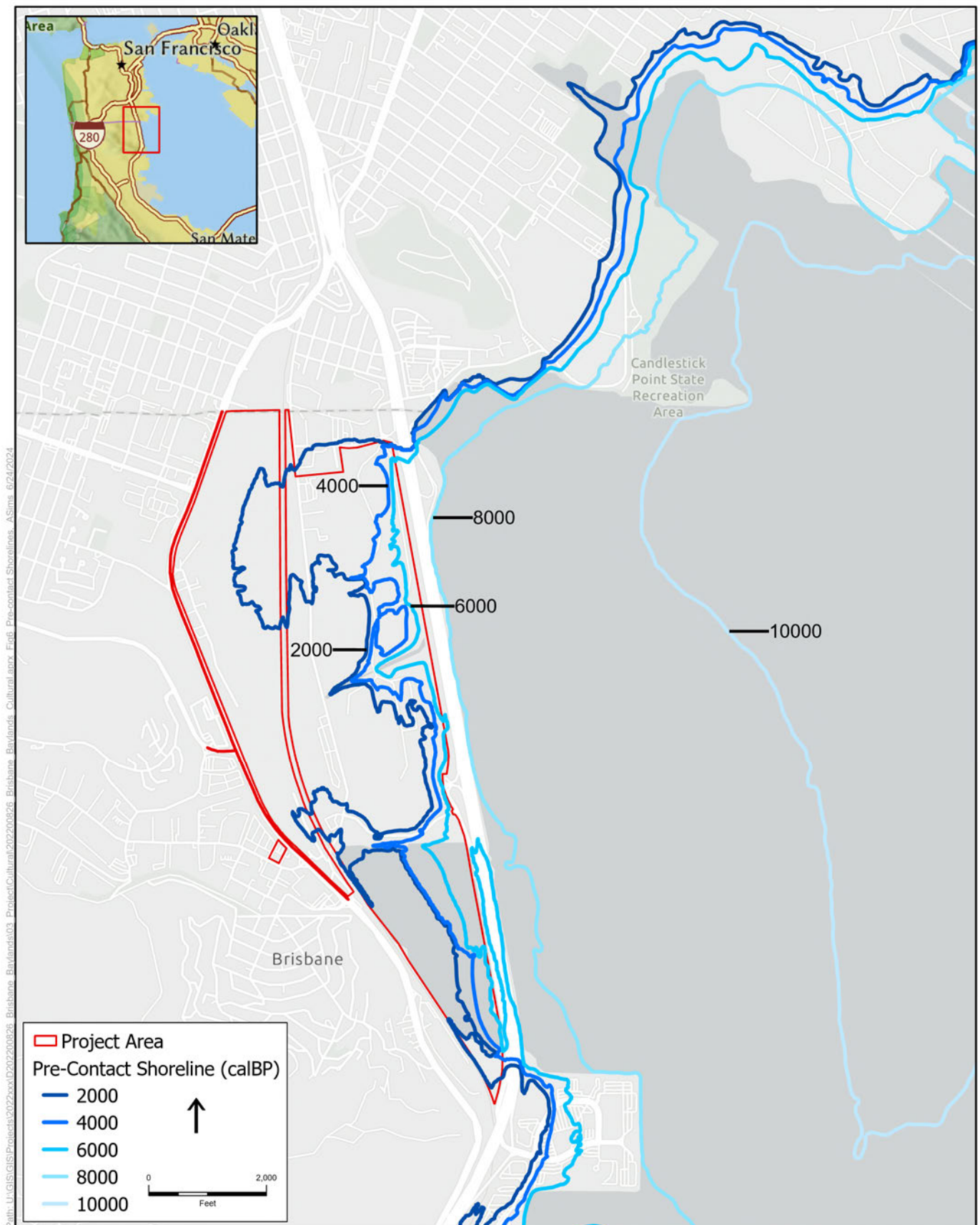
D202200826. Baylands Specific Plan

**Figure 3**  
Project Area

**Figure 4**  
Cultural Resources within the Project Area  
[REDACTED]

**Figure 5**  
Historical Resources within the Project Area  
[REDACTED]





SOURCE: ESA, 2024; ESRI, 2024

D202200826. Baylands Specific Plan

**Figure 6**  
Pre-Contact Shorelines

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# Appendix A

## **Brisbane Bayshore Roundhouse Stabilization and Rehabilitation Plan**





**BAYLANDS BAYSHORE ROUNDHOUSE  
STABILIZATION AND REHABILITATION PLAN**

BRISBANE, CALIFORNIA

PREPARED FOR  
BAYLANDS DEVELOPMENT, INC.  
[19406A]

NOVEMBER 17, 2020





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Interior view of Brisbane Baylands Roundhouse, looking west, date unknown. Source: Robert Morris Photography.

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# Introduction

## PURPOSE OF FEASIBILITY STUDY

This feasibility study was prepared for UPC as an effort to protect and preserve the Southern Pacific Railroad Roundhouse as required by the City of Brisbane Ordinance and General Plan Amendment No. GP-1-18. Listed on the National Register of Historic Places, the roundhouse and site are an integral part of the Brisbane Baylands Specific Plan which ensures that the rehabilitation of the Roundhouse for adaptive reuse occur at the developer's cost. This preparation of a stabilization and rehabilitation plan are a required mitigation measure per the Brisbane Baylands Draft Environmental Impact Report (DEIR) (Mitigation Measure 4.D-1a – page 4D-27-28). The project site is limited to the Roundhouse structure, the turntable pit, and area of land between, however landscape treatments include only general recommendations and are not included in the scope of this study.

This feasibility study also includes a Secretary of Interiors Standards analysis for rehabilitation or reconstruction as is required by the DEIR, based upon the enclosed adaptive reuse program and design. In addition, the mitigation measures note that the rehabilitation plan shall be consistent with a minimum of 7 of 10 of the Secretary of Interiors Standards for Rehabilitation and with NPS Preservation Briefs #17 and #18.

The majority of the Roundhouse Park site is located within an area projected to be affected by a potential 6-foot sea level rise, and is documented by FEMA to have 0.2% Annual Chance Flood Hazard. This provides significant challenges for the Roundhouse structure, site, and turntable pit which are historic resources. Per the National Park Service Guidelines on Flood Adaptation for Rehabilitating Historic Buildings, this is a site with an “established flood risk level” and thus these guidelines are appropriate to apply in the rehabilitation of the Roundhouse.

The purpose of this Stabilization Plan and Rehabilitation Study is to provide recommendations for two overall efforts: 1) Prevent any further deterioration of the historic structure due to water penetration, vandalism, infestation, and overall neglect; and 2) Lifting or raising the building above the anticipated sea level rise and rehabilitating the building as a community asset.

## CHALLENGES OF REUSE

The Bayshore Roundhouse is a unique property type and is the only remaining brick roundhouse in California. The building presents a number of challenges, not only in terms of stabilization in its current deteriorated condition, but also related to anticipated sea level rise and finding an appropriate new use. The site is proposed to be regraded in anticipation of sea level rise which presents an initial challenge regarding elevating the Roundhouse, a task that will require unique structural considerations as well as a thoughtful approach to preserve the historic integrity of the building. The Roundhouse is particularly challenging to lift due to its size, weight, and unusual shape. Furthermore, the building has not been occupied or in use for decades and has suffered from deterioration and exposure to the elements--worsened by fire in 2001 at the southwest end of the building.

The Roundhouse was designed and built as an open maintenance facility without interior walls. The primary challenge for reusing this building will be to maintain the openness of the interior space while accommodating several new enclosed spaces for multiple uses. The abundance of columns with close spacing is a challenge to uses that require clear spans and open floor plans; however, it is possible to take advantage of the current openness of the south portion of the Roundhouse that experienced fire damage, by not reconstructing columns in all historic locations. Although the unique size and shape of the building is a challenge for adaptive reuse, the building has the potential to become an active community asset and to tell a story about railroad development and maintenance in Brisbane.

## PROJECT TEAM

### PAGE & TURNBULL - ARCHITECTURE

Peter Birkholz, Principal in Charge  
Greg Yanito, Project Manager  
Steven Lee, Project Architect  
Hannah Simonson, Cultural Resources Planner  
Caitlin Turner, Architectural Conservator  
Chris Wright, Architectural Designer

### ZFA - STRUCTURAL ENGINEERING

### JR CONKEY, LLC - COST ESTIMATING

### L&R RESOURCES, LLC - FLOOD HAZARD MITIGATION SPECIALIST

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# Executive Summary

## SUMMARY

Upon evaluation, the Page & Turnbull Team has determined that lifting the Brisbane Baylands Roundhouse six to eight feet to accommodate sea level rise is physically feasible and will not compromise its status as a National Register-listed property. The building remains in fair to poor condition today and its ongoing material condition and historic integrity will benefit from immediate stabilization.

The project team recommends severing the building from its existing concrete slab, lifting the building, and setting it down upon a new concrete slab supported by new stem walls and compacted fill. This approach is recommended over dismantling and reconstruction, both from a cost and preservation standpoint. As detailed in the full report, the adaptive reuse program preferred by both the client and community--including a community center, railroad museum, café, and open air theater--would be in conformance with the Secretary of Interiors Standards for Rehabilitation and would therefore comply with the overall project's EIR. However, the team recommends that future consideration be given to the landscape and features surrounding the Roundhouse. Interpretive design and choice of landscape materials and features should help tell the story of how the whisker tracks, turntable pit and other landscape features contributed to the historic use of the building.



Birds-eye view of Brisbane Baylands Roundhouse, looking east, 1946. Source: ReelSF.

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# Background Information

## HISTORIC STATUS

The Southern Pacific Railroad Roundhouse was listed in the National Register of Historic Places (NR#10000113) in 2010, and was automatically listed in the California Register of Historical Resources. The Conservation Element of the Brisbane General Plan (1994) identified the former SPRR Roundhouse as a significant cultural resource.

## SIGNIFICANCE

The National Register of Historic Places Nomination for the Southern Pacific Railroad Roundhouse provides the following summary statement of significance:

The former Southern Pacific Railroad roundhouse at Bayshore and its functionally related turntable are significant under National Register of Historic Places Criterion C (Design/Construction) for embodying “the distinctive characteristics of a type, period, or method of construction,” of a Southern Pacific Railroad brick roundhouse of the late 19th and early 20th centuries. As an early 20th century

transportation building, it is significant on the local/regional level in the areas of Transportation and Architecture. Developed in response to the need to service and repair locomotives during the steam era, roundhouses typically had circular footprints with tracks that were arranged radially leading to separate stalls. Roundhouses often had turntables to position locomotives into individual stalls. Although there were non-roundhouse designs, the roundhouse form was a compact solution to service steam locomotives that were generally designed to run in the forward direction with only limited ability to back up. It is the only extant railroad roundhouse of the many that once existed in California during the era of steam locomotives. It is eligible for listing as an example of a distinctive railroad support facility. The period of significance is 1910-1957. This period coincides with the date the roundhouse was completed (1910) to the year when steam locomotives were last serviced in the roundhouse (1957).<sup>1</sup>

## INTEGRITY

The National Register Nomination provides the following discussion of historic integrity:

The roundhouse is in damaged condition but retains its integrity of location, design, setting, materials, workmanship, and feeling. [...] Despite the fire in 2001 and its somewhat degraded physical condition, the roundhouse retains its historic integrity. Although the condition of a portion of the building's roof structure has suffered, the effects have not compromised the property's overall historic integrity. All exterior walls remain intact, preserving the original plan extent of the building. The unique shape of the building, its relatively simple construction, and the retention of its distinctive materials allow it to convey its historic significance. There is no mistaking this building for anything other than a railroad roundhouse. [...] The roundhouse retains the majority of its features. The roundhouse retains all the physical features “important for illustrating a particular architectural style or construction technique” such as its massing, spatial relationships, proportion,

pattern of windows and doors, texture of materials, and ornamentation. Loss of some of the roof material, which is not a key feature, does not preclude the roundhouse from eligibility for the National Register.<sup>2</sup>

The condition of the Roundhouse has deteriorated somewhat since the National Register nomination was completed in 2010. All windows have deteriorated or been removed, although some wood mullions remain. The wood doors at the outer circumference have also further deteriorated. However, despite this additional loss of some materials, the integrity of the building remains largely the same as in 2010, when the building was listed on the National Register and still retains sufficient integrity for eligibility.

1 National Register of Historic Places Nomination, Southern Pacific Railroad Bayshore Roundhouse (NR#10000113), prepared by Richard Brandi (certified March 26, 2010), 8-1.

2 Ibid, 7-1, and 7-4 to 7-5.



CHARACTER DEFINING FEATURES

For a property to be eligible for national, state, or local designation under criteria related to type, period, or method of construction, the essential physical features (or character-defining features) that enable the property to convey its historic identity must be evident. These distinctive character-defining features are the physical traits that commonly recur in property types and/or architectural styles. To be eligible, a property must clearly contain enough of those characteristics to be considered a true representative of a particular type, period, or method of construction, and these features must also retain a sufficient degree of integrity. Characteristics can be expressed in terms such as form, proportion, structure, plan, style, or materials.

The National Register of Historic Places Nomination for the Southern Pacific Railroad Roundhouse does not explicitly outline character-defining features. Based on visual inspection and review of previous documentation, including the National Register Nomination, Page & Turnbull has identified the following character-defining features.

EXTANT (OR PARTIALLY EXTANT)  
CHARACTER-DEFINING FEATURES

- One-story, 108-degree curved massing
- Curved gable roof with metal smokestacks above each stall and monitor at east end
- Brick construction with wood interior framing
- Triangular parapet at east end wall
- Stepped brick parapet at south-facing end wall and central firewall
- Cast iron posts and lintels with stall numbers at the inner circumference
- Openings for 17 enclosed stalls at inner circumference (stalls 24 to 40)
- Fenestration pattern at outer circumference and end walls, featuring arched openings
- Wood barn doors with cross angles and metal hardware at outer circumference
- Rail tracks in each stall
- Concrete floor inside Roundhouse

- Drop pits between stalls 36 and 38
- Wood partition with beaded wainscot between stalls 39 and 40
- Industrial metal gooseneck light fixtures
- Lighting poles at open stalls 1 through 23.
- Circular concrete turntable pit

NON-EXTANT CHARACTER-  
DEFINING FEATURES

- South portion of the curved roof and supporting wood structure
- Raised skylight and ventilation monitors along full length of roof
- Multi-lite wood sash double-hung windows with arched headers
- Central steel turntable
- Whisker tracks.

SIGNIFICANCE DIAGRAMS

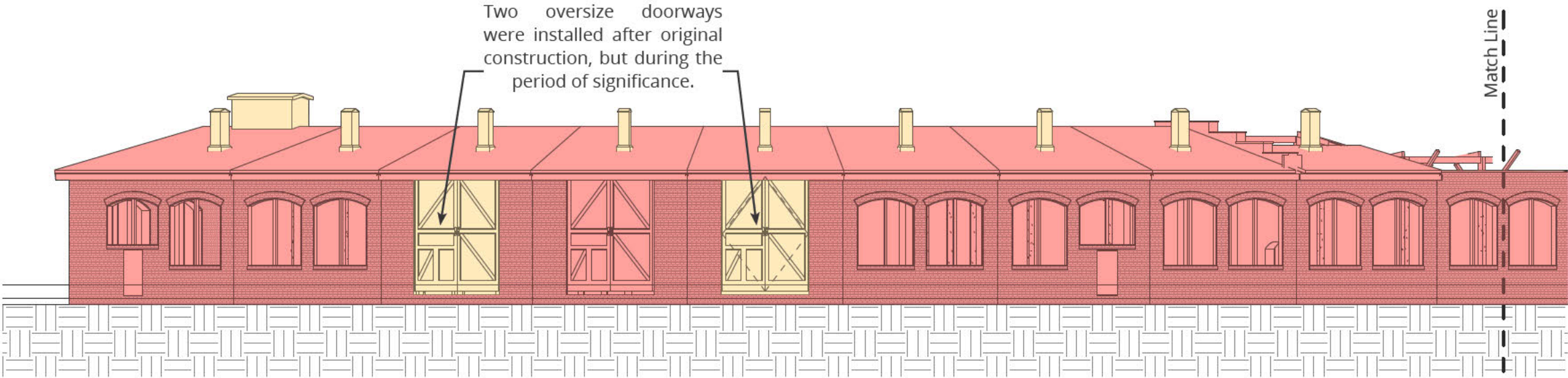
The following pages contain significance diagrams of the Souther Pacific Railroad Roundhouse, which were prepared by Page & Turnbull utilizing accepted standards for the evaluation of historic resources. Te diagrams illustrate a relative hierarchy of exterior features and interior spaces by categorizing them as “Primary Significant,” “Secondary Significant,” or “Not Significant.”

**Primary Significant** spaces, elements, or materials date to the period of significance (1910-1957) and are the most historically significant components of the building; these features and spaces have a high degree of historic integrity.

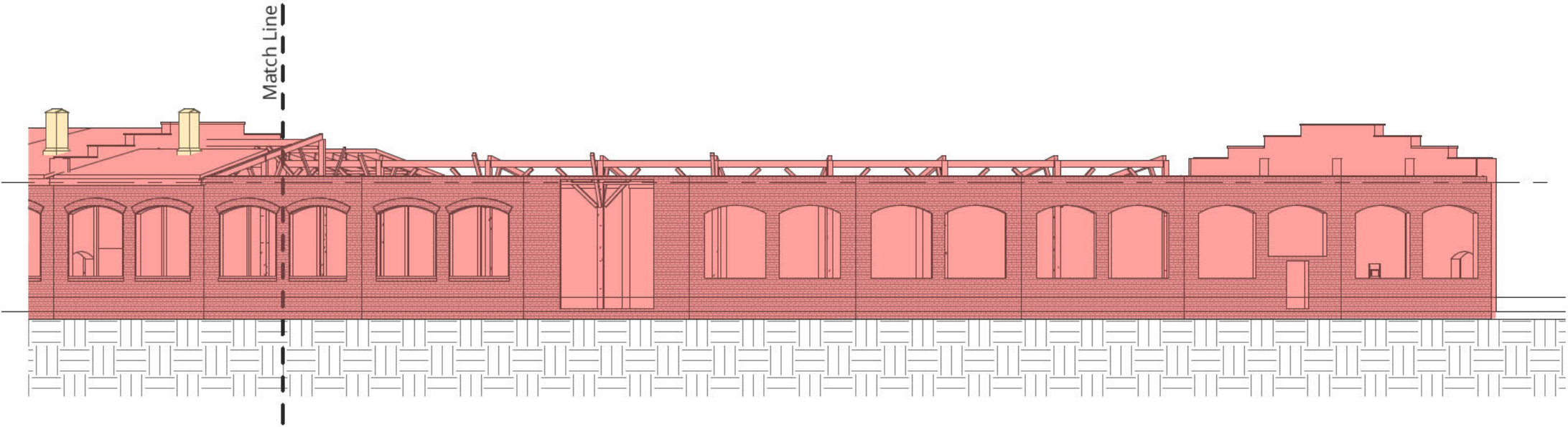
**Secondary Significant** spaces, elements, or materials date to the period of significance (1910-1957) and are less historically significant components, but cumulatively contribute to the overall historic character of the building. These include features that were altered or added after original construction, but within the period of significance.

**Not Significant** spaces, elements, or materials do not contribute to the overall historic character of the building, have been substantially altered, and/or were added or altered after the period of significance.

ELEVATIONS



OUTER WALL - EAST END



OUTER WALL - SOUTH END

Significance Diagram

LEGEND

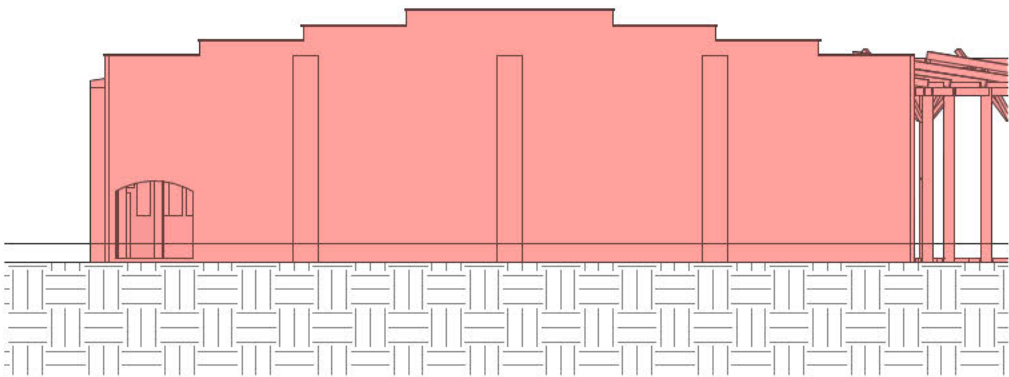
- Primary Significance** delineates features or spaces that date to the period of significance (1910-1957) and are the most historically significant components of the building.
- Secondary Significance** delineates features or spaces that date to the period of significance (1910-1957) and cumulatively contribute to the overall historic character of the building. These include features or spaces that were altered or added after original construction, but within the period of significance.
- Features or spaces that are **Not Significant** do not contribute to the overall historic character of the building; have been altered, added, or demolished; or were constructed after the period of significance. These features are not considered historic.

NOTES

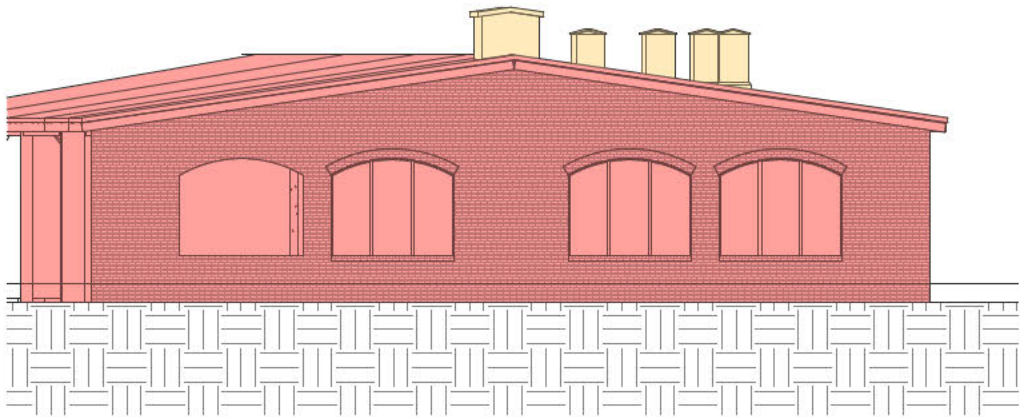
- No window sashes are extant. Wood window frames are extant in some locations, particularly north and east of the fire wall.
- Small doors are not extant. Oversize outward swinging doors are partially extant in three locations, but are in very poor condition. Oversize doors are no longer extant in the southernmost doorway.
- The base drawing for this diagram was drawn by Page & Turnbull, 2020.
- Not to scale.



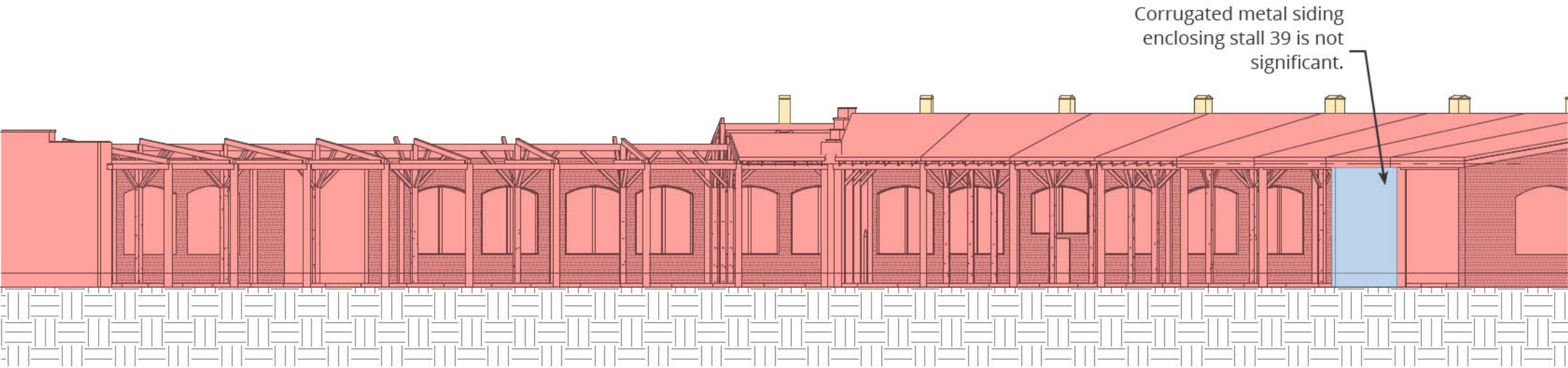
ELEVATIONS



SOUTH END WALL



EAST END WALL



INNER WALL

Significance Diagram

LEGEND

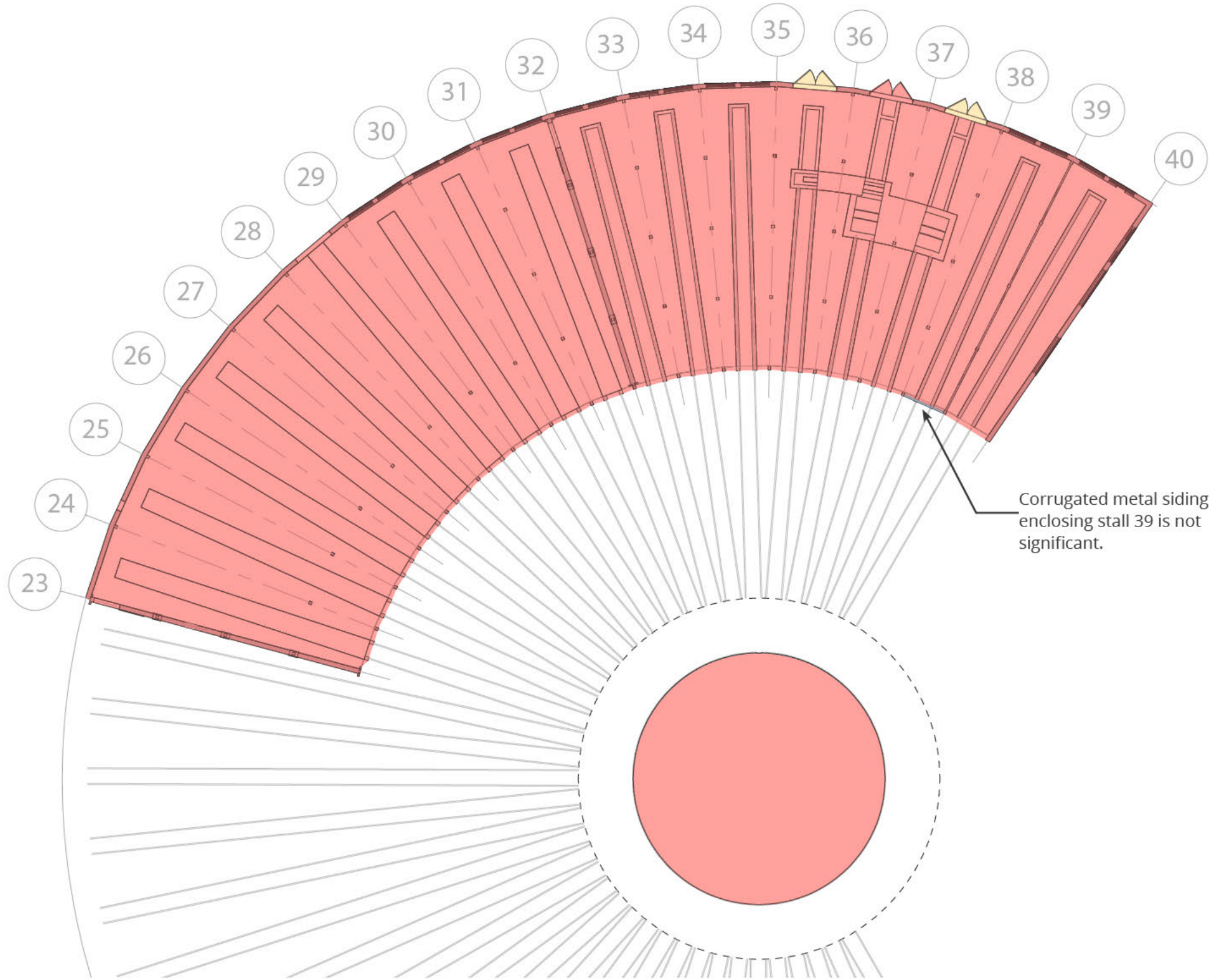
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- Not to scale.



FLOOR PLAN



LEGEND

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- **Secondary Significance** delineates features or spaces that date to the period of significance (1910-1957) and cumulatively contribute to the overall historic character of the building. These include features or spaces that were altered or added after original construction, but within the period of significance.
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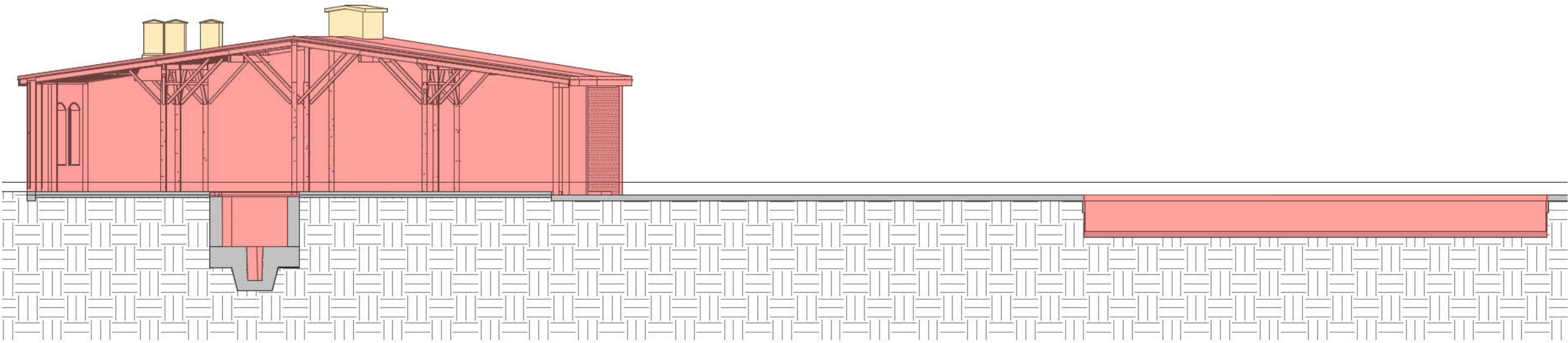
NOTES

- All extant rail tracks are of **Primary Significance**. Tracks outside the Roundhouse building have been removed or covered in dirt and vegetation.
- The interior space of the Roundhouse is of **Primary Significance**. The concrete floor are of **Secondary Significance**.
- Lighting poles (not shown in this drawing) at the outer ends of the whisker tracks are of **Secondary Significance**.

The base drawing for this diagram was drawn by Page & Turnbull, 2020.  
 Not to scale.

Significance Diagram

SECTION



LEGEND

- **Primary Significance** delineates features or spaces that date to the period of significance (1910-1957) and are the most historically significant components of the building.
- **Secondary Significance** delineates features or spaces that date to the period of significance (1910-1957) and cumulatively contribute to the overall historic character of the building. These include features or spaces that were altered or added after original construction, but within the period of significance.
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NOTES

The base drawing for this diagram was drawn by Page & Turnbull, 2020.  
 Not to scale.

Significance Diagram



# Conditions Assessment

## METHODOLOGY

Page & Turnbull staff visited the Roundhouse site on the morning of May 7, 2020. The weather was clear and sunny. The materials and surrounding site of the Roundhouse were evaluated for their current level of deterioration and their expected future deterioration given the current condition.

The Roundhouse was explored at close range from grade, and all areas were accessed. Conditions were recorded in digital notes and images taken with a digital camera.

## SUMMARY

Overall, the Roundhouse is in fair material condition. Its open-air nature and remote site makes it a desirable shelter for transient populations, which has resulted in considerable debris on the site. One half of the structure has sustained considerable damage due to fire, resulting in the loss of the wood structure, roof and any remaining interior walls. The extant half of the building suffers from many roof leaks due to roofing material loss.

Brick and its original mortar, however, is in good condition, though heavily defaced by many layers of graffiti. Remaining wood structure is in fair condition and the remaining cast iron structure is in good condition.

Windows have all been removed, though frames and some casing remains. Doors are in very poor condition or have been removed.

Even with these conditions, the Roundhouse can still be effectively rehabilitated in place to a useful purpose.

## ASSESSMENTS

Individual materials and objects are identified below and their conditions discussed. Refer to the accompanying images for additional detail.





SITE

The Roundhouse site can best be characterized as overgrown. It is protected by an ill-performing chain-link fence with a barb wire topper. Survey staff encountered transient, friendly trespassers living within the fence under the sheltered area of the building.

The track turntable pit serves as a wetland area with reeds, water, and small trees. The circular steel beam at the edge of the pit remains in place and is corroded. Tracks leading from the turntable to the Roundhouse bays were not evident, though layers of dirt may obscure any remaining material. Grasses, dirt, and gravel covers the infield area between the turntable pit and the bays; debris and garbage is strewn haphazardly.

The area of the site to the south of the Roundhouse, where tracks previously extended in a radial fashion, through the unbuilt area of the Roundhouse is likewise heavily overgrown. Concrete and evidence of the former whisker tracks remain (in the form of steel spikes and tie-downs), but no rails were present. Lighting poles with electrical remnants still stand and carry original green light shades. Operability of the lights is unknown and many remain with bent shades and some corrosion of the underlying sheet metal. One similar light remains on the exterior of the Roundhouse and presents a similar condition.

Piles of railroad ties and stacks of removed rails and whisker tracks are present beyond the immediate area. Ties are deteriorated, though in fair condition. Rails are surface corroded as expected, but are typically not bent or otherwise damaged. Two whisker track sections were found as well and in similar condition.



Figure 1: (right) Lighting pole with original light.



Figure 2: (above far right) The steel of the turntable edge visible, and here, with a tree growing over it.

Figure 3: (middle far right) Whisker track sections.



Figure 4: (below far right) Removed rails stacked on site.

Figure 5: (below) An overgrowth of weeds fills the infield where the turntable pit is.





BRICKS AND MORTAR

The bricks in the Roundhouse walls are themselves in good condition. There is no evidence of rising damp or other mechanism of deterioration to the brick itself other than externally-caused impact damage or removal and the considerable graffiti covering much of the brick surfaces.

There are some major cracks through the brickwork, especially through window arch openings and at the south end wall. A section of east end wall under one window opening is broken and missing. Brickwork in the area of the building that burned does not exhibit fire scaling or other obvious damage. The south end wall is leaning out from the circumference walls.

Mortar is likewise original and sound in most locations. It appears to be a portland cement-based mortar mix.

Two oversize door openings were added into the east end of the radial brick wall each flanking the single original oversize door in the area. Where these openings were made a non-matching brick was used to infill the removal areas to fit the doors. Mortar also does not look similar to the original mix.



Figure 9: The south end firewall is leaning out from the rest of the Roundhouse. Whether it is a function of settlement or simply itself is tilting is unknown.



Figure 6: Graffiti along the outer circumference is obvious. Some staining at down spout and roof leaks are also apparent.



Figure 7: There is cracking through some arches over openings.



Figure 8: At the south end firewall, the inner wall's end parapet is detaching from its supports.



WINDOWS AND DOORS

Window sashes have all been removed from the Roundhouse building. Images taken by Page & Turnbull on a previous visit to the site in 2009 show the sashes extant at that time. No sashes were found on site. Window frames remain in most openings, though frames in the burned half of the building are heavy burned and/ or partially lost. Remaining frames in the non-burnt end are in fair to poor condition depending on their sun exposure. Interior casing at the windows is present on some windows, though it is in poor condition, typically being only partially intact and heavily impacted and painted.

Doors that remain are in very poor condition. Only one original oversize door remains. Stiles and panels are detaching from each other, many elements are broken or damaged. Everything is heavily graffitied. No pedestrian-sized doors remain.

Oversize door hinges are embedded in the brickwork and are in fair condition, carrying expected corrosion. The related strap hinges remain on the east original oversize door.



Figure 10: Original door leaf.



Figure 11: Altered door leaf in a modified doorway.



Figure 12: The only oversize opening in the burned south area has no leaf and minimal window material remains.



Figure 13: In the unburnt east end window frames remain.



Figure 14: Window casing remain at the interior of few windows.



ROOF

The roof over the unburnt section of the Roundhouse is in poor condition. The roof was not accessible at close range, but appears to be asphaltic roll-up roofing over plank sheathing. Gaps in the plank sheathing are visible from below where the roof topping layer has been lost. Some areas of sheathing are also missing. Roofing beams end at exposed rafter tails, some of which are broken or otherwise deteriorated from exposure.

A small wood monitor remain at the east end of the Roundhouse. It likewise is in poor condition. It has louvered sides, but multiple pieces of its exterior are missing or have been altered.

Wood fascia, soffit, and trim, once painted a dark brick red, lines the edge of the roof. Many sections of the trim

and soffit are split or detached. Bird nests appear in some of the openings.

Sheet metal smokestacks also sit atop the roof. These appear in fair condition, though, as with the other roof elements, were not surveyed at close range. The smokestacks align in with large sheet metal hoods at the interior of the Roundhouse.



Figure 19: (above, top) Roof view.

Figure 20: (above, middle) Broken and deteriorated rafter tails.

Figure 21: (above, bottom) Missing and damaged trim section which houses a bird's nest.



Figure 22: Missing roofing resulting in visible gaps through roof.



Figure 15:  
 (left, above) Sheet metal smokestacks.



Figure 16:  
 (left, below) Sheet metal hoods correlating to smokestacks.



Figure 17:  
 (right, above) Louvered Roof Monitor.



Figure 18:  
 (right, below) Interior view of louvered roof monitor.



WOOD TIMBER STRUCTURE

The primary structure of the Roundhouse is comprised of wood timbers between the brick firewalls. In the burned area, the posts and beams are in very poor condition where they remain in their original locations. Steel water pipes remain attached to the burned structure. In the unburnt section, timbers are in fair to good condition.

Some columns have cutout sections or some insect damage at grade. Upper timbers show little deterioration

beyond some water staining. Upper timbers were not assessed at close range.

A wood panel wall at the eastern-most bay remains in tact, but in poor condition. Heavily graffitied, this wall also has damaged or removed sections.



Figure 23: The wood post and beam structure with knee braces.



Figure 24: The wood panel wall between two bays. Sections are lost.



Figure 25: Some posts have sections cut out of them.



Figure 26: Insect damage where the posts are bolted to the slab.



Figure 27: Splits in the timbers are present.

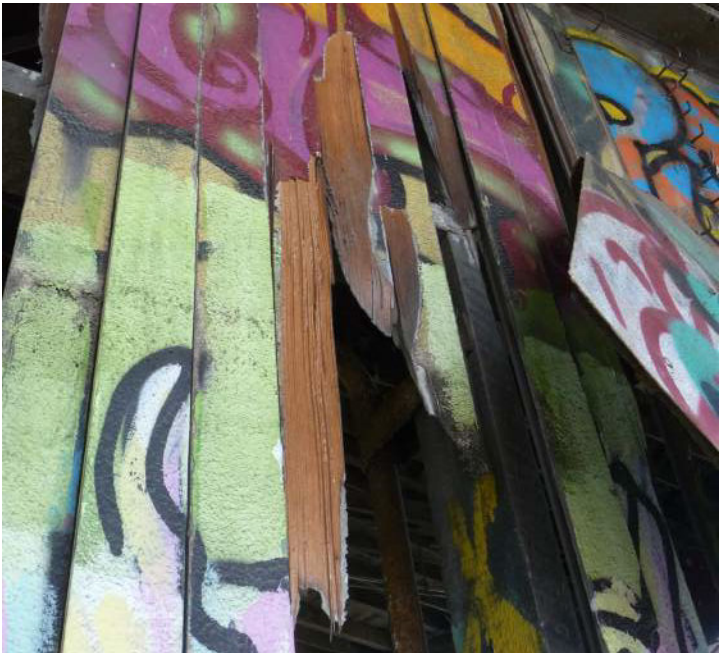


Figure 28: Breakage in the wood panel wall.



CAST IRON STRUCTURE

Cast iron posts and lintels line the inner circumference of the Roundhouse. Posts are hollow. The steel is in fair condition with expected surface corrosion and graffiti. All stalls retain their cast numbers, even in the burnt section.



Figure 29: The steel structure provided the inner circumference support.



Figure 30: Expected surface corrosion and paint are evident.



Figure 31: Cast stall numbers appear to have once been painted.

CONCRETE SLAB AND FLOOR PITS

The interior floor of the Roundhouse is a concrete slab on grade. Much of the concrete slab is covered in dirt, volunteer plants and debris.

Pits remain below the Roundhouse stall tracks, though most are filled with dirt and/or debris.



Figure 32: (left) Floor pits filled with debris.

Figure 33: (above) Plants filling pits and dirt over concrete slab.

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# Project Phasing

The stabilization and restoration of the roundhouse is proposed to be completed in five phases:

- 1. Safety and Security Phase:** The safety and security phase will include fencing the site and installing security measures to prevent unwanted access, as well as mitigation of imminent hazards and removal of pests and plants. This phase is recommended to occur immediately while the project goes through entitlements..
- 2. Initial Stabilization:** The second phase includes temporarily “mothballing” the structure to prevent further damage and deterioration. It will include protecting the structure from further moisture penetration, plant and pest infestation, and stabilizing the structural components against some magnitude of wind and seismic forces. These stabilization measures are expected to be in place for two to three years.
- 3. Raising the Building & Re-grading the Site:** In response to expected sea level rise, the site and this structure is expected to be lifted or deconstructed piece-by-piece and reconstructed at the higher elevation and on a new foundation. A comprehensive materials testing scope is proposed at the start of this phase to inform the strategy for the forthcoming restoration and retrofit.
- 4. Second Stabilization:** After the site is raised, a new foundation will likely be poured, and the structure will be connected or re-built atop the new foundation. Some portion of the final retrofit scope will be installed at this time to stabilize the structure, such as columns that need to be attached to the new foundations, steel framing to brace the brick walls, and repairs to wood roof framing. Protection against moisture and infestation will then be reinstated (similar to the initial stabilization) while awaiting final restoration.
- 5. Restoration and Retrofit:** The final restoration of the structure will involve a seismic retrofit and strengthening of the gravity system using the California Historic Building Code to extend the life of the structure for many decades as appropriate for the occupancy programming.



# Near Term Stabilization (Phases 1-2)

## PRESERVATION ISSUES

As identified in the conditions assessment section, the condition of the Roundhouse during the May 2020 site visit is fair - poor overall, with significant damage due to fire and exposure to the elements. It is recommended that a near-term stabilization plan include an initial safety and security phase that provides increased protection against vandalism and also immediately addresses safety concerns. In addition to this initial safety and security phase, near term stabilization will effectively mothball the building and freeze it in time to prevent further decay, deterioration and loss of integrity until lifting and rehabilitation efforts can begin.

## STABILIZATION APPROACH

The anticipated overall project phasing includes five phases of work to both stabilize and rehabilitate the Roundhouse. The first two phases, 1) Safety & Security; and 2) Initial Stabilization encompass the near-term stabilization plan and are intended to prevent further decay and deterioration. The following steps outline the approach to stabilization.

### PHASE 1: SAFETY AND SECURITY

The purpose of the safety and security phase is to secure the site against trespassers as well as address any imminent hazards. The measures below are recommended.

- Cleaning, Fencing, and Security:** It is recommended that the site be cleaned of foliage and debris, and security fencing and monitoring be installed during this phase. Fencing should be installed at least a minimum distance from the building equal to the height of the exterior wall plus four feet on all sides. This is to mitigate localized building collapse and/or structural component falling hazards to anyone within or around the building.
- Sheet Metal Hoods:** A number of sheet metal hoods at the roof level connect to chimneys above the roof. These hoods are hanging from the roof, and their connection to the roof is not able to be confirmed without a scissor lift and/or roof access. It is recommend that either the connections of these hoods are confirmed to be stable (e.g. bolts to in-tact roof elements) or that they are temporarily shored with wood framing to prevent any risk of a falling hazard.

- Charred Wood Removal:** Remnants of charred wood framing in the west half of the building is present, much of which is not stable or partially collapsed. All charred or partially burned wood framing should be removed. If wood members are to remain in the west portion of the structure, they are to be shored when the charred framing is removed.
- Notched Columns:** In at least one location, as shown in Figure 34, a wood column was observed to be significantly notched. This column should be shored or repaired (e.g. by sistered wood framing), as should any other columns that have been similarly compromised. A more detailed review of site conditions is recommended to confirm the shoring scope at wood framing.
- Cracked Brick Lintels:** In at least one location, as shown in Figure 35, a brick lintel was observed to have a significant vertical crack at the center of its span. This lintel should be shored for gravity and out-of-plane seismic loads, as should any other lintels with similar cracks. A more detailed review of site conditions is recommended to confirm the shoring scope at brick lintels.

### PHASE 2: INITIAL STABILIZATION

The intent of the initial stabilization phase is to “mothball” the structure until the site is raised. Mothballing is meant to freeze a building in time, preventing further decay of the structure and its components.

- Sealing the Structure:** It is recommended that rodent- and weather-resistant measures be implemented to prevent effects of continued water infiltration and infestation will assist with protecting the wood framing and brick from deteriorating further from a materials and a structural perspective.

Weather resistance can be achieved by covering the structure, possibly with shrink-wrapping. Any wrapping method will need to be evaluated to understand the impact on the structure, which could include how the wrap is secured to the structure, increased wind loading to the structure, and increased weight on the structural roof members. Thus, further coordination will be required as the design for this phase is developed. Covering methods should also ensure appropriate ventilation so that water is not trapped in the structure, and annual maintenance should be expected to provide any repairs to the covering that may be damaged over time.



Additionally, as covering measures are installed, we recommend that people not walk on the existing roof framing unless shoring is put in place and designed to support live loads.

- Foliage and Debris Removal:** All ivy and plants should be removed from the interior and exterior walls of the structure, if it has not already been completed in the previous phase. Any remaining debris should be removed from the interior of the structure to allow unobstructed access to all areas. In any locations where brick has been penetrated and strength loss occurred due to plant growth, those areas should be assessed for guidance on how to best remove (or not remove), repair, or shore the area.
- Shoring:** Shoring is intended to temporarily brace the brick walls out-of-plane against a seismic event corresponding to a reduced hazard level (as compared to the seismic hazard level that governs the design of permanent structures). This can be achieved via installation of a horizontal “girt” member toward the top of the wall, which is then braced with diagonal members down to a stable surface (e.g. the mat slab). These girts typically run continuously along brick walls, with diagonal braces occurring at a regular spacing, as shown in Figure 37. The diagonal wood braces would be connected to the existing slab with expansion anchors. To stabilize the wall more reliably, a girt should be placed on both the interior and exterior of the wall, “sandwiching” the brick between them. Through bolts would connect the girts together to effectively sandwich the brick.

Additionally, at the walls with large openings, in-plane bracing is recommended, which would include diagonal rod “X” bracing every second or third bay (see Figure 37).

As the longitudinal exterior (north) wall includes many regularly spaced large openings, we propose to infill these openings, both to support the lintel framing above, and also to provide a bearing surface to bolt the girt members, to avoid drilling through the brick wherever possible, as shown in Figure 36.



Figure 34: (top) Notched wood column to receive shoring.

Figure 35: (above) Cracked lintel to receive shoring.

STABILIZATION DIAGRAMS

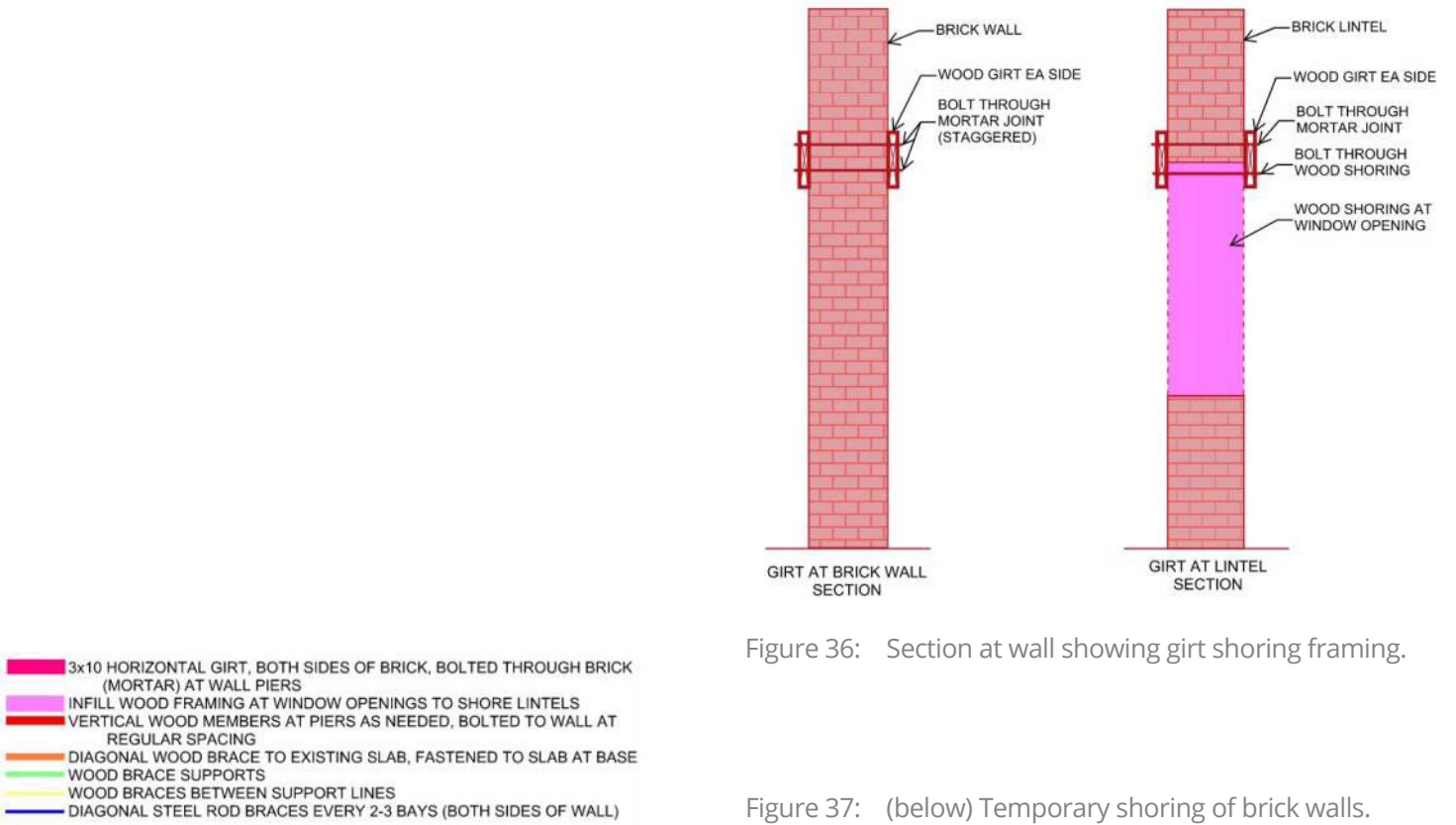


Figure 36: Section at wall showing girt shoring framing.

Figure 37: (below) Temporary shoring of brick walls.



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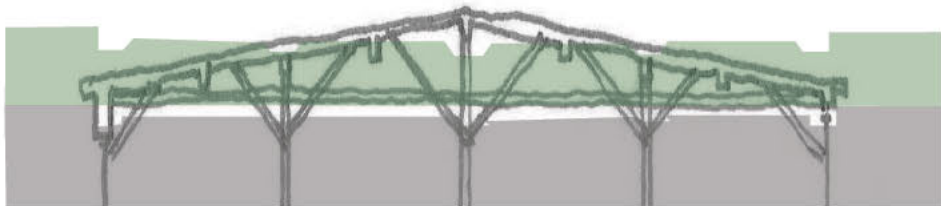


# Accommodation for Sea Level Rise (Phases 3-4)

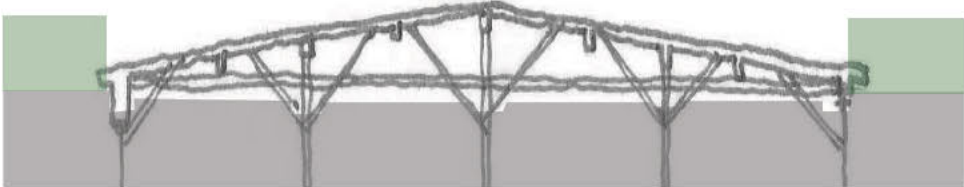
## LIFTING BUILDING VS. RECONSTRUCTION

Due to anticipated Sea Level Rise (SLR), Roundhouse Park (and the structures within it) will be re-graded and raised approximately 6-8 feet. The majority of the Roundhouse Park site is located within an area projected to be affected by a potential 6-foot sea level rise, and is documented by FEMA to have 0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with an average depth less than one foot or with drainage areas of less than one square mile. This provides significant challenges for the Roundhouse structure, site, and turntable pit which are historic resources. Per the National Park Service Guidelines on Flood Adaptation for Rehabilitating Historic Buildings, this is a site with an “established flood risk level” and thus these guidelines are appropriate to apply in the rehabilitation of the Roundhouse.

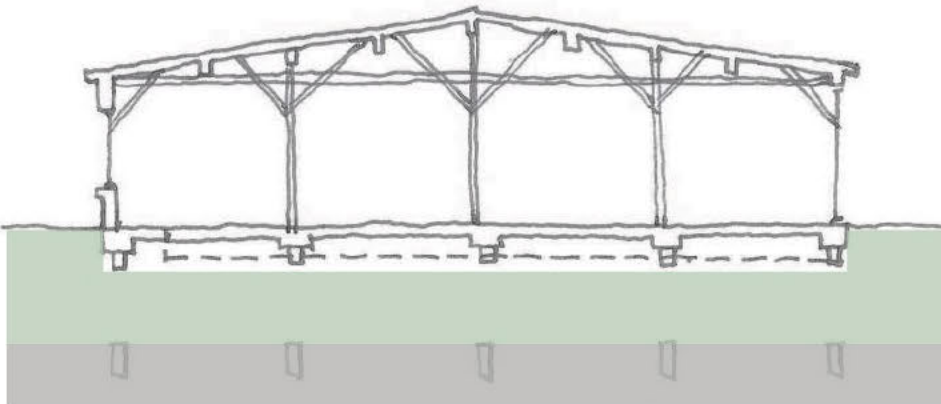
Upon review with ZFA structural engineers and L&R Resources who have expertise with raising historic structures, the recommendation from the Design Team is to pursue a more direct approach that lifts the historic structure rather than dismantling and reconstruction. While lifting the building presents a challenge due to its curved shape and size, there is considerable precedent for lifting buildings of this size, relocating, and re-siting on new foundations. In addition, maintaining the remaining historic integrity of the building will be easier if the building remains intact as it currently stands. The Design Team investigated multiple scenarios related to the existing concrete slab, including raising along with the building, however it is recommended to abandon the slab and replace with new once the building has been lifted.



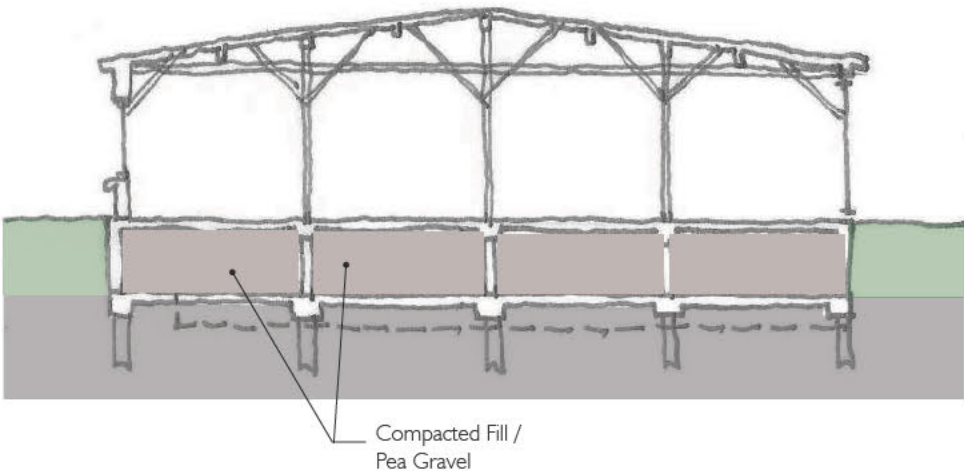
NEW SLAB OVER FILL



NEW SLAB OVER CRAWL SPACE



CUT PILE SOLUTION



NEW SLAB OVER FILL WITH STEM WALLS

## RECOMMENDED LIFTING APPROACH

The recommended approach for lifting the Roundhouse is to detach the existing brick walls and wood columns from the existing slab, lift to the desired re-grading level and temporarily shore on wood cribbing until the site re-grading is complete. In preparation for the re-grading, new concrete stem walls and columns will be extended up from the existing slab to create compartments for compacted fill, pea gravel, or other fill suitable for supporting a new concrete slab.

It is assumed that the Roundhouse will be lifted just prior to the site re-grading. It is recommended that a new structural concrete foundation and slab be poured as soon as possible after lifting to anchor the walls and columns and avoid damage to the structure from a prolonged period of support on cribbing. A professional lifting company should be engaged early in the planning process to prepare preliminary engineering, confirm assumptions of this feasibility study, and provide further details.

### PHASE 3: LIFTING THE BUILDING: OVERALL STRATEGY & APPROACH

1. Initiate materials testing scope, including piles if deemed necessary.
2. Design restoration and retrofit as it pertains to the raised structure.
3. Shore the structure as needed for lifting (typically designed and installed by the lifting contractor), including needling beams at the base of brick walls, vertical supports adjacent to brick walls, and shoring under wood roof framing and wood/steel columns.
4. Sever the brick walls and wood/steel columns from the foundation.
5. Lift the structure (walls and roof) approximately 10 feet onto cribbing.
6. Install new concrete stem walls and columns extending up from the existing mat slab; incorporate anchorage of new structural elements (columns, braces) into new concrete, as needed. Reinforce existing foundations as required.
7. Infill the footprint of the structure with compacted soil, gravel, or sand in accordance with geotechnical recommendations.

8. Install a new structural slab over the new stem walls and columns; incorporate anchorage of new structural elements (columns, braces) into new concrete, as needed.
9. Lift the existing building off the cribbing, remove the cribbing, and lower the existing building onto the new foundation.
10. Attach the existing building to the new foundation and new structural elements.

#### Additional Considerations:

Upon review with a professional lifting contractor, the following considerations were identified:

#### Assumptions:

- 30 jack points
- Brick walls, columns and structure would be lifted simultaneously
- Union labor would be utilized
- Estimate one month of design engineering for lift

#### Recommendations for potential cost savings:

- Partial building disassembly: roof and columns removed prior to lift
- Raise brick walls separately from columns and structure
- Reassembly of columns and structure by crane after new slab is poured
- Perform a field visit & preliminary engineering by professional lifting company to confirm assumptions

### PHASE 4: SECONDARY STABILIZATION

Similar to the initial stabilization (Phase 2), the intent of the second stabilization phase is to mothball the structure until the restoration and retrofit can be completed, assuming it will not be completed immediately following the previous phase. The end of Phase 3 includes re-attachment of the structure to the foundation and likely installing some portion of the final retrofit structure. Any remaining structural elements that are not yet stabilized as part of Phase 3 will be temporarily braced in this phase (e.g. shoring for roof members that have not yet been repaired). The structure will be shrink-wrapped or otherwise covered similar to Phase 2.



# Potential Design & Reuse (Phase 5)

## COMMUNITY PREFERRED USES

A community workshop on Open Space as part of the Baylands Specific Plan effort was conducted led by OJB Landscape Architects in September 2019. With nearly 80 community member participants, the results of this workshop identified and prioritized new uses for the Roundhouse building. The following are the most highly requested programmatic uses to reimagine the Roundhouse as a community asset:

- Roundhouse Museum
- Restaurant / Outdoor Dining
- Amphitheater
- Exhibition / Educational Space

## DESIGN NARRATIVE

### PHASE 5: RESTORATION AND RETROFIT

The intent of the restoration and retrofit will be to provide a fully code-compliant building, safe for use of the structure and to extend its usable life for many more decades, which is typically at least 50 years in accordance with building code intent.

### PROPOSED REHABILITATION PROGRAM:

The Design Team presented nine conceptual programming schemes for adaptively reusing the Roundhouse in June 2020.

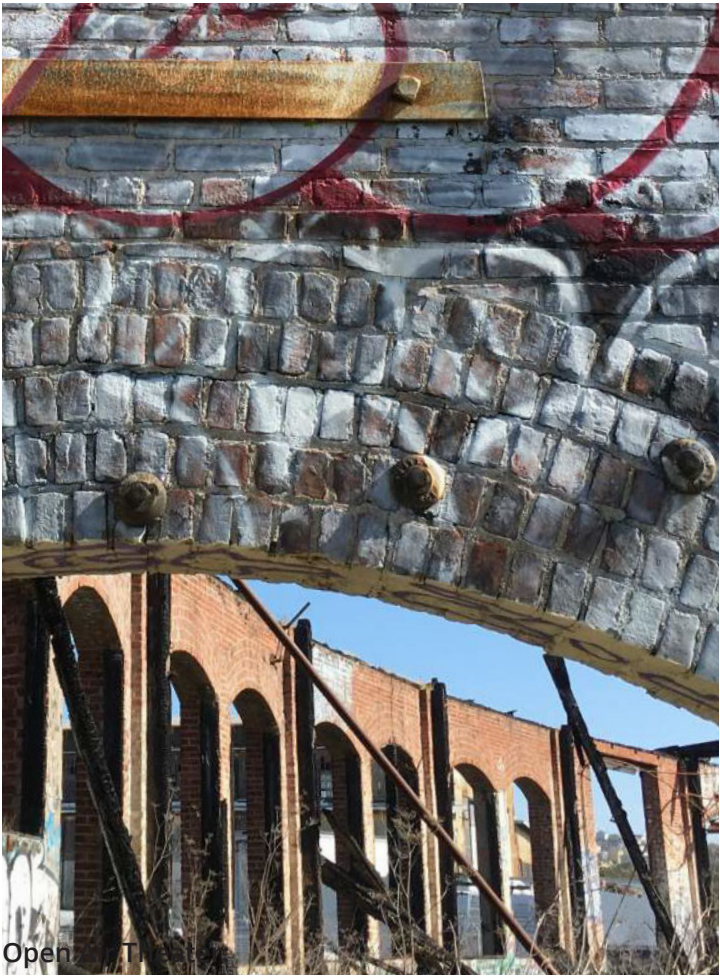
The proposed Brisbane Baylands Roundhouse rehabilitation conceptual scheme brings the site back to life as a public amenity with an emphasis on preserving the unique character of this railroad building typology. The conceptual scheme proposes reuse of the existing enclosed structure (east side) as conditioned space while reusing the existing open air part of the structure (west side), which was previously damaged by fire, as a semi-enclosed unconditioned space. New program uses follow the original column grid and take advantage of existing column locations to create program use separation walls. These demising walls between program uses extend up to the underside of the roof deck, while other interior partitions extend up approximately ten feet, to allow users to experience the open roof trusses of the original building.

Upon review of preliminary options, Baylands Development, Inc. provided design direction to the Design Team to prepare conceptual drawings for a combined rehabilitation scheme that includes four primary uses.

The following presents one scenario/programmatic option that has been developed and analyzed in this report for compliance with The Secretary of the Interior Standards. This proposed programming reflects current thinking, however the treatment of the building/space would remain the same even if programming changes or shifts.

1. Open Air Theater with flexible seating and stage, approximately 4,700 sf, seating for approximately 200 persons
2. Community Space, approximately 3,700 sf
3. Railroad Museum, approximately 3,700 sf
4. Café, approximately 2,400 sf

The scheme also incorporates restrooms, utility, storage space, and a covered arcade space at the southeastern (park) side of the building that can accommodate outdoor seating for the various programming uses. The arcade is separated from each program space by a full height glass storefront wall system with access doors into each space. In addition to entries at the arcade, each program space also has entry access in existing historic openings at the outer curved wall of the Roundhouse to provide maximum flexibility.



**Open Air Theater:**

The open air space west of the existing fire wall will be programmed as a flat, flexible open space with a fixed ADA-accessible 18” high stage at the park side. New steel columns and beams will be installed at the original column locations and will structurally tie the existing building walls together. Existing cast iron columns and beams at the inner curved wall will be retained. The new steel post and beam structure will be differentiated from the existing cast iron columns and beams at the inner curved wall in size and/or material finish. Window openings at the open air space will have a sheer and relatively transparent metal mesh set into rebuilt wood window frames (no sash) for safety and to create a sense of enclosure to the space while allowing natural ventilation into the open space. Door openings at the southern end wall will remain open, while the large wooden stall doors at the western outer curved wall will be reconstructed and lockable in the open position.

**Community Space:**

One of three new uses within the enclosed portion of the Roundhouse, the Community Space will have a central open area suitable for large group meetings, flexible breakout conference rooms, and interior storage room. Restrooms are located between the community space and open air theater.

**Railroad Museum:**

The central three bays of the enclosed portion of the Roundhouse is designated to house a publicly accessible railroad museum and possibly a restoration shop to provide an interpretive opportunity for this historic building. Historic locomotive and other railroad vehicles could be housed in the museum as part of this use, which would lend authenticity to the rehabilitation effort.

The central three bays were purposefully chosen for the museum due to the location of the original drop pits and large door openings at the outer curved wall in stalls 36, 37 and 38, which can be used for rotating display of large museum pieces. The drop pits are an important historic feature of this structure and may be reconstructed or noted as an interpretive feature within the museum and shop.

**Café:**

The café occupies the easternmost portion of the Roundhouse. Interior walls float within the space and partition off a prep kitchen and storage area, as well as restrooms.

**Reuse of Historic Elements within the Roundhouse:**

The rehabilitation scheme proposes minimal changes to the exterior of the historic structure, and recommends graffiti removal with “gentlest means possible” in accordance with NPS Preservation Brief 38 - Removing Graffiti from Historic Masonry. The proposed rehabilitation scheme will use existing door openings for entrances and accommodate existing window openings into the new program uses. At the east side, original wood window frames and sashes will be fully reconstructed with glass and at the west side only the wood window frames will be reconstructed with a steel mesh infill for security. Six of the historic wood doors are proposed to be reconstructed. Three of the doors would be at the east end at the outer curved wall and three would be on the west side at the inner curved wall.

Chimneys and interior hood/vents would be retained at each stall and reinforced as necessary. The cast iron columns and beams at the inner curved wall will be retained as an element of the arcade and the remaining existing wood structure will be retained, except where damaged by fire or rot. New steel posts and beams will replace those that have been damaged or destroyed by fire at the western portion of the building. The locations of the historic railroad track and pits will be reflected in the new concrete floor slab through changes in color, pattern, material, or scoring to indicate original locations throughout the open air and enclosed area of the building. Where columns are missing or removed and not replaced, original locations will also be identified by treatment of the concrete slab. Historic rail will be reintroduced and historic pits may be reintroduced at the new slab location in the Railroad museum program space.

**Site Improvements:**

Although not part of this scope, the historic turntable pit and tracks are an integral part of understanding the history of the site and should be memorialized in some way to preserve the historic context. At this

time it is assumed that the concrete turntable pit will be reconstructed as a planter and historic interpretive element. The site area at the inside curve of the roundhouse may be a combination of hardscape and landscape features with permeable pavers or pavement treatment that references the previous presence of rails. At the outer curved wall north of the Roundhouse, the design may include a simple sidewalk and scored concrete apron with minimal landscape treatment. At the reconstructed wooden barn door entrance to the Railroad museum at the north side, special pavement treatment with reference to the previous presence of rail lines at each of the three barn door openings could suggest the previous function of the building at this location. Throughout the site perimeter pole lighting reminiscent of the historic light fixtures will illuminate the surrounding park space and building entrances.

**Structural Considerations for Retrofit:**

The structural report contained in the Appendix fully describes the following structural considerations for the rehabilitation program described in this study. These considerations are consistent with seismic retrofit of similar historic structures and include:

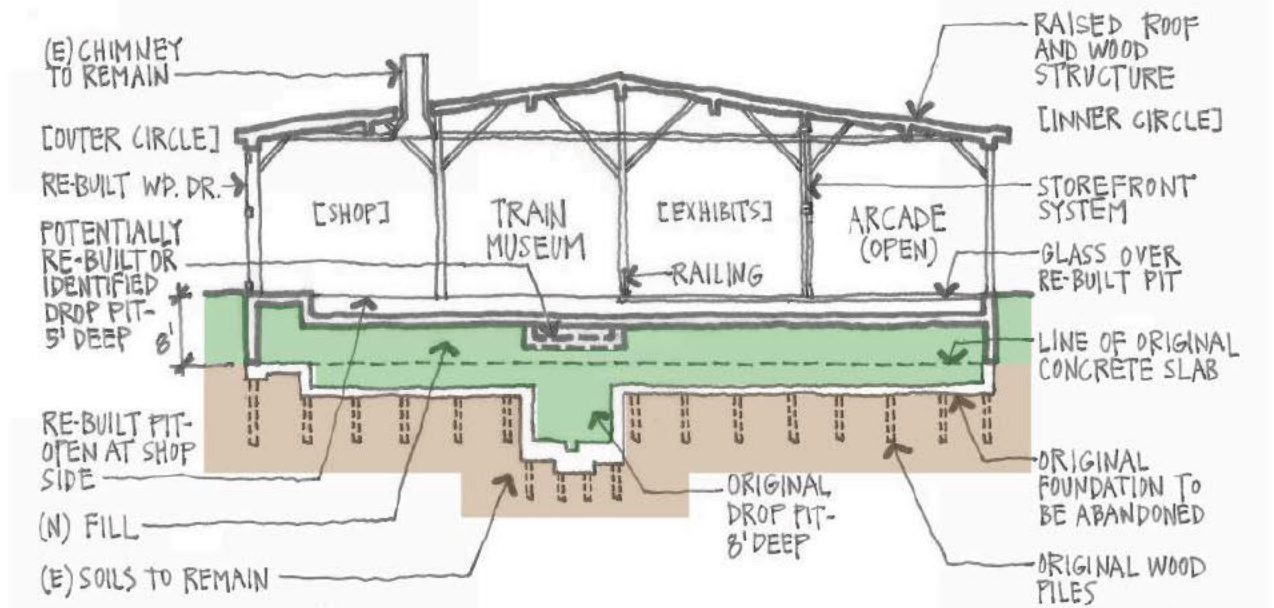
- Out-of-plane Brick Wall Bracing: The existing brick walls do not meet the height to thickness ratios required in the California Existing Building Code Appendix A and therefore require out-of-plane bracing.
- In-Plane Brick Wall Strengthening: It is likely that the two transverse solid brick walls in the building (central firewall and west wall) can support significant in-plane seismic force and would not need to be reinforced in-plane. (This would need to be confirmed with materials testing.) The brick walls with large openings, however, would likely require in-plane strengthening
- Additional Transverse Lateral-Resisting Elements: Because the distance between the interior brick firewall and the east exterior wall is large (~150 feet), the roof diaphragm will likely require an intermediate transverse lateral-resisting element.
- Additional Longitudinal Lateral-Resisting Elements: Steel columns and beams occur along the south side of the structure, which will likely not provide

enough lateral resistance at the east half of the building where the roof occurs. A new lateral-resisting element(s) will be required along the south face of the building to augment this steel framing.

- Roof Repair: Repairs to the roof will involve strengthening or replacing framing to achieve adequate strength to support gravity loads. A more detailed survey of the existing framing, and possibly material testing, will be required to evaluate the scope and approach for these members. To maintain the historic significance, the goal will be to preserve the existing framing that is exposed from underneath as much as possible. New roof plywood would be added above the existing straight sheathing to provide adequate lateral resistance.
- Roof to Wall Connections: The roof framing will be connected to the brick walls via anchor rods and some type of horizontal holdown or strap. These anchors will be spaced regularly around the perimeter of the roof to the surrounding brick. The goal will be to avoid disturbing the historic materials as much as possible.
- Foundation Strengthening: To be determined based on materials testing, approach for raising the building, and retrofit scope/approach. We recommend that a geotechnical engineer be consulted to assist with assessing existing foundations and design of new foundations if required.



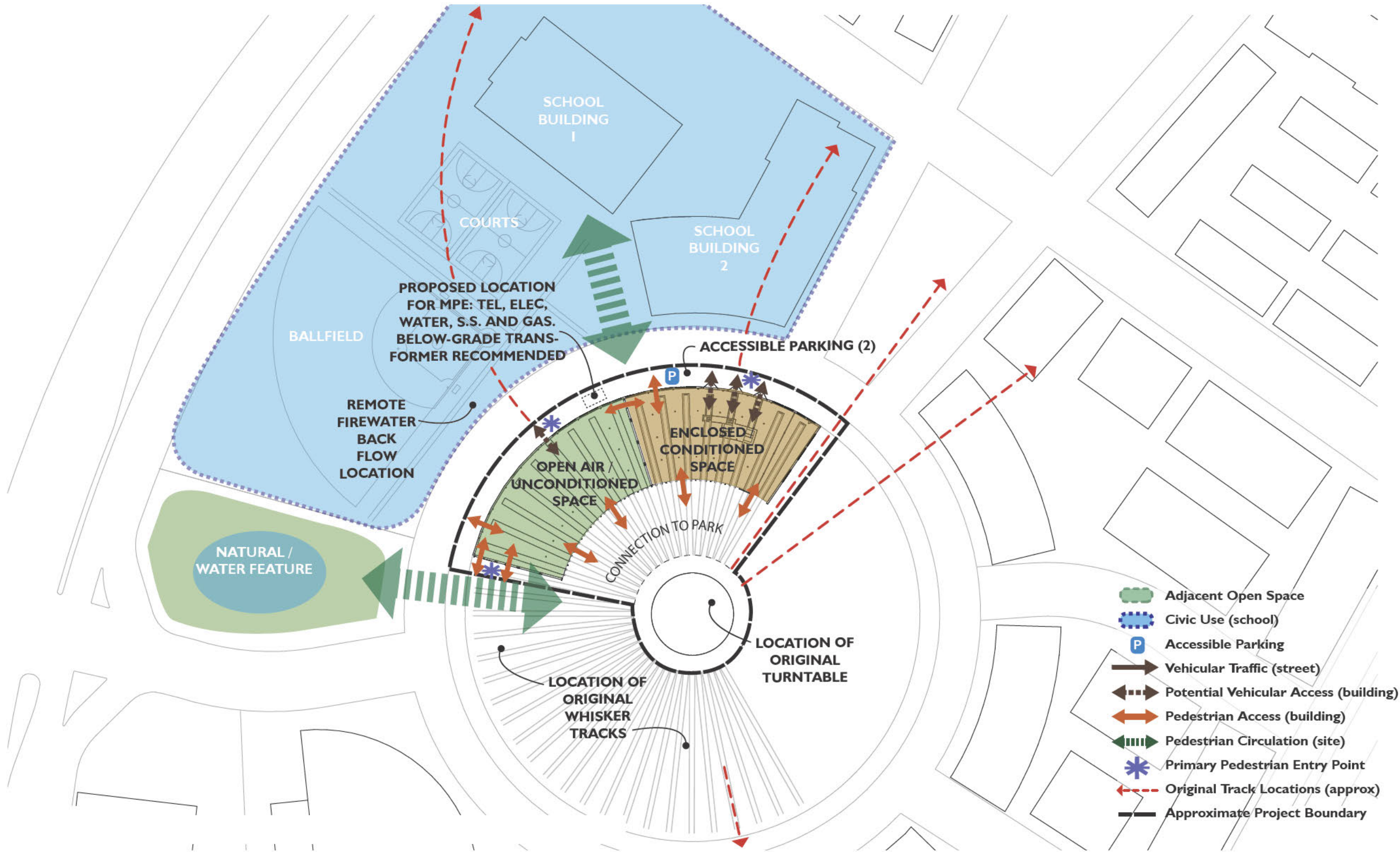
## CONCEPTUAL SKETCHES



TRANSVERSE SECTION

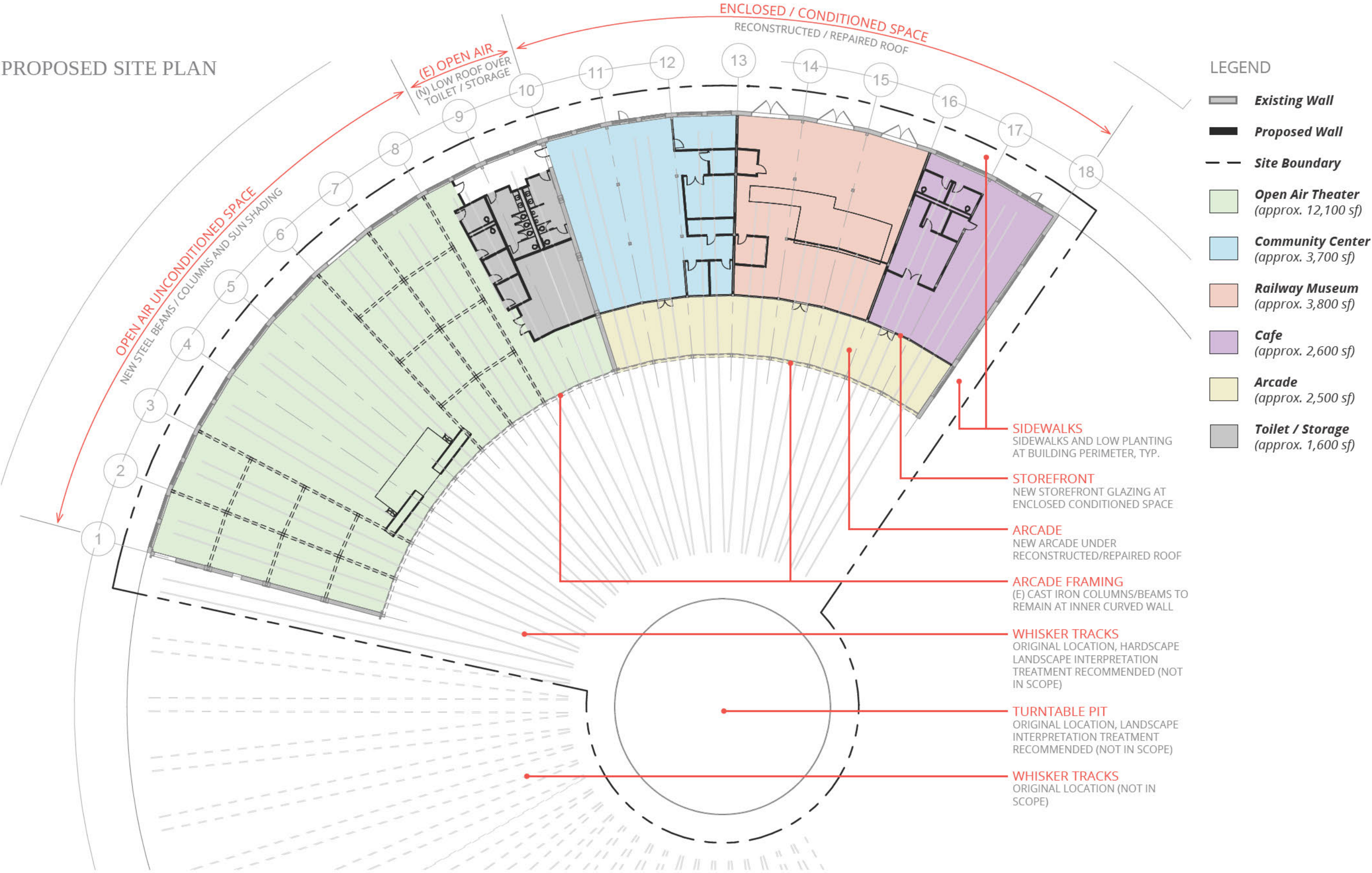


SITE ANALYSIS AND MASTER PLAN CONCEPT

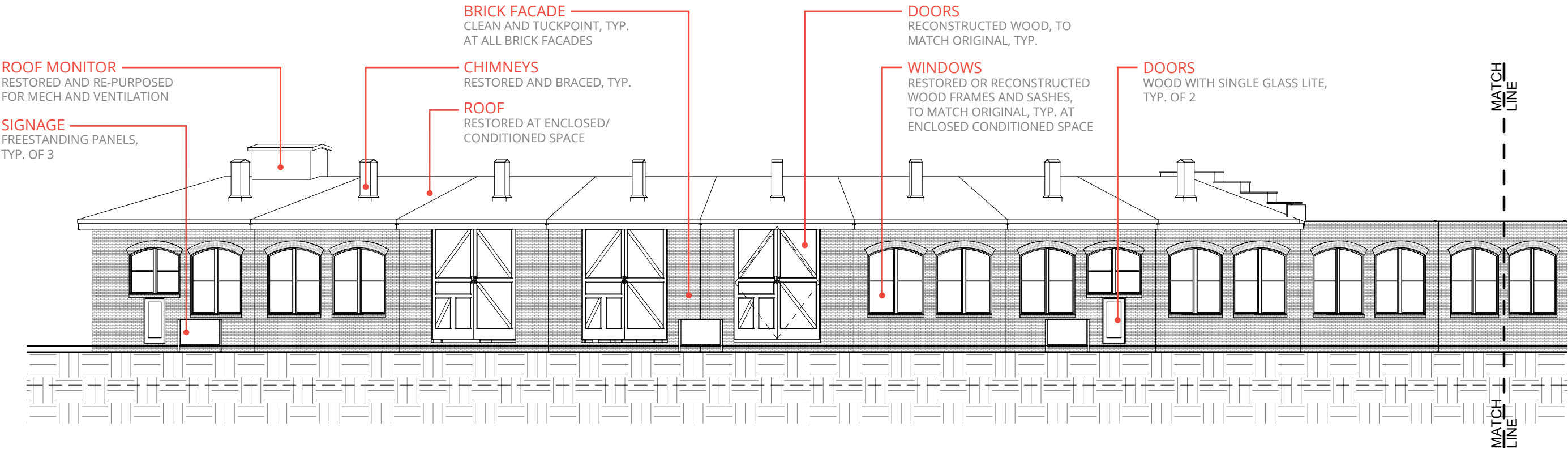




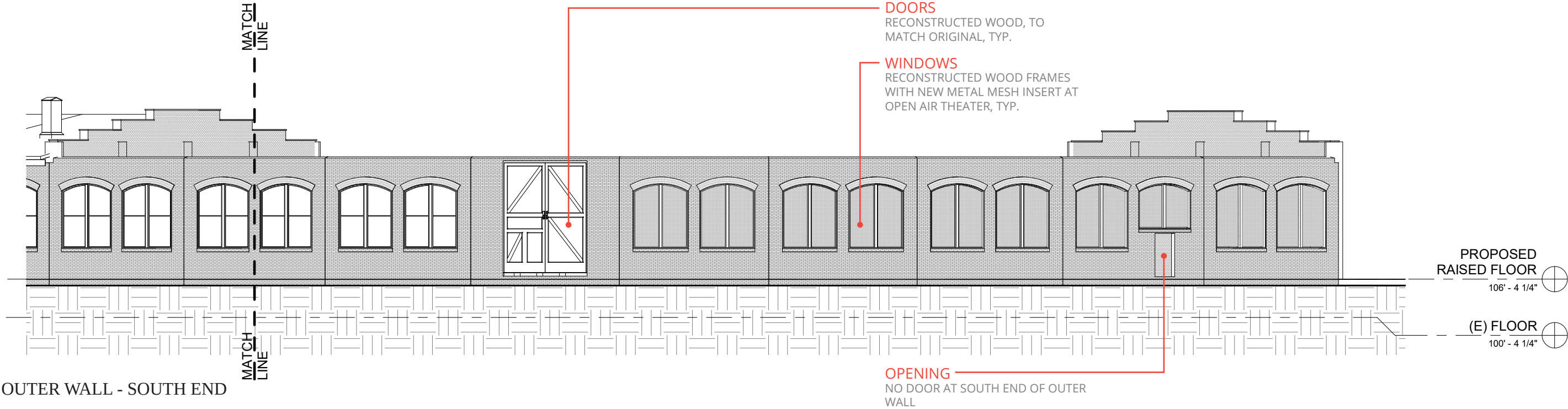
PROPOSED SITE PLAN



PROPOSED ELEVATIONS



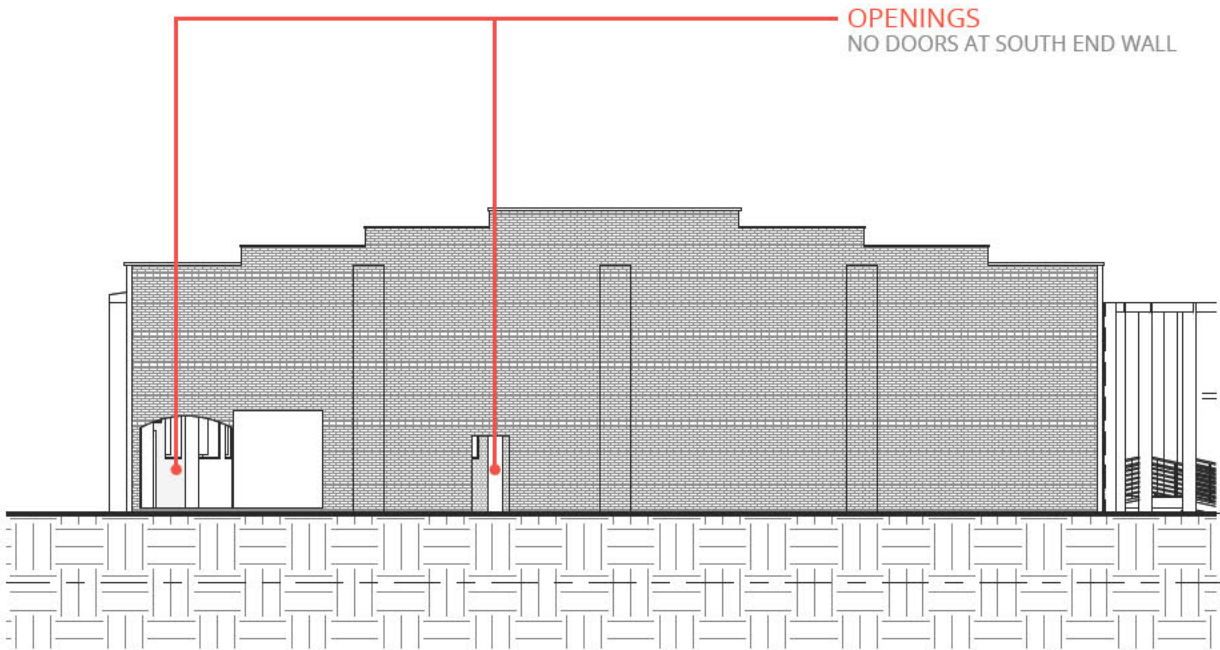
OUTER WALL - EAST END



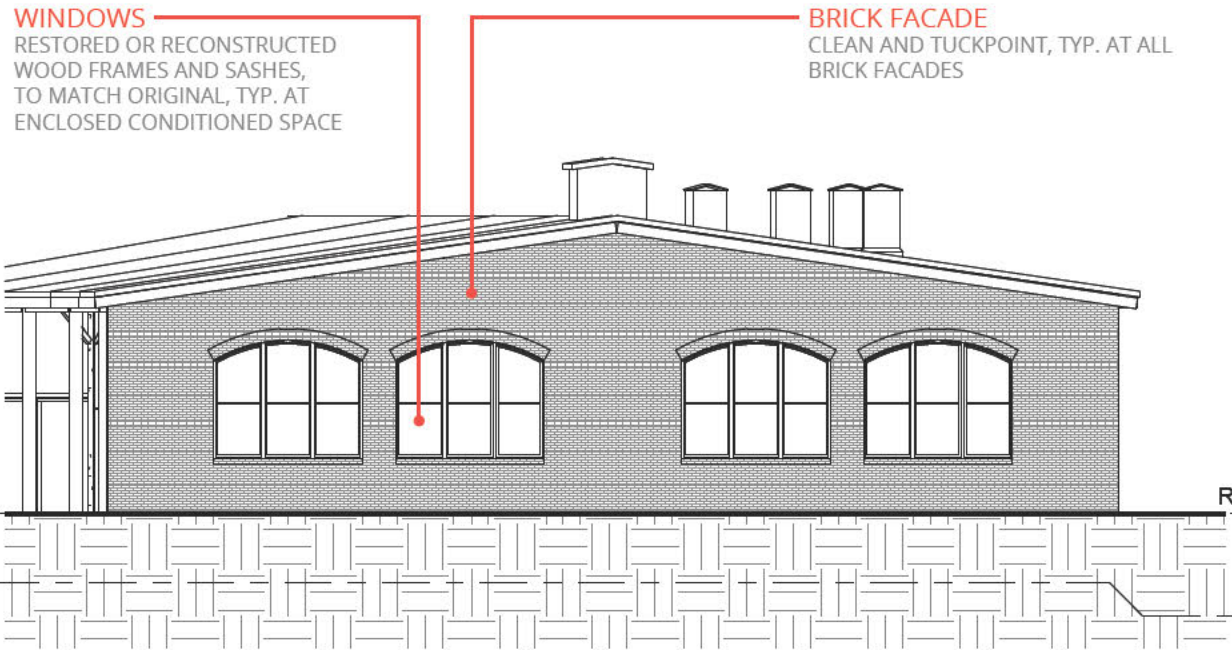
OUTER WALL - SOUTH END



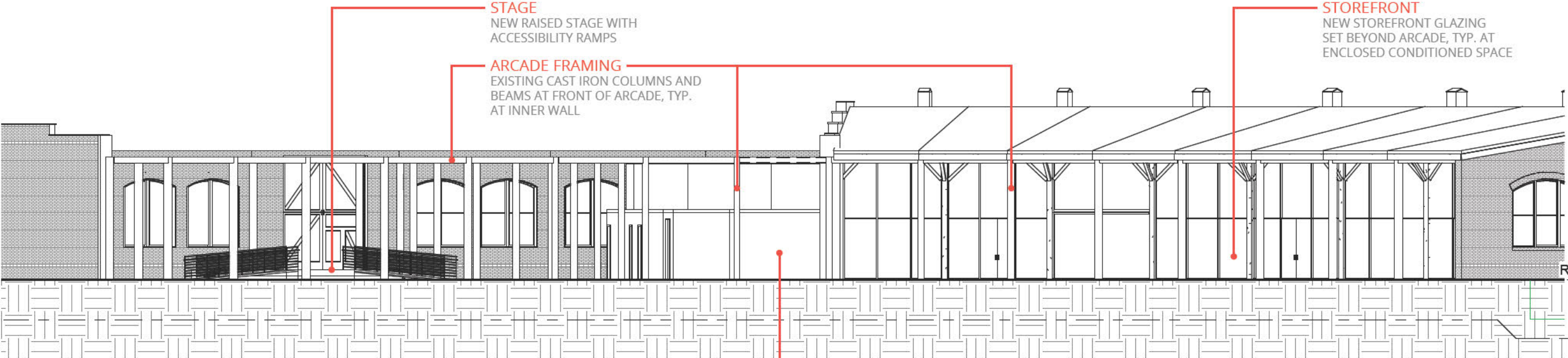
PROPOSED ELEVATIONS



SOUTH END WALL



EAST END WALL



INNER WALL

SUPPORT BUILDING  
 NEW RESTROOMS AND STORAGE  
 SPACES BEYOND

PROPOSED  
 RAISED FLOOR  
 106' - 4 1/4"

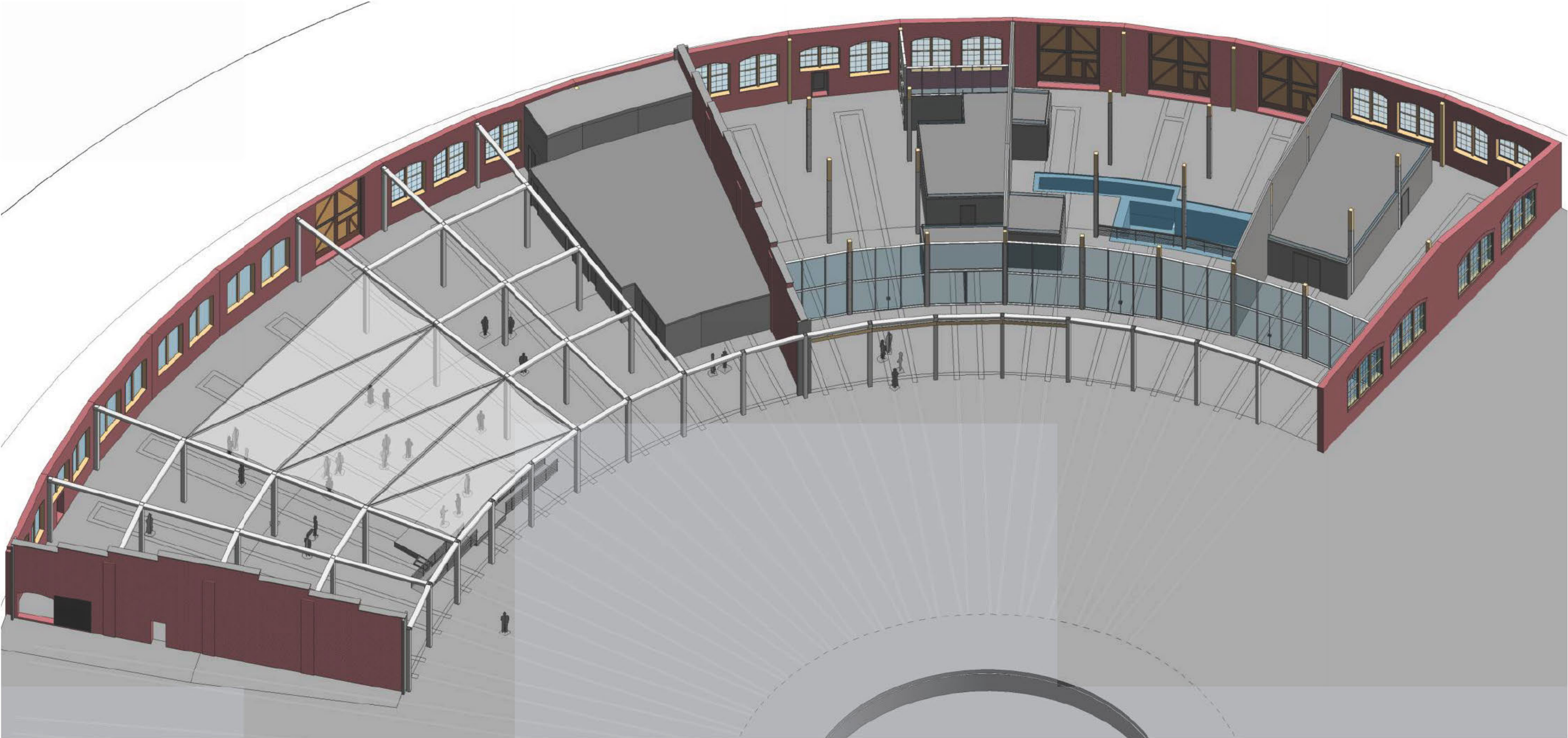
(E) FLOOR  
 100' - 4 1/4"

PROPOSED  
 RAISED FLOOR  
 106' - 4 1/4"

(E) FLOOR  
 100' - 4 1/4"



PROPOSED AXONOMETRIC VIEW



# Secretary of the Interior’s Standards Analysis

## SECRETARY OF THE INTERIOR’S STANDARDS

*The Secretary of the Interior’s Standards for the Treatment of Historic Properties with Guidelines for Preserving, Rehabilitating, Restoring and Reconstructing Historic Buildings* provides standards and guidance for reviewing proposed work on historic properties.<sup>1</sup> The Standards for the Treatment of Historic Properties are used by federal agencies in evaluating work on historic properties. They have also been adopted by local government bodies across the country for reviewing proposed rehabilitation work on historic properties under local preservation ordinances. The Standards for the Treatment of Historic Properties are a useful analytic tool for understanding and describing the potential impacts of substantial changes to historic resources. Projects that comply with the Standards for the Treatment of Historic Properties benefit from a regulatory presumption that they would have a less-than-significant adverse impact on a historic resource.<sup>2</sup> Projects that *do not* comply with the Standards for the Treatment of Historic Properties may cause either

a substantial or less-than-substantial adverse change in the significance of a historic resource.

The Secretary of the Interior offers four sets of standards to guide the treatment of historic properties: Preservation, Rehabilitation, Restoration, and Reconstruction.

**Preservation:** The Standards for Preservation “require retention of the greatest amount of historic fabric, along with the building’s historic form, features, and detailing as they have evolved over time.”

**Rehabilitation:** The Standards for Rehabilitation “acknowledge the need to alter or add to a historic building to meet continuing or new uses while retaining the building’s historic character.”

**Restoration:** The Standards for Restoration “allow for the depiction of a building at a particular time in its history by preserving materials from the period of significance and removing materials from other periods.”

**Reconstruction:** The Standards for Reconstruction “establish a limited framework for recreating a vanished or non-surviving building with new materials, primarily for interpretive purposes.”

Typically, one set of standards is chosen for a project based on the project scope. In this case, the proposed project scope is seeking to alter a historic building for a new use. Therefore, the Standards for Rehabilitation will be applied.

<sup>1</sup> Anne E. Grimmer, *The Secretary of the Interior’s Standards for the Treatment of Historic Properties with Guidelines for Preserving, Rehabilitating, Restoring and Reconstructing Historic Buildings* (U.S. Department of the Interior National Park Service Technical Preservation Services, Washington, D.C.: 2017), accessed July 6, 2020, <https://www.nps.gov/tps/standards/treatment-guidelines-2017.pdf>.

<sup>2</sup> CEQA Guidelines subsection 15064.5(b)(3).



# STANDARDS FOR REHABILITATION ANALYSIS

**1. The property will be used as it was historically or be given a new use that requires minimal change to its distinctive materials, features, spaces, and spatial relationships.**

The property has not been actively used as a railroad roundhouse since 1957, and has been vacant for decades. The majority of other Southern Pacific buildings and structures immediately surrounding the Roundhouse, including the tracks in the rail yard, have been removed—with the exception of the adjacent Lazzari Charcoal Building (Former SPRR Tank & Boiler Shop)—thus, continued use as a full-time maintenance railcar maintenance facility is not feasible. The proposed new use includes several public amenity and retail uses, which may include an outdoor amphitheater, community rooms, a railroad museum, and/or a café. Although the community and retail amenities are a new use, they do not require extensive changes to distinctive features, spaces, or materials. The museum space would be dedicated to railroad and locomotive history, which would provide a space to interpret the history of the building and site, and will contain one or more restored locomotives from the period of significance. The presence of a locomotive within the Roundhouse museum space would provide spatial and historic context to the building. The construction of new interior rooms and lifting of the building is discussed in more detail under Standard 2.

As planned, the proposed project is in compliance with Rehabilitation Standard 1.

**2. The historic character of a property will be retained and preserved. The removal of distinctive materials or alteration of features, spaces, and spatial relationships that characterize the property will be avoided.**

The proposed project includes several phases of work, including stabilization, lifting and placement on a new foundation, and rehabilitation. Initial stabilization will include shoring that is bolted through the mortar joints of the exterior brick wall, minimizing impact to the brick material. The building will need to be lifted 6 to 8 feet and placed on a new, higher foundation in order

to address concerns about anticipated sea level rise. The site has an “established flood risk level” per the definitions in the National Park Service Guidelines on Flood Adaptation for Rehabilitating Historic Buildings, thus these guidelines are appropriate to apply in the rehabilitation of the Roundhouse. Although lifting a historic building on a new foundation is not the most common or first recommended treatment to address potential flooding, in this case, other methods such as filling the basement, dry or wet floodproofing, and landscape adaptations are either not available or not acceptable long-term solutions. In this case, the whole site will be regraded, most former surrounding railyard buildings and structures have been demolished, and the Roundhouse is not part of an existing streetscape. As such, raising the Roundhouse will not substantially impact the property’s historic character with respect to its relationship to surrounding buildings. Since the whole site will be regraded, the Roundhouse’s massing and relationship with the ground plane will be retained, and no additional access points, stairs, or ramps will be required. The foundation is proposed to be constructed in a manner that is compatible with the historic character of the building, including a concrete floor that will match the design and material of the existing concrete floor, with reconstructed drop pits within the museum area. As proposed, the project meets the Guidelines on Flood Adaptation for Rehabilitating Historic Buildings with respect to planning and preparation, new foundation design and construction, access, and associated site alterations, for elevating a historic building on a new foundation.

Landscape design of the surrounding area—including plans for the concrete turntable pit, lighting poles, and whisker track area—is outside the scope of the proposed rehabilitation project at this time, and should be evaluated for compliance with the Secretary of Interior’s Standards when the design is developed.

The proposed project does not involve any new exterior vertical or horizontal additions. Several interior partitions and rooms will be constructed at the interior of the Roundhouse, as well as the south portion of the Roundhouse which is currently open due to the destruction of the roof in a 2001 fire. The interior space of the Roundhouse is characterized by a generally open volume with stalls defined by rail tracks and wood

posts in between. The character of this interior space will remain legible within each of the separated interior spaces, which will each be two to three stalls wide and have ceilings below the roof of the Roundhouse. The space at the south end of the Roundhouse will remain open except for restrooms along the fire wall.

The majority of character defining features of the Roundhouse will be preserved—including the massing, orientation, gable roof and wood framing, cast iron columns, brick walls, fenestration pattern, arrangement of stalls, spatial relationship to central turntable pit—retaining the overall historic character of the building.

As planned, the proposed project is in compliance with Rehabilitation Standard 2.

**3. Each property will be recognized as a physical record of its time, place and use. Changes that create a false sense of historical development, such as adding conjectural features or elements from other historical properties, will not be undertaken.**

No conjectural features or elements from other historical properties will be added to the Roundhouse building. If reconstructed, the drop pits within the museum area will be clearly presented and interpreted as reconstructed; if not reconstructed, the former location of the drop pits will be indicated by scoring or an outline in the new concrete floor. If locomotives are moved into the possible railroad museum within the rehabilitated Roundhouse, they would be locomotives that were themselves historically serviced at the Bayshore Roundhouse or of a type that would have come through the Roundhouse during the building’s the period of significance. As such, any potential trains within the museum would not create a false sense of historical development.

As planned, the proposed project is in compliance with Rehabilitation Standard 3.

**4. Changes to a property that have acquired significance in their own right will be retained and preserved.**

The Roundhouse has undergone a number of changes over the years, including changes during the 1910 to 1957 period of significance, and changes after the period of significance. Changes during the period of significance include alterations to the rooftop monitors

and smokestacks to their present configuration, addition of a wood partition wall between stalls 39 and 40, and the addition of two oversize doorways at stalls 36 and 38. These features have secondary significance, and will be retained except for the wood partition wall which will be removed for construction of the café. Changes that have occurred since the period of significance include destruction of the south portion of the roof due to fire, various graffiti, addition of a corrugated metal enclosure at stall 39, removal of the turntable pit and whisker tracks, and demolition of most surrounding railroad-related buildings and structures. None of these changes has acquired significance in its own right.

Since the wood partition wall is being removed, the proposed project is only in partial compliance with Standard 4.

**5. Distinctive materials, features, finishes and construction techniques or examples of craftsmanship that characterize a property will be preserved.**

As noted in the discussion of Standard 2, the initial stabilization will include shoring that is bolted through the mortar joints of the exterior brick wall, minimizing impact to the brick material. Although the building will be lifted to address the “established flood risk level,” the distinctive materials, features, finishes, construction techniques and examples of craftsmanship will generally be retained—including the brick exterior walls, wood trusses and framing, and cast iron columns. The whisker tracks outside the Roundhouse are no longer extant, but tracks within the covered stalls of the Roundhouse are currently extant. The proposed project would remove all the existing interior tracks in order to lift the building and construct a new foundation and concrete floor. As proposed, historic rail tracks may be installed in at least one stall for the railroad museum, and in other stalls the former location of the tracks would reflected in the new concrete floor slab through changes in color, pattern, material, or scoring. As discussed below in Standard 6, severely deteriorated or missing features will be reconstructed based on documentary and physical evidence.

As planned, the proposed project is in partial compliance with Rehabilitation Standard 5.



**6. Deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature will match the old in design, color, texture, and, where possible, materials. Replacement of missing features will be substantiated by documentary and physical evidence.**

The Roundhouse has a number of deteriorated and missing historic features due in part to a fire in 2001, as well as continued neglect and exposure to the elements. The roof and wood timber structure at portion of the building (to be enclosed conditioned space) will be restored. The brick walls are generally in good condition and will be cleaned and tuck-pointed as necessary. The roof monitor will be rehabilitated and repurposed for mechanical and ventilation systems, and the smokestacks (chimneys) will be braced in place. The roof beams and supporting columns south of the fire wall have been destroyed and/or damaged beyond repair by the fire. However, the cast iron columns and beams at the inner curve remain intact, even at the southern fire-damaged portion of the building, and will be rehabilitated and retained.

Paired wood doors at three of the oversize doorways are partially extant, but deteriorated beyond repair. Paired doors that once were located at a fourth oversize doorway are missing. The oversize paired wood doors will be reconstructed based on available historic drawings and photographs to replicate the material, design, and size of the original doors within their historic openings.

All window sashes have been removed from the building, and none were found on site. Wood frames remain in some locations, but are generally in poor condition and are heavily burned and partly lost in the south portion of the building that was damaged by fire. The proposed project specifies that window frames will be restored or reconstructed as necessary. Window sashes will be reconstructed to match the original size, design, material, and profile of the historic windows based on available historical drawings and photographs; the reconstructed windows will be installed at all historic openings at the north portion of the building within enclosed conditioned spaces. At the south portion of the building—at the open air auditorium—window frames will be reconstructed and new mesh metal inserts will be installed to provide

security while maintaining air flow. As planned, the proposed project is in compliance with Rehabilitation Standard 6.

**7. Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.**

The brick masonry of the Roundhouse has extensive graffiti that will require abatement. Although specific treatments have not been specified at this stage; the proposed project indicates that graffiti removal will be undertaken in accordance with guidance in “NPS Preservation Brief 38 – Removing Graffiti from Historic Masonry” and the gentlest means possible will be used when removing graffiti from brick masonry, as well as any locations on wood or metal materials. As planned, the proposed project is in compliance with Rehabilitation Standard 7.

**8. Archeological resources will be protected and preserved in place. If such resources must be disturbed, mitigation measures will be undertaken.**

The proposed project will involve excavation work to lift the historic building and construct a new foundation. No known archaeological resources are located at the Roundhouse site; however, if any archeological material is discovered during the process of excavation or construction, provided that standard discovery procedures for the City of Brisbane and Mitigation Measure 4.D-2 of the 2013 Brisbane Baylands Specific Plan EIR are followed, the proposed project will adhere to Rehabilitation Standard 8.

**9. New additions, exterior alterations, or related new construction will not destroy historic materials, features, and spatial relationships that characterize the property. The new work shall be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing to protect the integrity of the property and environment.**

The proposed project does not involve and new exterior vertical or horizontal additions. Several interior partitions and rooms will be constructed at the interior of the Roundhouse, as well as the south portion of the Roundhouse which is currently open due to the

destruction of the roof in a 2001 fire. The interior space of the Roundhouse is characterized by a generally open volume with stalls defined by rail tracks and wood posts in between. The character of this interior space will remain legible within each of the proposed new separated interior spaces—which will each be two to three stalls wide and have ceilings below the roof of the Roundhouse—and the space at the south end of the Roundhouse will remain open except for restrooms along the fire wall. A recessed arcade at the inner curve of the north portion (enclosed conditioned space) of the building will allow the arrangement of stalls, marked at the inner curve by historic cast iron columns, to remain legible. A fully glazed storefront system will be installed the recessed arcade to enclose the community, museum, and café spaces; this wall will be differentiated as a contemporary material but is compatible in its transparency as it will allow for the roof system and depth of the space to remain legible from the arcade and immediately outside the Roundhouse at the inner curve. At the open air auditorium in the south portion of the building, all former roof and framing structures were severely damaged by fire and only the concrete floor and brick walls remain intact. New steel beams and columns will be contemporary in their material and finish, but will be located at historic column locations between stalls so as to maintain the historic character of the stall arrangement. Three low free-standing signs will be installed along the outer curve of the Roundhouse, and will not affect any materials, features, or spatial relationships.

As planned, the proposed project is in compliance with Rehabilitation Standard 9.

**10. New additions and adjacent or related new construction will be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.**

The proposed project does not include any exterior vertical or horizontal additions. If any of the interior room additions or the new steel beams and posts over the amphitheater were removed in the future the essential form and integrity of the Roundhouse would be retained. The interior room additions have limited physical connection points with the historic building and have lower ceilings—below the rafters of the Roundhouse—

which ensure that the interior form of the building is retained. No new buildings are proposed within the circular area that encompasses the Roundhouse site—inclusive of the turntable pit and whisker tracks. The lifting of the Roundhouse, regrading of site, and placement of the Roundhouse on a new foundation are not easily reversible; however, these changes are being proposed in accordance with NPS Guidelines on Flood Adaptation for Rehabilitating Historic Buildings such that essential form and integrity of the historic Roundhouse will be retained.

Landscaping and hardscaping may be proposed within this area at a future time, but has not yet been designed and should be evaluated for compliance with the Secretary of Interior's Standards if/when it is proposed. New construction is proposed in the area surrounding the Roundhouse, but will have a minimum 50-foot setback and the circular area and is out of the scope of this project.

As planned, the proposed project is in compliance with Rehabilitation Standard 10.

OVERALL STANDARDS COMPLIANCE

The proposed rehabilitation of the Roundhouse, as currently designed, is in full compliance with eight of the ten Standards for Rehabilitation and is in partial compliance with two of the Standards. Overall, the proposed project is in compliance with the Standards for Rehabilitation and would not result in the loss of historic integrity or negatively affect the building's eligibility for listing on the National Register.

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# Appendix

STRUCTURAL REPORT

ZFA STRUCTURAL ENGINEERS

san francisco  
silicon valley  
sacramento  
santa rosa  
napa

Peter Birkholz  
**PAGE & TURNBULL**  
170 Maiden Lane, 5<sup>th</sup> Floor  
San Francisco, CA 94108

July 6, 2020

**RE: Brisbane Baylands Restoration Study – Structural Report**

Peter,

Please see following our recommendations and concept structural scope for the stabilization and restoration/retrofit of the brick roundhouse building in the Brisbane Baylands.

Should you have questions, please contact the undersigned.

Regards,  
**ZFA STRUCTURAL ENGINEERS**



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Introduction

This project consists of the restoration of the remnants of a registered historic, 1907-era unreinforced masonry brick, steel, and wood-framed roundhouse in Brisbane, California. The restoration is proposed to occur in phases over approximately the next 10-15 years. The structure is a single-story building composed of perimeter brick walls on the north, east, and west sides, and steel post-and-beam framing along the south “front” side, as shown in Figure 1. The building is curved in plan; its front and rear walls forming a portion of a large-diameter circle. The interior roof framing was originally wood girders supporting wood purlins and straight sheathing. The girders run in the longitudinal direction (along the curve) and are supported on a grid of equally spaced interior wood columns. The east half of the building’s roof is still largely intact, but the west half is mostly destroyed, reportedly by a fire that occurred sometime in the last decade. Generally, the building appears to have received little or no maintenance or preservation measures for several decades, has structurally significant deterioration, and is in an unsafe condition.

The foundation is composed of a 12-inch-thick mat slab on hundreds of piles, the material and depth of which are currently unconfirmed but expected to be timber. A series of approximately four-foot-deep pits run transversely across the building approximately every 16 feet, to be used to access the underside of the train cars that were meant to dock in each bay.

Existing construction documents furnished by the California State Railroad Museum were referenced for structural information for this report. These documents provide limited foundation and roof framing information and were not dated.

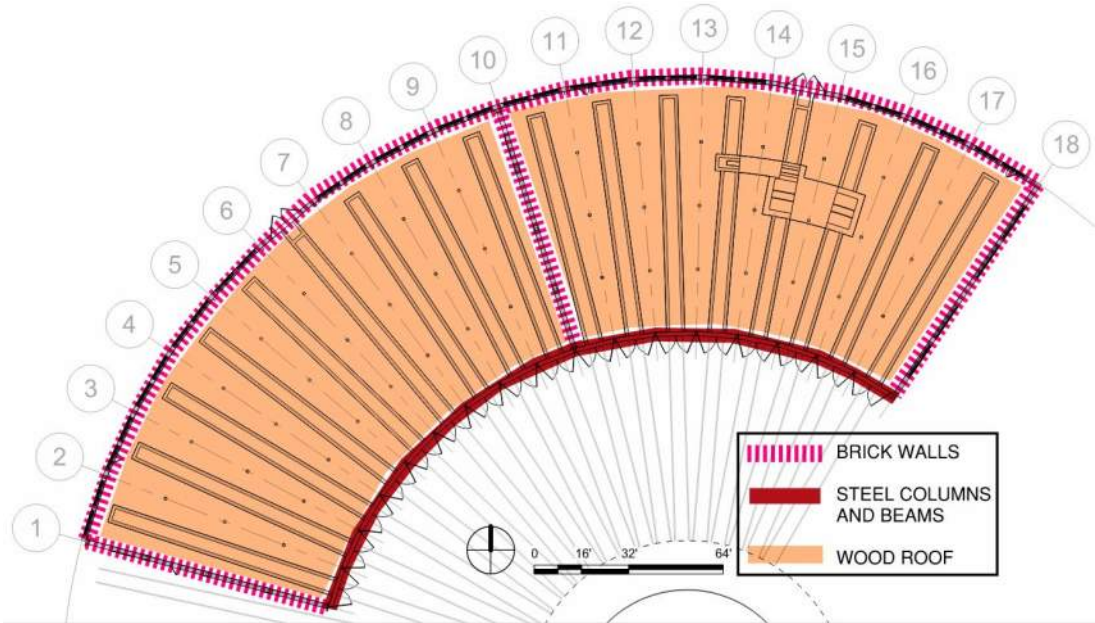


Figure 1: Plan view of the Brick Roundhouse



Project Phasing

The restoration of the roundhouse is proposed to be completed in five phases:

1. **Safety and Security Phase:** The safety and security phase will include fencing the site and installing security measures to prevent unwanted access, as well as mitigation of imminent hazards and removal of pests and plants. This phase is recommended to occur immediately while the project goes through entitlements and is expected to last from mid-2020 to the end of 2021.
2. **Initial Stabilization:** The next phase includes temporarily “mothballing” the structure to prevent further damage and deterioration. It will include protecting the structure from moisture penetration, plant and pest infestation, and stabilizing the structural components against some magnitude of wind and seismic forces. These stabilization measures are expected to be in place for two to three years, hence we would use a return period of about 20 to 25 years to quantify earthquake and wind forces.
3. **Raising the Site:** In response to expected sea level rise, this site is proposed to be raised six to eight feet. This structure is expected to be lifted and reinstated or deconstructed piece-by-piece and reconstructed at the higher elevation and on a new foundation. A comprehensive materials testing scope is proposed at the start of this phase to inform the strategy for the forthcoming restoration and retrofit.
4. **Second Stabilization:** After the site is raised, a new foundation will likely be poured, and the structure will be re-built atop the new foundation. Some portion of the final retrofit scope will be installed at this time to stabilize the structure, such as columns that need to be attached to the new foundations, steel framing to brace the brick walls, and repairs to wood roof framing. Protection of the structure will then be reinstated (similar to the initial stabilization) for protection from moisture and infestation for two to eight years to await final restoration.
5. **Restoration and Retrofit:** The final restoration of the structure will involve a seismic retrofit and strengthening of the gravity system using the California Historic Building Code to extend the life of the structure for many decades as appropriate for the occupancy programming.

Existing Conditions

A site visit was performed on May 5, 2020 by ZFA Structural Engineers (ZFA) and Page and Turnbull (P&T) to review the existing conditions of the roundhouse. The west half of the structure’s interior wood framing burned down in a fire approximately ten years ago. In the east half of the structure, the wood framing remains, although the roof is damaged in many places and allows water penetration. Only a cursory review of the framing from the ground was performed; a detailed assessment of the condition of the wood framing has not been completed.

The brick walls are three-wythe and include large window and door openings along the north and east walls. No windows remain; the openings are empty and expose the interior of the structure to the elements. The west wall and interior brick firewall are both solid with no

openings. The brick walls appear to generally be stable with no visible out-of-plane movement, except at the west wall, which appears to lean out-of-plane to some degree. No significant crumbling or mortar failure was observed. Structurally significant cracks in lintels were observed.

The steel framing at the front of the structure appears to be intact, though surficial rust is present, as it is all exposed to the elements. The columns and beams are tube steel sections, and oval cutouts on the interior face of the columns revealed the thickness of the column sections to be approximately 3/4-inch. Column anchorage was not visible and appeared to be embedded within the concrete foundation.

The existing mat slab foundation was largely obstructed by foliage and other debris, and the pits that run transversely in each framing bay appear to be infilled, either by debris or soil. The existing condition of the piles below is unknown, however there were no obvious signs of foundation damage or settlement visible in the structure.

Phase 1: Safety and Security Phase

The purpose of the safety and security phase is to secure the site against trespassers as well as address any imminent hazards. The measures below are recommended.

1. **Cleaning, Fencing, and Security:** As discussed with P&T, the site will be cleaned of foliage and debris, and security fencing and monitoring will be installed during this phase. Fencing should be installed at least a minimum distance from the building equal to the height of the exterior wall plus four feet on all sides. This is to mitigate localized building collapse and/or structural component falling hazards to anyone within or around the building.
2. **Sheet Metal Hoods:** A number of sheet metal hoods at the roof level connect to chimneys above the roof. These hoods are hanging from the roof, and their connection to the roof is not able to be confirmed without a scissor lift and/or roof access. We recommend that either the connections of these hoods are confirmed to be stable (e.g. bolts to in-tact roof elements) or that they are temporarily shored with wood framing to prevent any risk of a falling hazard.
3. **Charred Wood Removal:** Remnants of charred wood framing in the west half of the building is present, much of which is not stable or partially collapsed. All charred or partially burned wood framing should be removed. If wood members are to remain in the west portion of the structure, they are to be shored when the charred framing is removed.
4. **Notched Columns:** In at least one location, as shown in Figure 2, a wood column was observed to be significantly notched. This column should be shored or repaired (e.g. by sistered wood framing), as should any other columns that have been similarly compromised. A more detailed review of site conditions is recommended to confirm the shoring scope at wood framing.

- Cracked Brick Lintels:** In at least one location, as shown in Figure 3, a brick lintel was observed to have a significant vertical crack at the center of its span. This lintel should be shored for gravity and out-of-plane seismic loads, as should any other lintels with similar cracks. A more detailed review of site conditions is recommended to confirm the shoring scope at brick lintels.



Figure 2: Notched wood column to receive shoring.



Figure 3: Cracked lintel to receive shoring.

Phase 2: Initial Stabilization

The intent of the initial stabilization phase is to “mothball” the structure until the site is raised. “Mothballing” is meant to freeze a building in time, preventing further decay of the structure and its components. The measures below are recommended for this phase based on our visual observation of the existing conditions, guidance from *ASCE 37-14 Design Loads on Structures during Construction*, and our understanding of the architectural and historic needs.

- Sealing the Structure:** We understand that P&T will specify rodent- and weather-resistant measures to prevent effects of continued water infiltration and infestation. These measures will assist with protecting the wood framing and brick from deteriorating further from a structural perspective as well, so we also provide this recommendation.

It is anticipated that this will be achieved by covering the structure, possibly with shrink-wrapping. Any wrapping method will need to be evaluated to understand the impact on the structure, which could include how the wrap is secured to the structure, increased wind loading to the structure, and increased weight on the structural roof members. Thus, further coordination will be required as the design for this phase is developed. Covering methods should also ensure appropriate ventilation so that water is not trapped in the structure, and annual maintenance should be expected to provide any repairs to the covering that may be damaged over time.

Additionally, as covering measures are installed, we recommend that people not walk on the existing roof framing unless shoring is put in place and designed to support live loads.

- Foliage and Debris Removal:** All ivy and plants should be removed from the interior and exterior walls of the structure, if it has not already been completed in the previous phase. Any remaining debris should be removed from the interior of the structure to allow unobstructed access to all areas. In any locations where brick has been penetrated and strength loss occurred due to plant growth, those areas should be assessed for guidance on how to best remove (or not remove), repair, or shore the area.
- Shoring:** Shoring design is recommended to follow the provisions of *ASCE 37-14 Design Loads on Structures during Construction*. This standard addresses partially completed structures, temporary structures, and temporary supports used during construction. The roundhouse is considered “partially complete” due to the disrepair and damage due to fire, therefore this standard is deemed appropriate for use during the stabilization phases.

Shoring is intended to temporarily brace the brick walls out-of-plane against a seismic event corresponding to a reduced hazard level (as compared to the seismic hazard level that governs the design of permanent structures). This can be achieved via installation of a horizontal “girt” member toward the top of the wall, which is then braced with diagonal members down to a stable surface (e.g. the mat slab). These girts typically run continuously along brick walls, with diagonal braces occurring at a regular spacing, as shown in Figure 4. The diagonal wood braces would be connected to the existing slab with expansion anchors. To stabilize the wall more reliably, a girt should be placed on both the interior and exterior of the wall, “sandwiching” the brick between them. Through bolts would connect the girts together to effectively sandwich the brick.

Additionally, at the walls with large openings, in-plane bracing is recommended, which would include diagonal rod “X” bracing every second or third bay (see Figure 4).

As the longitudinal exterior (north) wall includes many regularly spaced large openings, we propose to infill these openings, both to support the lintel framing above, and also to provide a bearing surface to bolt the girt members, to avoid drilling through the brick wherever possible, as shown in Figure 5.

This method is a cost-effective way to shore the brick walls with minimal intrusion into the existing brick. If it is desired to avoid installing connectors through the brick entirely, diagonal braces would also be required to be installed on the exterior of the structure, along with concrete “dead men” to anchor the base of the diagonals. This added shoring would be less efficient, though structurally adequate. We would expect budgetary and

schedule concerns to necessitate the use of shoring on only one side and bolting through the brick as described above.



Figure 4: Temporary shoring of brick walls.

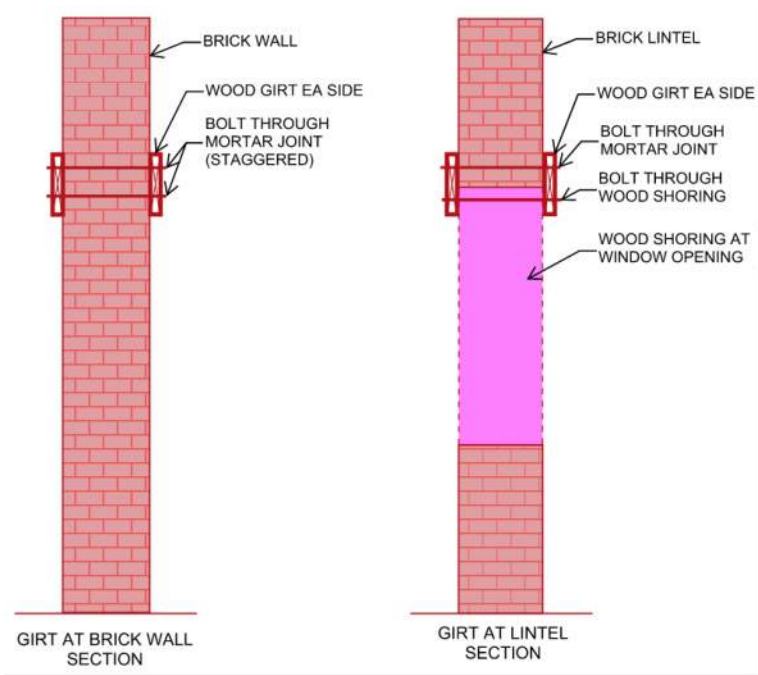


Figure 5: Section at wall showing girt shoring framing.

Phase 3: Raising the Site

In response to expected sea level rise, the site is proposed to be raised six to eight feet. Thus, the building is proposed to be raised and placed on the new site.

This phase will begin with designing the new structural components that will support the building after it is raised. While the building could be re-shored again with temporary shoring after being raised, a streamlined construction sequence that incorporates some extent of the final retrofit into the lifting phase would be more efficient. As such, we propose that the restoration and retrofit is largely designed during this phase to inform the sequence and strategy for raising the building.

The first step in the retrofit design is to develop and execute a comprehensive materials testing strategy. Materials testing will be in accordance with ASCE 41, *The National Standard for Evaluation and Retrofit of Existing Structures*, and will include testing on any material that will be relied upon structurally in the retrofit scope. This is likely to include testing on the following existing materials:

- Mortar
- Wood framing (possibly)
- Steel columns and beams
- Concrete mat slab
- Concrete reinforcement (scanning and testing)
- Pile capacity testing, if deemed necessary

Once materials testing is complete and the new structural components are designed to support the raised building, the building can be raised.

There are two options for raising the structure to the elevated level of the proposed new site.

1. **Option 1 – Raise the Structure:** The first option is to physically raise the structure by severing it from its base; supporting it from below with needling beams, shoring, and bracing; and lifting it, either onto cribbing in place or onto a truck, trailer, or other customized vehicle that can transport the portion of the building off the building pad. Once the site is raised, the building pieces would be transported back to the building pad (or lowered from the cribbing) and reconnected together and to a new foundation. This would be done by a specialized contractor, an example of which is shown in Figure 6, where a steel frame is used to brace a brick structure as it is transported.

For this option of raising the structure, two foundation approaches can be considered:

- a. **Leave the mat slab in place:** The walls and columns could be severed from the 12” mat slab foundation, raised, and placed on a new foundation. New foundations would likely consist of new concrete stem walls poured on top of the mat slab to support the structural building framing and a new ground floor slab above. The below-grade building footprint could then be infilled with gravel or compacted fill; a geotechnical engineer should be consulted to provide recommendations for fill material, compaction, and drainage. The existing foundations would need to be evaluated for the additional weight of the fill material (and new building loads) and augmentations made as part of the retrofit design before the new foundations are



installed. Instead of an infilled space, the below-grade area could also be left as an access space or storage area beneath the new building’s ground floor programming. We recommend that the new foundations are poured as soon as possible after the building is raised to limit the amount of time that the building is suspended on cribbing.

- b. *Raise the mat slab:* The mat slab could be excavated, severed from the piles below, and raised along with the walls, columns, and framing above.

Both options (“a” and “b”) can be discussed from a constructability and cost standpoint with the lifting contractors.



Figure 6: Example of moving a structure.

2. **Option 2 – Deconstruct and Reconstruct:** The second option for raising the building is to deconstruct the building piece-by-piece, possibly labeling each component and carefully storing them while the site is raised, depending on the historic requirements. Then the building is reconstructed with these pieces, while being attached to new foundations and structural strengthening elements. In this scenario, it would be possible to include a 4-inch concrete wall to replace the center wythe of brick, which would allow for fewer structural modifications to be visible on the exterior surface of the walls. With this option, it is assumed that the existing mat slab would remain and new stem walls would be built on top, as outlined in item a in Option 1.

Proposed Phase 3 Sequence

The current proposed methodology to achieve this phase (assuming the Option 1a approach) is as follows, though is still open to further discussion and consideration. A similar phase sequence would be proposed for Options 1b and 2.

1. Initiate materials testing scope.  
 2. Design restoration and retrofit as it pertains to the raised structure.

3. Shore the structure as needed for lifting (typically designed and installed by the lifting contractor), including needling beams at the base of brick walls, vertical supports adjacent to brick walls, and shoring under wood roof framing and wood/steel columns.  
 4. Sever the brick walls and wood/steel columns from the foundation.  
 5. Lift the structure (walls and roof) ~10 feet onto cribbing.  
 6. Install new concrete stem walls and columns extending up from the existing mat slab; incorporate anchorage of new structural elements (columns, braces) into new concrete, as needed. Reinforce existing foundations as required.  
 7. Infill the footprint of the structure with compacted soil, gravel, or sand in accordance with geotechnical recommendations.  
 8. Install a new structural slab over the new stem walls and columns; incorporate anchorage of new structural elements (columns, braces) into new concrete, as needed.  
 9. Lift the existing building off the cribbing, remove the cribbing, and lower the existing building onto the new foundation.  
 10. Attach the existing building to the new foundation and new structural elements.

Phase 4: Second Stabilization

Similar to the initial stabilization (Phase 2), the intent of the second stabilization phase is to “mothball” the structure until the restoration and retrofit can be completed, if it cannot be completed immediately following the previous phase. The end of Phase 3 includes re-attachment of the structure to the foundation and likely some portion of the final retrofit being installed. Any remaining structural elements that are not yet stabilized as part of Phase 3 will be temporarily braced in this phase (e.g. shoring for roof members that have not yet been repaired). The structure will be shrink-wrapped or otherwise covered similar to Phase 2.

Phase 5: Restoration and Retrofit

The intent of the restoration and retrofit will be to provide a fully code compliant building, safe for use of the structure and to extend its usable life for many more decades, which is typically at least 50 years in accordance with building code intent. The building’s use and the historic preservation strategy of the structure are currently in development and will heavily influence the structural strategy for the retrofit of the building.

Figure 7 indicates the current plan for the programming of the new space. Demising walls between program uses (museum, café, and community center) in the enclosed space will be full height and connect to the underside of the roof decking, while interior partitions will be lower and “float” within the space in order to maintain sense of the historic openness of the building. The restrooms and storage area to the west of the firewall will float as well, with a lower ceiling.



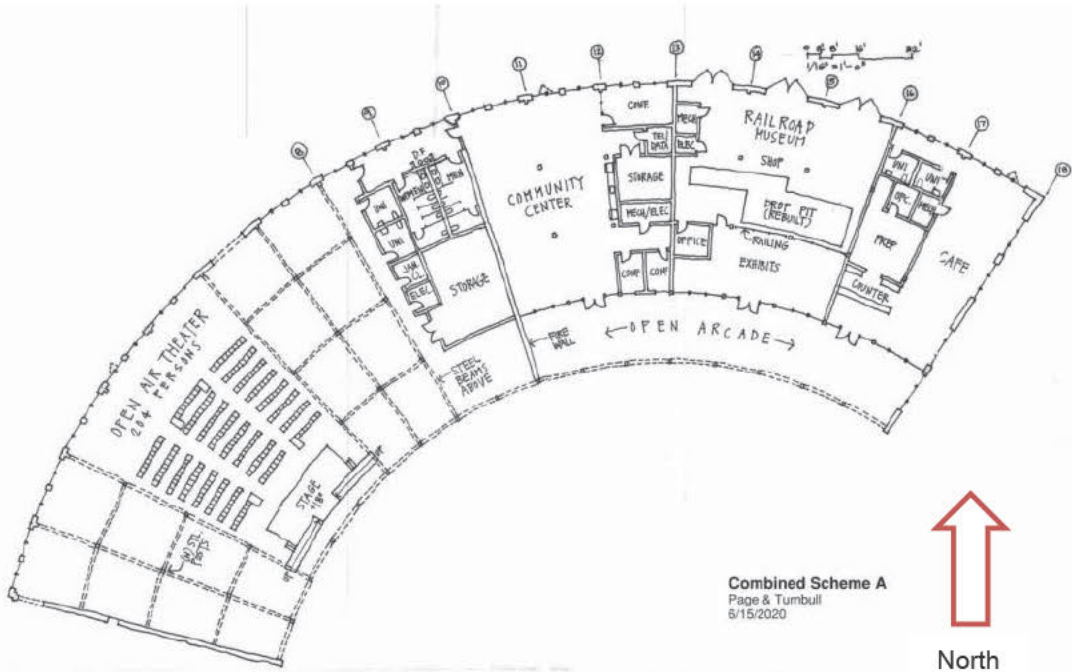


Figure 7: Schematic architectural plan of building restoration

Based on our experience with restorations of similar historic structures, we would expect a seismic retrofit to include the following items:

- Out-of-plane Brick Wall Bracing:** The existing brick walls do not meet the height to thickness ratios required in the *California Existing Building Code* Appendix A and therefore require out-of-plane bracing. Bracing of brick walls is typically provided with steel (e.g. HSS or wide flange) “strongbacks” that are placed adjacent to the brick wall and span from the ground to the roof. These strongbacks would be connected to the brick walls via some type of steel dowel connector (e.g. bolts, screws).

In the west half of the building, a new open-air theater area is proposed. In this area, no columns or roof framing will be replaced from the original design. This leaves a ~60-foot-long expanse of brick wall on the north side of the theater space, shown in Figure 8, that will not be braced at the roof level. To brace this wall out-of-plane, one solution could be to provide a series of robust cantilever steel column elements adjacent to the inside face of the wall that support a grid of horizontal girts. These members would be anchored to the brick via dowel connectors and would provide support for out-of-plane seismic (and wind) loads.



Figure 8: West half of proposed programming, 60-foot-long unbraced brick wall highlighted

- In-Plane Brick Wall Strengthening:** It is likely that the two transverse solid brick walls in the building (central firewall and west wall) can support significant in-plane seismic force and would not need to be reinforced in-plane. (This would need to be confirmed with materials testing.) The brick walls with large openings, however, would likely require in-plane strengthening, as shown in Figure 9. This could be achieved via steel moment frames, steel braced frames, or shotcrete walls applied to the face of the brick in targeted locations.

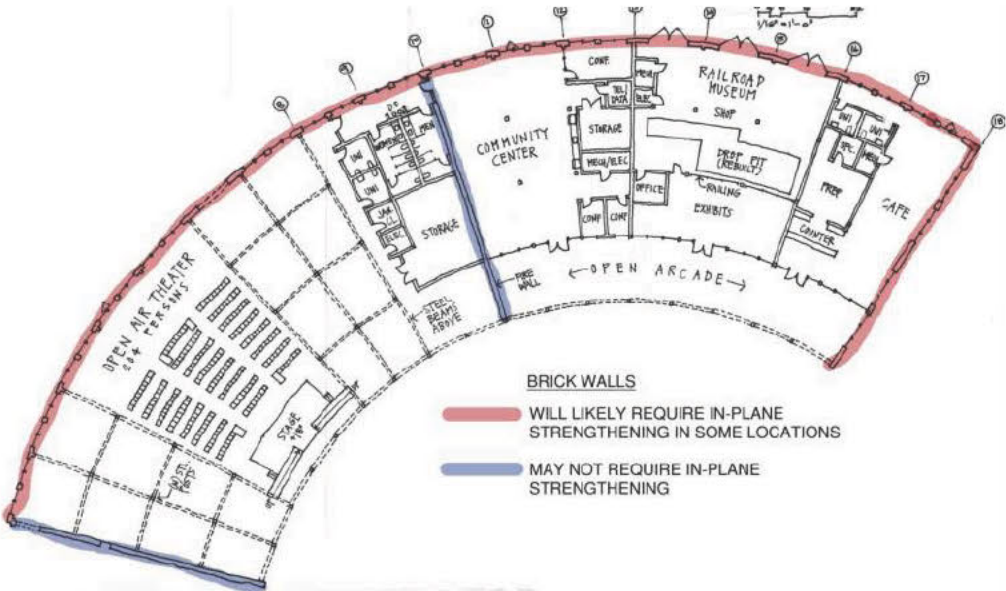


Figure 9: Schematic architectural plan showing in-plane brick wall strengthening locations

- **Additional Transverse Lateral-Resisting Elements:** Because the distance between the interior brick firewall and the east exterior wall is large (~150 feet), the roof diaphragm will likely require an intermediate transverse lateral-resisting element, such as a steel moment frame, a steel braced frame, or a wood shear wall. We would expect this element to be required along Grids 13 and/or 16 in the proposed architectural layout, as shown highlighted in Figure 10.

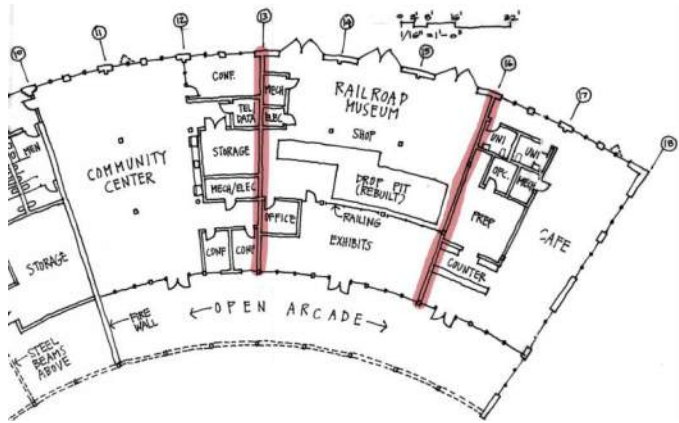


Figure 10: Plan of east half of structure, lines of proposed new lateral resistance shown highlighted (shear wall, brace frame, etc.)

- **Additional Longitudinal Lateral-Resisting Elements:** Steel columns and beams occur along the south side of the structure, which will likely not provide enough lateral resistance at the east half of the building where the roof occurs. A new lateral-resisting element(s) will be required along the south face of the building to augment this steel framing. Based on the current proposed programming configuration, it may be preferred to place a new lateral element(s) between the open arcade and other interior community spaces, as shown in Figure 11. Based on the proposed architectural configuration of glazing in this area, it appears that a steel moment frame or braced frame may be preferred.

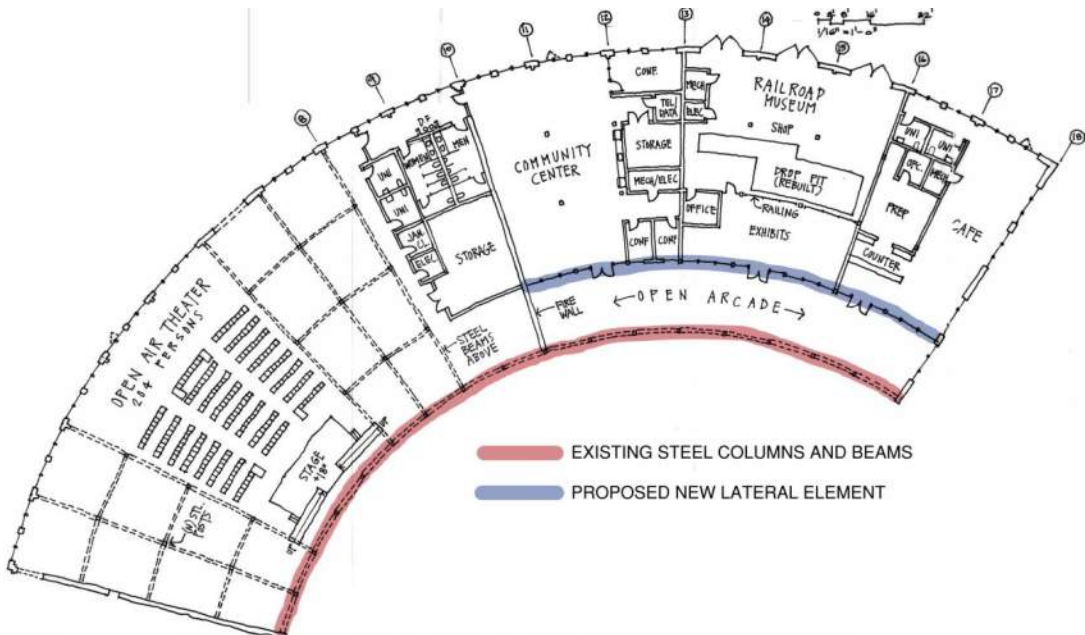
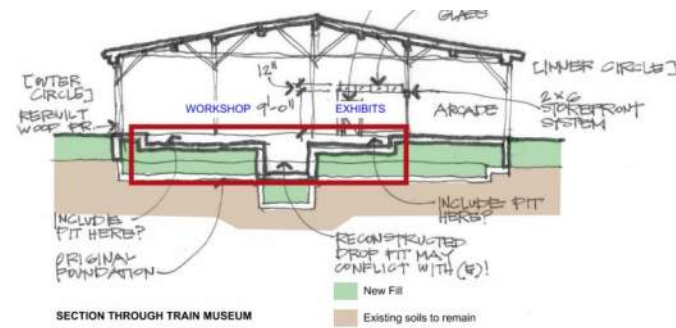


Figure 11: Schematic architectural plan showing existing and proposed steel framing along the south face

- **Roof Repair:** Repairs to the roof will involve strengthening or replacing framing to achieve adequate strength to support gravity loads. A more detailed survey of the existing framing, and possibly material testing, will be required to evaluate the scope and approach for these members. To maintain the historic significance, the goal will be to preserve the existing framing that is exposed from underneath as much as possible. New roof plywood would be added above the existing straight sheathing to provide adequate lateral resistance.
- **Roof to Wall Connections:** The roof framing will be connected to the brick walls via anchor rods and some type of horizontal holdown or strap. These anchors will be spaced regularly around the perimeter of the roof to the surrounding brick. The goal will be to avoid disturbing the historic materials as much as possible.
- **Foundation Strengthening:** To be determined based on materials testing, approach for raising the building, and retrofit scope/approach. We recommend that a geotechnical engineer be consulted to assist with assessing existing foundations and design of new foundations if required.

In addition to seismic retrofits and strengthening, the proposed plan involves reconstruction of one of the “drop pits” that currently exist in the slab, as shown in Figures 12 and 13. We recommend that this pit is placed to avoid conflict with columns above. Additionally, we recommend that the depth of the pit is limited to avoid conflict with the existing mat slab foundation. Otherwise, the pit will be constructed of concrete along with the new ground floor slab and will be configured to match the existing conditions as much as possible.





*Figure 12: Section through building with proposed new drop pit highlighted*

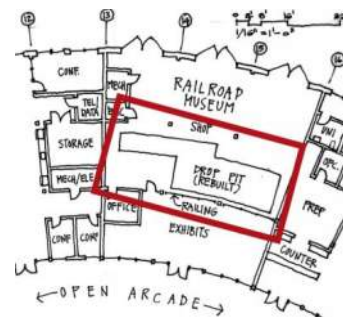


Figure 13: Plan view of location of proposed rebuilt drop pit

## Closing

The process and methodology of completing the renovation and retrofit of this century-old building will require extensive coordination, thoughtful solutions, and creative methods. ZFA is pleased to be able to bring a practical, informed perspective to the preservation of this important historical resource and looks forward to working with the design and construction teams to implement this exciting, yet challenging, project.



PAGE&TURNBULL

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170 MAIDEN LANE, 5TH FLOOR SAN FRANCISCO, CALIFORNIA 94108 TEL 415.362.5154

# Appendix B

## **Site Records**





**TABLE B-1**  
**CULTURAL RESOURCES RECORDED WITHIN 0.5 MILE OF THE PROJECT AREA**  
**[REDACTED]**



State of California — The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary  
HRI #  
Trinomial  
**NRHP Status Code**

Other Listings  
Review Code

Reviewer

Date

Page 1 of 6

**\*Resource Name or #:** 21-27 Industrial Way

**P1. Other Identifier:** 23 Industrial Way, 25 Industrial Way, 27 Industrial Way

**\*P2. Location:** ☐ Not for Publication ☒ Unrestricted

**\*a. County** San Mateo

**\*b. USGS 7.5' Quad** San Francisco South

**Date** 1981 Within unsectioned portion of Cañada de Guadalupe land grant

c. Address 21 Industrial Way

City Brisbane

Zip 94005

d. UTM: Zone 10N, 552271 mE/ 4172752 mN; NAD 83

e. Other Locational Data: APN 005-310-120

**\*P3a. Description:**

The subject property is a 0.881-acre lot that includes a large warehouse building. The building has an irregular footprint, a concrete slab foundation, and a low pitched, side gabled roof. The standing seam, metal clad walls sit atop concrete stem walls. The areas surrounding the building are paved with the area between the building and Industrial Way to the west stripped for parking.

The primary (west) façade includes four large metal roll-up doors, four pedestrian doors, and four windows. The windows are aluminum framed sliders covered by metal security bars. A four-bay boarded up opening is present at the center of the west façade. There are no window or door openings on the south façade; a single flood light is centered on the façade below the gabled peak. There are no window or door openings on the north façade. The east (rear) façade is stepped with four planes; the widest part of the building is the southern portion. The east (rear) facade was not visible from the right-of-way.

**\*P3b. Resource Attributes:** HP6. 1-3 story commercial building

**\*P4. Resources Present:** ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)

P5b. Description of Photo: West and south  
facades, looking north, ESA 2023.



**\*P6. Date Constructed/Age and Source:**  
1965 (loopnet.com)

☒ Historic ☐ Prehistoric ☐ Both

**\*P7. Owner and Address:**

L & C Diversified LLC  
150 Executive Park Blvd Ste 4000  
San Francisco CA 94134

**P8. Recorded by:**

Becky Urbano and Kathy Cleveland, ESA  
2600 Capitol Avenue, #200  
Sacramento, CA 95816

**\*P9. Date Recorded:**

February 7, 2023

**\*P10. Survey Type:**

Intensive

**\*P11. Report Citation:** Sims, Cleveland, and Grady, *Baylands Specific Plan, Cultural Resources Technical Report*, Prepared by Environmental Science Associates, Prepared for Metis Environmental Group, 2023.

**\*Attachments:** ☐ NONE ☒ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record  
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record  
☐ Artifact Record ☐ Photograph Record ☐ Other (List):

## BUILDING, STRUCTURE, AND OBJECT RECORD

\*Resource Name or # (Assigned by recorder) 21-27 Industrial Way \*NRHP Status Code 6Z

Page 2 of 6

B1. Historic Name: none  
B2. Common Name: none  
B3. Original Use: Light industrial B4. Present Use: Light industrial  
\*B5. Architectural Style: Utilitarian  
\*B6. Construction History: (Construction date, alterations, and date of alterations)  
1965 – original construction

\*B7. Moved? ☒No ☐Yes ☐Unknown Date: n/a Original Location: n/a

\*B8. Related Features:

none

B9a. Architect: unknown b. Builder: unknown  
\*B10. Significance: Theme Mid-Century Industrial Development Area Brisbane  
Period of Significance 1965 Property Type Industrial Applicable Criteria n/a  
(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

### Brisbane

As the subject property is in the City of Brisbane, which was not incorporated until the 1960s, the early history of the area is best understood from the history of San Mateo County. "San Mateo County was formed in April 1856 out of the southern portion of then-San Francisco County with a population of fewer than 5,000 people and little in the way of commerce." (County of San Mateo, 2023) It is 455 square miles and is bordered by the Pacific Ocean to the west, San Francisco Bay and Santa Clara County to the east, the City and County of San Francisco to the north, and Santa Cruz County to the south (County of San Mateo, 2023). A large portion of the County is occupied by open space including state parks while population centers are primarily located along the two coast lines. Brisbane is located at the northern end of the county between South San Francisco and Millbrae.

[Continued on page 3]

B11. Additional Resource Attributes: (List attributes and codes) none

\*B12. References:

B13. Remarks:  
none

(Sketch Map with north arrow required.)  
See location map

\*B14. Evaluator: Amber Grady, ESA  
\*Date of Evaluation: February 16, 2023

(This space reserved for official comments.)



**\*B10. Significance:** (Continued from page 2)

*Southern Pacific Railroad*

The subject property is located on land that was previously owned and occupied by the Southern Pacific Railroad. The following context is an excerpt from Carey & Company's technical report that was prepared for the Visitation Valley Redevelopment EIR (Carey & Company, 2008):

The Southern Pacific Railroad Company was one of the most powerful and prominent of American railroad companies in the nineteenth and twentieth centuries. Founded in 1865 by San Francisco businessmen, with the aim to construct a line between San Francisco and San Diego, California, it eventually merged with Central Pacific Railroad Company in 1870, expanding the reach of the western portion of the First Transcontinental Railroad in the United States. In 1904, the Southern Pacific Railroad Company had begun filling in land along the Bay and building their tunnel, still extant, at the corner of Blanken and Tunnel avenues. Southern Pacific provided commuter service to Visitation Valley on the line called the "Bayshore cutoff," which was known as the most important access route for the San Francisco Peninsula.

The creation of the Cutoff was the product of massive efforts to fill, level and dig through the land in order to connect the burgeoning industrial factory district of the South Bay to the heart of the city and spur development. In 1907, the cutoff was complete, and gave Southern Pacific trains a quick and more direct route into San Francisco. Shop facilities for repairing cars and locomotives were constructed along the Cutoff. A freight yard was established adjacent to the tracks and was noted as the major classification yard for the San Francisco terminal. Freight traffic in the Bay Area experienced a gradual decline in the 1970s, and by 1979, the Bayshore freight yard began moving yard activity to South San Francisco. In 1996, Southern Pacific Railroad Company was succeeded by Union Pacific Railroad. The former lots belonging to Southern Pacific are currently under the ownership of Universal Paragon, and the buildings are now occupied by various commercial organizations.

*Bayshore Boulevard*

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. "According to the 1913-1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse" (Carey & Company, 2008). "The Schlage Lock Factory was well established by this time, with numerous industrial buildings lining the eastern side of Bayshore Boulevard" (Carey & Company, 2008).

*Incorporation*

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. San Francisco to the north Daly City to the northwest and South San Francisco to the south each could have annexed the area. Development pressure was also felt from the Southern Pacific railroad, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a subsidiary of the Crocker Estate Company (Oral History Associates, Inc., 1989). Prior to incorporation, the area was under the control of San Mateo County (Oral History Associates, Inc., 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased the threat of annexation as the development would need to be annexed into only one of the surrounding cities in order to provide services such as police, fire, water, and sewer (Oral History Associates, Inc., 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc., 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted to incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc., 1989). The subject property is north of the originally incorporated city.

*Crocker Land Company*

The Crocker Land Company, a subsidiary of Crocker Estate Company, owned properties throughout California (Oral History Associates, Inc., 1989). The owner of the Crocker Estate Company was Charles Crocker, founder of Crocker Bank (Oral History Associates, Inc., 1989). "Its San Mateo County holdings included all of the undeveloped property on San Bruno Mountain and a large tract of tidelands on the east side of the Bayshore Freeway" (Oral History Associates, Inc., 1989). "The town of Brisbane was almost completely surrounded by Crocker-owned property" (Oral History Associates, Inc., 1989).

*Industrial Development*

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both Southern Pacific Railroad and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History

\*Recorded by: Amber Grady, ESA

\*Date: February 2023

☒ Continuation

☐ Update

Associates, Inc., 1989). Some of Southern Pacific Railroad's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc., 1989). The subject property is located on what was, in the early 1960s, lands owned by Southern Pacific Railroad (Oral History Associates, Inc., 1989). Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the Southern Pacific ceased operations.

Subject Property

The following is an excerpt from Paleowest's Cultural Resource Technical Report (Paleowest, 2022):

The original bay shoreline is approximately followed by Bayshore Boulevard. Construction of the Southern Pacific Railroad began in the early 1900s, extending the railroad tracks across the Bay. Following the 1906 San Francisco earthquake, the area west of the rail corridor was filled in primarily with demolition rubble.

On the east side of the rail corridor was a municipal landfill site which was established in the 1930s. [The subject property is to the west of the rail corridor and was developed prior to 1946 (Historicaerials.com, 1946). Deposition began in the north and it progressed southward until expansion of the landfill was terminated in the 1960s at the edge of what is now Brisbane Lagoon. The construction of US Highway 101 in the mid-1950s established the easternmost boundary of the Bay fill. The Brisbane Landfill site is 364 acres and is bounded by the Union Pacific/Joint Powers Board railroad corridor (Caltrain tracks) to the west, US Highway 101 to the east, and Brisbane Lagoon to the south. After the landfill operation was discontinued in 1967, approximately 20 to 30 ft of soil was placed over the site. Since the 1940s, the landfill has been developed into an industrial area and the existing lumberyards and warehouse buildings having been built on the oldest part of the landfill.

The subject property is developed with a large, light industrial warehouse that allows for four separate tenants. It was constructed in 1965.

Occupants

1993 – PSSI, Inc. (sewer testing, cleaning and inspection) (Public Works Journal Corporation, 1993)

2011 – Bayporter Express (27 Industrial Way), New Century Roofing Co. (25 Industrial Way), Tiger's Auto Body (23 Industrial Way), Skyland Construction (21 Industrial Way)

2013 to 2017 – Bayporter Express (27 Industrial Way), Tiger's Auto Body (23 Industrial Way), New Century Roofing Co. (21 Industrial Way)

2019 – PHK Wood Flooring & Moulding (27 Industrial Way), Tiger's Auto Body (23 Industrial Way), New Century Roofing Co. (21 Industrial Way)

2021 – HK Wood Flooring & Moulding (27 Industrial Way), Arbor Upcycle (25 Industrial Way), Tiger's Auto Body (23 Industrial Way), JBI (21 Industrial Way)

2022 – HK Wood Flooring & Moulding (27 Industrial Way), Tiger's Auto Body (23 Industrial Way), JBI (21 Industrial Way)

Evaluation

ESA staff evaluated the subject property for its potential historical significance under California Register of Historical Resources Criteria 1 through 4. The subject property at 21 Industrial Way includes a light industrial building that has housed several businesses since it was constructed. The subject property and surrounding land was originally occupied by the Southern Pacific Railroad. Industrial Way was subsequently redeveloped with a variety of light industrial buildings after Southern Pacific no longer occupied the site. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. 21 Industrial Way is a utilitarian building with no distinct architectural style. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property at 21 Industrial Way would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

Integrity

Page 5 of 6

\*Resource Name or # 21-27 Industrial Way

\*Recorded by: Amber Grady, ESA

\*Date: February 2023

☒ Continuation

☐ Update

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. 21 Industrial Way does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

Summary

As the property at 21 Industrial Way does not meet any of the California Register criteria, ESA recommends it to be ineligible for listing in the California Register.

**\*B12. References:** (Continued from page 2)

Carey & Company. *Visitation Valley Redevelopment EIR Historic Resources Technical Report*. April 29, 2008.

County of San Mateo. *About San Mateo County*. Accessed February 15, 2023. <https://www.smcgov.org/about-san-mateo-county>

Oral History Associates, Inc. *Brisbane City of Stars: The First Twenty-Five Years 1961-1986*. 1989

Paleowest. *Cultural Resource Technical Report for the Brisbane Data Gap Investigation Project in the City of Brisbane, San Mateo County, California*. April 13, 2022.

Public Works Manual. United States, Public Works Journal Corporation. 1993.



## LOCATION MAP

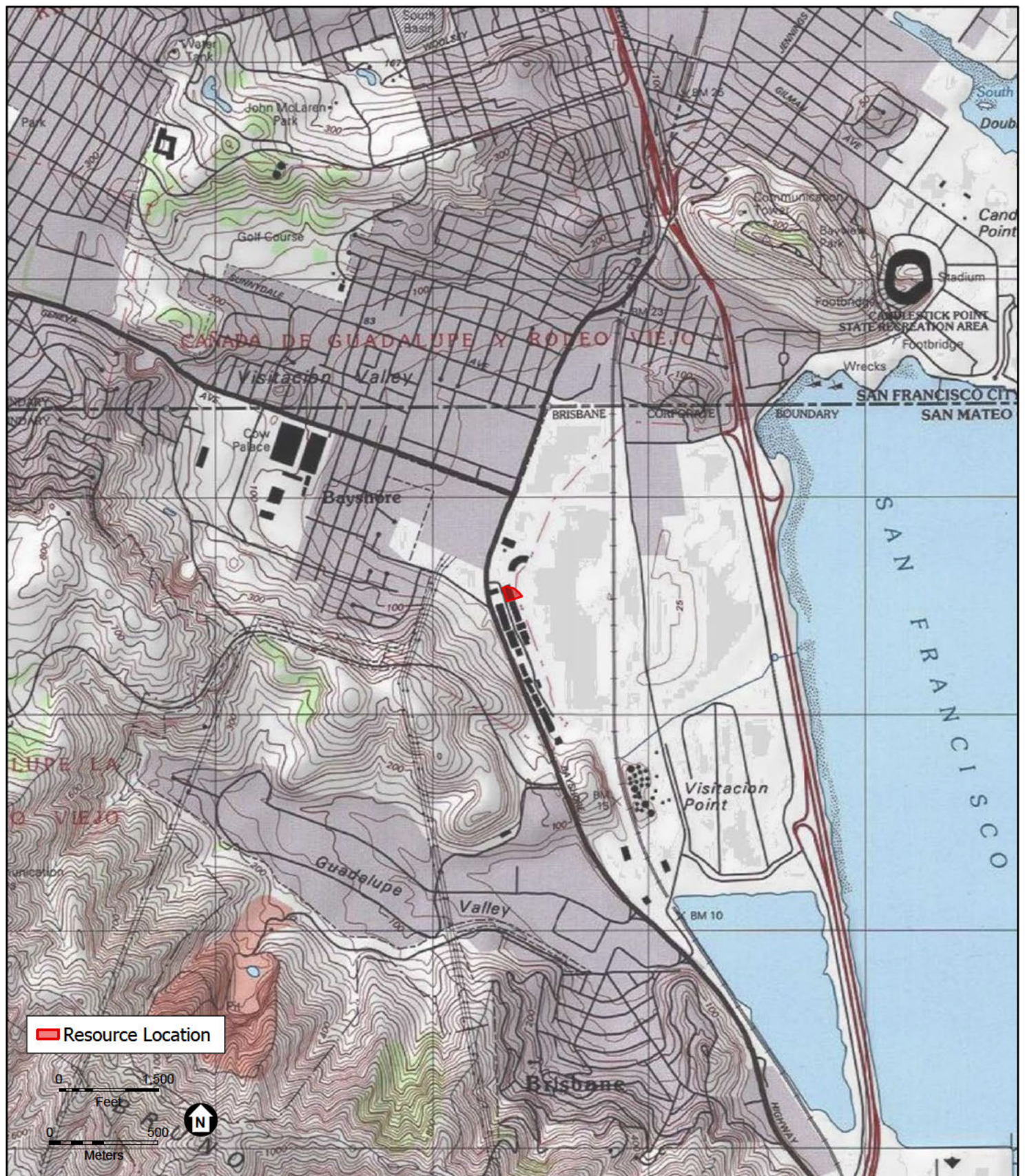
Page 6 of 6

\*Resource Name or Number: 21-27 Industrial

\*Map name: South San Francisco, CA

\*Scale: 1:24000

\*Date of Map: 1981





State of California — The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary  
HRI #  
Trinomial  
**NRHP Status Code**

Other Listings  
Review Code

Reviewer

Date

Page 1 of 6

**\*Resource Name or #:** 60-130 Industrial Way

**P1. Other Identifier:** 60 Industrial Way, 80 Industrial Way, 130 Industrial Way

**\*P2. Location:** ☐ Not for Publication ☒ Unrestricted

**\*a. County** San Mateo

**\*b. USGS 7.5' Quad** San Francisco South

**Date** 1981

Within unsectioned portion of Cañada de Guadalupe land grant

c. Address 100 Industrial Way

City Brisbane

Zip 94005

d. UTM: Zone 10N, 552232 mE/ 4172638 mN; NAD 83

e. Other Locational Data: APN 005-311-070

**\*P3a. Description:**

The subject property is a 1.912-acre lot that includes a large warehouse building. The building has a rectangular footprint, a concrete slab foundation, and a low pitched, side gabled roof. The standing seam, metal clad walls sit atop concrete stem walls. The areas that surround the building are paved with the area between the building and Industrial Way stripped for parking.

The primary (east) façade includes a loading dock that runs the full length of the façade with concrete steps at the north end. The primary façade also includes six large barn-style doors, five pedestrian doors, and four vinyl sash windows. Two of the windows appear to be horizontal sliders while the other two are either fixed or hopper style. The south façade includes one metal roll-up door and at least three windows. The north façade has no window or door openings. The west (rear) façade was not visible from the right-of-way.

**\*P3b. Resource Attributes:** HP6. 1-3 story commercial building

**\*P4. Resources Present:** ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



**P5b. Description of Photo:**  
South and east facades, looking west, ESA 2023.

**\*P6. Date Constructed/Age and Source:**

1968 (assessor data)

☒ Historic ☐ Prehistoric ☐ Both

**\*P7. Owner and Address:**

Sierra Hotel Management Corp  
150 Executive Park Blvd Ste 42  
San Francisco CA 94134

**P8. Recorded by:**

Becky Urbano and Kathy Cleveland, ESA  
2600 Capitol Avenue, #200  
Sacramento, CA 95816

**\*P9. Date Recorded:**

February 7, 2023

**\*P10. Survey Type:**

Intensive

**\*P11. Report Citation:** Sims, Cleveland, and Grady, *Baylands Specific Plan, Cultural Resources Technical Report*, Prepared by Environmental Science Associates, Prepared for Metis Environmental Group, 2023.

**\*Attachments:** ☐ NONE ☒ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record  
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record  
☐ Artifact Record ☐ Photograph Record ☐ Other (List):



## BUILDING, STRUCTURE, AND OBJECT RECORD

\*Resource Name or # (Assigned by recorder) 60-130 Industrial Way \*NRHP Status Code 6Z

Page 2 of 6

B1. Historic Name: none  
B2. Common Name: none  
B3. Original Use: Light industrial B4. Present Use: Light industrial  
\*B5. Architectural Style: Utilitarian  
\*B6. Construction History: (Construction date, alterations, and date of alterations)  
1968 original construction

\*B7. Moved? ☒No ☐Yes ☐Unknown Date: n/a Original Location: n/a

\*B8. Related Features:

none

B9a. Architect: unknown b. Builder: unknown

\*B10. Significance: Theme Mid-Century Industrial Development Area Brisbane  
Period of Significance 1968 Property Type Industrial Applicable Criteria n/a

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

### Brisbane

As the subject property is in the City of Brisbane, which was not incorporated until the 1960s, the early history of the area is best understood from the history of San Mateo County. "San Mateo County was formed in April 1856 out of the southern portion of then-San Francisco County with a population of fewer than 5,000 people and little in the way of commerce." (County of San Mateo, 2023) It is 455 square miles and is bordered by the Pacific Ocean to the west, San Francisco Bay and Santa Clara County to the east, the City and County of San Francisco to the north, and Santa Cruz County to the south (County of San Mateo, 2023). A large portion of the County is occupied by open space including state parks while population centers are primarily located along the two coast lines. Brisbane is located at the northern end of the county between South San Francisco and Millbrae.

[Continued on page 3]

B11. Additional Resource Attributes: (List attributes and codes) none

\*B12. References:

B13. Remarks:  
none

\*B14. Evaluator: Amber Grady, ESA  
\*Date of Evaluation: February 16, 2023

(This space reserved for official comments.)

(Sketch Map with north arrow required.)  
See location map

**\*B10. Significance:** (Continued from page 2)

*Southern Pacific Railroad*

The subject property is located on land that was previously owned and occupied by the Southern Pacific Railroad. The following context is an excerpt from Carey & Company's technical report that was prepared for the Visitation Valley Redevelopment EIR (Carey & Company, 2008):

The Southern Pacific Railroad Company was one of the most powerful and prominent of American railroad companies in the nineteenth and twentieth centuries. Founded in 1865 by San Francisco businessmen, with the aim to construct a line between San Francisco and San Diego, California, it eventually merged with Central Pacific Railroad Company in 1870, expanding the reach of the western portion of the First Transcontinental Railroad in the United States. In 1904, the Southern Pacific Railroad Company had begun filling in land along the Bay and building their tunnel, still extant, at the corner of Blanken and Tunnel avenues. Southern Pacific provided commuter service to Visitation Valley on the line called the "Bayshore cutoff," which was known as the most important access route for the San Francisco Peninsula.

The creation of the Cutoff was the product of massive efforts to fill, level and dig through the land in order to connect the burgeoning industrial factory district of the South Bay to the heart of the city and spur development. In 1907, the cutoff was complete, and gave Southern Pacific trains a quick and more direct route into San Francisco. Shop facilities for repairing cars and locomotives were constructed along the Cutoff. A freight yard was established adjacent to the tracks and was noted as the major classification yard for the San Francisco terminal. Freight traffic in the Bay Area experienced a gradual decline in the 1970s, and by 1979, the Bayshore freight yard began moving yard activity to South San Francisco. In 1996, Southern Pacific Railroad Company was succeeded by Union Pacific Railroad. The former lots belonging to Southern Pacific are currently under the ownership of Universal Paragon, and the buildings are now occupied by various commercial organizations.

*Bayshore Boulevard*

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. "According to the 1913-1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse" (Carey & Company, 2008). "The Schlage Lock Factory was well established by this time, with numerous industrial buildings lining the eastern side of Bayshore Boulevard" (Carey & Company, 2008).

*Incorporation*

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. San Francisco to the north Daly City to the northwest and South San Francisco to the south each could have annexed the area. Development pressure was also felt from the Southern Pacific railroad, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a subsidiary of the Crocker Estate Company (Oral History Associates, Inc., 1989). Prior to incorporation, the area was under the control of San Mateo County (Oral History Associates, Inc., 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased the threat of annexation as the development would need to be annexed into only one of the surrounding cities in order to provide services such as police, fire, water, and sewer (Oral History Associates, Inc., 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc., 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted to incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc., 1989). The subject property is north of the originally incorporated city.

*Crocker Land Company*

The Crocker Land Company, a subsidiary of Crocker Estate Company, owned properties throughout California (Oral History Associates, Inc., 1989). The owner of the Crocker Estate Company was Charles Crocker, founder of Crocker Bank (Oral History Associates, Inc., 1989). "Its San Mateo County holdings included all of the undeveloped property on San Bruno Mountain and a large tract of tidelands on the east side of the Bayshore Freeway" (Oral History Associates, Inc., 1989). "The town of Brisbane was almost completely surrounded by Crocker-owned property" (Oral History Associates, Inc., 1989).

*Industrial Development*

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both Southern Pacific Railroad and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History

\*Recorded by: Becky Urbano and Kathy Cleveland, ESA

\*Date: February 2023

☒ Continuation

☐ Update

Associates, Inc., 1989). Some of Southern Pacific Railroad's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc., 1989). The subject property is located on what was, in the early 1960s, lands owned by Southern Pacific Railroad (Oral History Associates, Inc., 1989). Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the Southern Pacific ceased operations.

#### Subject Property

The following is an excerpt from Paleowest's Cultural Resource Technical Report (Paleowest, 2022):

The original bay shoreline is approximately followed by Bayshore Boulevard. Construction of the Southern Pacific Railroad began in the early 1900s, extending the railroad tracks across the Bay. Following the 1906 San Francisco earthquake, the area west of the rail corridor was filled in primarily with demolition rubble.

On the east side of the rail corridor was a municipal landfill site which was established in the 1930s. [The subject property is to the west of the rail corridor and was developed prior to 1946 (Historicaerials.com, 1946). Deposition began in the north and it progressed southward until expansion of the landfill was terminated in the 1960s at the edge of what is now Brisbane Lagoon. The construction of US Highway 101 in the mid-1950s established the easternmost boundary of the Bay fill. The Brisbane Landfill site is 364 acres and is bounded by the Union Pacific/Joint Powers Board railroad corridor (Caltrain tracks) to the west, US Highway 101 to the east, and Brisbane Lagoon to the south. After the landfill operation was discontinued in 1967, approximately 20 to 30 ft of soil was placed over the site. Since the 1940s, the landfill has been developed into an industrial area and the existing lumberyards and warehouse buildings having been built on the oldest part of the landfill.

The subject property is developed with a large, light industrial warehouse that allows for four separate tenants. It was constructed in 1968.

#### Occupants

1974 – Brenton Equipment Co (60 Industrial Way) (United States, Department of Defense, Defense Logistics Agency, Defense Logistics Services Center, 1974)

1977 – George Short Company (80 Industrial Way) – Gasket manufacturers and packing distributor, owner Frank F. Gutierrez (Department of Commerce, Maritime Administration, 1977.)

1984-1990 – Maximum Technology (60-80 Industrial Way) (U.S. General Services Administration, 1985) (U.S. General Services Administration, 1986) (Illuminating Engineering Society of North America, 1990)

1994 – Acme Scenery Company (Sunflower Unlimited, 1994)

1997 – Mechanical Research & Design (Thomas Publishing Company, 1997)

#### Evaluation

ESA staff evaluated the subject property for its potential historical significance under California Register of Historical Resources Criteria 1 through 4. The subject property at 100 Industrial Way is a light industrial building that has housed several businesses since it was constructed. The subject property and surrounding land was originally occupied by the Southern Pacific Railroad. Industrial Way was subsequently redeveloped with a variety of light industrial buildings after Southern Pacific no longer occupied the site. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. 100 Industrial Way is a utilitarian building with no distinct architectural style. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property at 100 Industrial Way would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

#### Integrity

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. 100 Industrial Way does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

Page 5 of 6

\*Resource Name or # 60-130 Industrial Way

\*Recorded by: Becky Urbano and Kathy Cleveland, ESA

\*Date: February 2023

☒ Continuation

☐ Update

Summary

As the property at 100 Industrial Way does not meet any of the California Register criteria, ESA recommends it to be ineligible for listing in the California Register.

**\*B12. References:** (Continued from page 2)

Carey & Company. *Visitacion Valley Redevelopment EIR Historic Resources Technical Report*. April 29, 2008.

County of San Mateo. *About San Mateo County*. Accessed February 15, 2023. <https://www.smcgov.org/about-san-mateo-county>

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Lighting Design + Application: LD + A.. United States, Illuminating Engineering Society of North America, 1990.

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New Item Introductory Schedule. United States, U.S. General Services Administration, Office of Federal Supply and Services, 1985.

Nongovernment Organization Codes for Military Standard Contract Administration Procedures (MILSCAP), United States and Canada, Code to Name. United States, Department of Defense, Defense Logistics Agency, Defense Logistics Services Center, 1974.

Oral History Associates, Inc. *Brisbane City of Stars: The First Twenty-Five Years 1961-1986*. 1989

Paleowest. *Cultural Resource Technical Report for the Brisbane Data Gap Investigation Project in the City of Brisbane, San Mateo County, California*. April 13, 2022.

Public Works Manual. United States, Public Works Journal Corporation. 1993.

The Reel Directory. United States, Sunflower Unlimited, 1994.

Thomas Register of American Manufacturers and Thomas Register Catalog File. United States, Thomas Publishing Company, 1997.



## LOCATION MAP

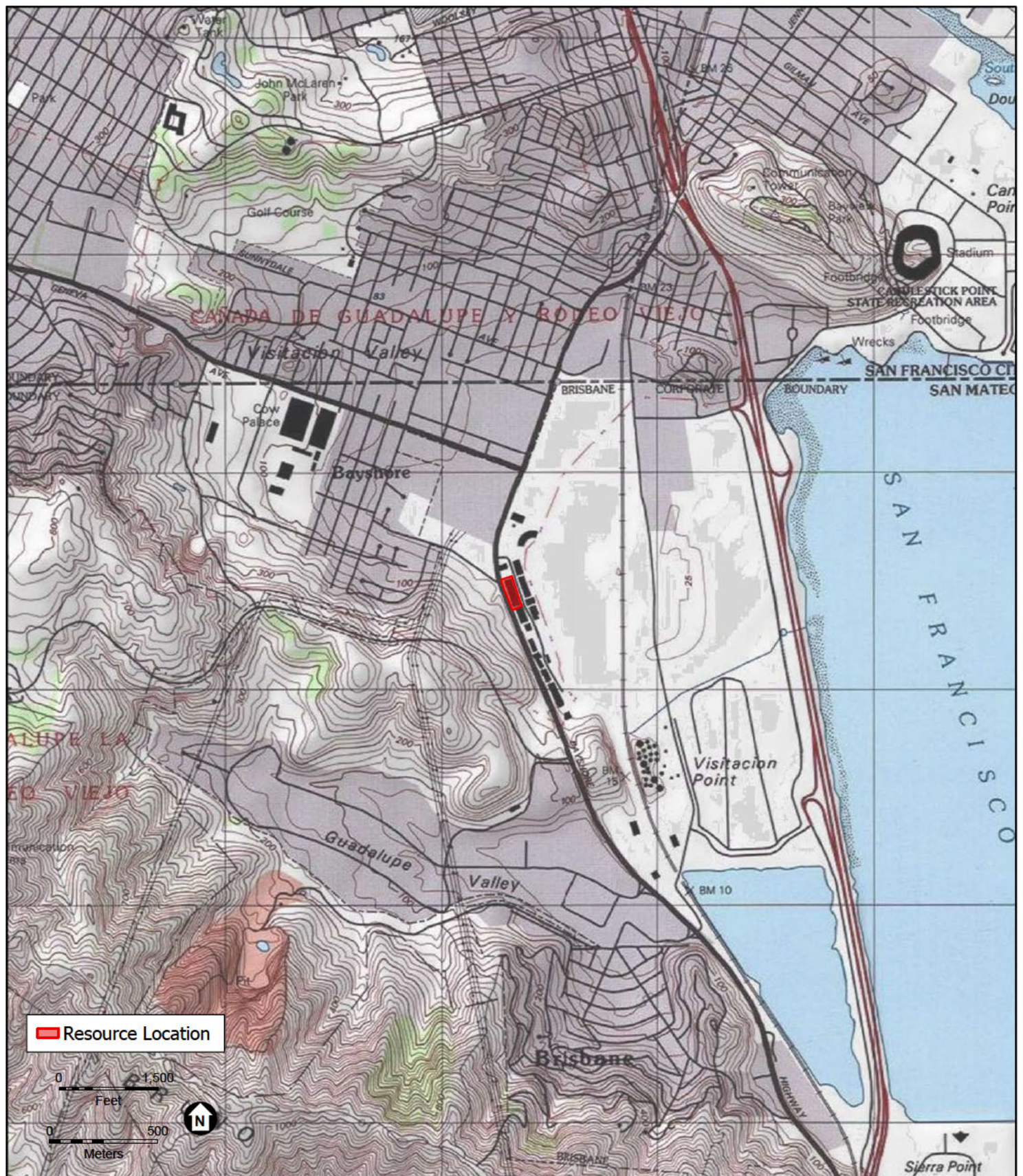
Page 6 of 6

\*Resource Name or Number: 60-130 Industrial

\*Map name: South San Francisco, CA

\*Scale: 1:24000

\*Date of Map: 1981





State of California — The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary  
HRI #  
Trinomial  
**NRHP Status Code**

Other Listings  
Review Code

Reviewer

Date

Page 1 of 6

**\*Resource Name or #:** 131 Industrial Way

**P1. Other Identifier:**

**\*P2. Location:** ☐ Not for Publication ☒ Unrestricted

**\*a. County** San Mateo

**\*b. USGS 7.5' Quad** San Francisco South

**Date** 1981

Within unsectioned portion of Cañada de Guadalupe land grant

c. Address 131 Industrial Way

City Brisbane

Zip 94005

d. UTM: Zone 10N, 552319 mE/ 4172606 mN; NAD 83

e. Other Locational Data: APN 005-312-110

**\*P3a. Description:**

The subject property is a 0.43-acre lot that includes a large warehouse building. The building has a rectangular footprint, a concrete slab foundation, and a low pitched, front gabled roof. The walls are clad in standing seam metal sheeting. The areas that surround the building are paved with the area between the building and Industrial Way to the west stripped for parking.

The primary (west) façade includes a large, metal roll-up door in the center and a pedestrian door and two windows on the north end. The north façade includes one door and three vinyl sash windows; two windows appear to be the one-over-one and the third window is a horizontal slider. The south façade does not appear to have any window or door openings; a portion of the façade is not visible due to the close proximity of the building on the adjacent parcel to the south. The east (rear) façade includes a large, metal roll-up door in the center and a single door at the north end of the façade.

**\*P3b. Resource Attributes:** HP6. 1-3 story commercial building

**\*P4. Resources Present:** ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo: facing northeast

**\*P6. Date Constructed/Age and Source:**  
1969 (assessor data)

☒ Historic ☐ Prehistoric ☐ Both

**\*P7. Owner and Address:**

Sierra Hotel Management Corp  
150 Executive Park Blvd Ste 42  
San Francisco CA 94134

**P8. Recorded by:**

Becky Urbano and Kathy Cleveland, ESA  
2600 Capitol Avenue, #200  
Sacramento, CA 95816

**\*P9. Date Recorded:**

February 7, 2023

**\*P10. Survey Type:**

Intensive

**\*P11. Report Citation:** Sims, Cleveland, and Grady, *Baylands Specific Plan, Cultural Resources Technical Report*, Prepared by Environmental Science Associates, Prepared for Metis Environmental Group, 2023

**\*Attachments:** ☐ NONE ☒ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record  
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record  
☐ Artifact Record ☐ Photograph Record ☐ Other (List):

## BUILDING, STRUCTURE, AND OBJECT RECORD

\*Resource Name or # (Assigned by recorder) 131 Industrial Way \*NRHP Status Code 6Z

Page 2 of 6

B1. Historic Name: none  
B2. Common Name: none  
B3. Original Use: Light industrial B4. Present Use: Light industrial  
\*B5. Architectural Style: Utilitarian  
\*B6. Construction History: (Construction date, alterations, and date of alterations)  
1969 original construction

\*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date: n/a Original Location: n/a

\*B8. Related Features:  
none

B9a. Architect: unknown b. Builder: unknown

\*B10. Significance: Theme Mid-Century Industrial Development Area Brisbane  
Period of Significance 1970s Property Type Industrial Applicable Criteria n/a  
(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

### Brisbane

As the subject property is in the City of Brisbane, which was not incorporated until the 1960s, the early history of the area is best understood from the history of San Mateo County. "San Mateo County was formed in April 1856 out of the southern portion of then-San Francisco County with a population of fewer than 5,000 people and little in the way of commerce." (County of San Mateo, 2023) It is 455 square miles and is bordered by the Pacific Ocean to the west, San Francisco Bay and Santa Clara County to the east, the City and County of San Francisco to the north, and Santa Cruz County to the south (County of San Mateo, 2023). A large portion of the County is occupied by open space including state parks while population centers are primarily located along the two coast lines. Brisbane is located at the northern end of the county between South San Francisco and Millbrae.

[Continued on page 3]

B11. Additional Resource Attributes: (List attributes and codes) none

\*B12. References:

B13. Remarks:  
none

(Sketch Map with north arrow required.)  
See location map

\*B14. Evaluator: Amber Grady, ESA  
\*Date of Evaluation: February 21, 2023

(This space reserved for official comments.)

**\*B10. Significance:** (Continued from page 2)

*Southern Pacific Railroad*

The subject property is located on land that was previously owned and occupied by the Southern Pacific Railroad. The following context is an excerpt from Carey & Company's technical report that was prepared for the Visitation Valley Redevelopment EIR (Carey & Company, 2008):

The Southern Pacific Railroad Company was one of the most powerful and prominent of American railroad companies in the nineteenth and twentieth centuries. Founded in 1865 by San Francisco businessmen, with the aim to construct a line between San Francisco and San Diego, California, it eventually merged with Central Pacific Railroad Company in 1870, expanding the reach of the western portion of the First Transcontinental Railroad in the United States. In 1904, the Southern Pacific Railroad Company had begun filling in land along the Bay and building their tunnel, still extant, at the corner of Blanken and Tunnel avenues. Southern Pacific provided commuter service to Visitation Valley on the line called the "Bayshore cutoff," which was known as the most important access route for the San Francisco Peninsula.

The creation of the Cutoff was the product of massive efforts to fill, level and dig through the land in order to connect the burgeoning industrial factory district of the South Bay to the heart of the city and spur development. In 1907, the cutoff was complete, and gave Southern Pacific trains a quick and more direct route into San Francisco. Shop facilities for repairing cars and locomotives were constructed along the Cutoff. A freight yard was established adjacent to the tracks and was noted as the major classification yard for the San Francisco terminal. Freight traffic in the Bay Area experienced a gradual decline in the 1970s, and by 1979, the Bayshore freight yard began moving yard activity to South San Francisco. In 1996, Southern Pacific Railroad Company was succeeded by Union Pacific Railroad. The former lots belonging to Southern Pacific are currently under the ownership of Universal Paragon, and the buildings are now occupied by various commercial organizations.

*Bayshore Boulevard*

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. "According to the 1913-1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse" (Carey & Company, 2008). "The Schlage Lock Factory was well established by this time, with numerous industrial buildings lining the eastern side of Bayshore Boulevard" (Carey & Company, 2008).

*Incorporation*

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. San Francisco to the north Daly City to the northwest and South San Francisco to the south each could have annexed the area. Development pressure was also felt from the Southern Pacific railroad, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a subsidiary of the Crocker Estate Company (Oral History Associates, Inc., 1989). Prior to incorporation, the area was under the control of San Mateo County (Oral History Associates, Inc., 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased the threat of annexation as the development would need to be annexed into only one of the surrounding cities in order to provide services such as police, fire, water, and sewer (Oral History Associates, Inc., 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc., 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted to incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc., 1989). The subject property is north of the originally incorporated city.

*Crocker Land Company*

The Crocker Land Company, a subsidiary of Crocker Estate Company, owned properties throughout California (Oral History Associates, Inc., 1989). The owner of the Crocker Estate Company was Charles Crocker, founder of Crocker Bank (Oral History Associates, Inc., 1989). "Its San Mateo County holdings included all of the undeveloped property on San Bruno Mountain and a large tract of tidelands on the east side of the Bayshore Freeway" (Oral History Associates, Inc., 1989). "The town of Brisbane was almost completely surrounded by Crocker-owned property" (Oral History Associates, Inc., 1989).

*Industrial Development*

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both Southern Pacific Railroad and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History

\*Recorded by: Amber Grady, ESA

\*Date: February 2023

☒ Continuation

☐ Update

Associates, Inc., 1989). Some of Southern Pacific Railroad's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc., 1989). The subject property is located on what was, in the early 1960s, lands owned by Southern Pacific Railroad (Oral History Associates, Inc., 1989). Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the Southern Pacific ceased operations.

*Subject Property*

The following is an excerpt from Paleowest's Cultural Resource Technical Report (Paleowest, 2022):

The original bay shoreline is approximately followed by Bayshore Boulevard. Construction of the Southern Pacific Railroad began in the early 1900s, extending the railroad tracks across the Bay. Following the 1906 San Francisco earthquake, the area west of the rail corridor was filled in primarily with demolition rubble.

On the east side of the rail corridor was a municipal landfill site which was established in the 1930s. [The subject property is to the west of the rail corridor and was developed prior to 1946 (Historicaerials.com, 1946). Deposition began in the north and it progressed southward until expansion of the landfill was terminated in the 1960s at the edge of what is now Brisbane Lagoon. The construction of US Highway 101 in the mid-1950s established the easternmost boundary of the Bay fill. The Brisbane Landfill site is 364 acres and is bounded by the Union Pacific/Joint Powers Board railroad corridor (Caltrain tracks) to the west, US Highway 101 to the east, and Brisbane Lagoon to the south. After the landfill operation was discontinued in 1967, approximately 20 to 30 ft of soil was placed over the site. Since the 1940s, the landfill has been developed into an industrial area and the existing lumberyards and warehouse buildings having been built on the oldest part of the landfill.

The subject property is developed with a large, light industrial warehouse that was constructed in 1969.

Occupants

2013 to 2017 – Davey Tree

2019 to 2023 – Traffic Management Incorporated

Evaluation

ESA staff evaluated the subject property for its potential historical significance under California Register of Historical Resources Criteria 1 through 4. The subject property at 131 Industrial Way is a light industrial building that has housed several businesses since it was constructed. The subject property and surrounding land was originally occupied by the Southern Pacific Railroad. Industrial Way was subsequently redeveloped with a variety of light industrial buildings after Southern Pacific no longer occupied the site. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. 131 Industrial Way is a utilitarian building with no distinct architectural style. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property at 131 Industrial Way would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

Integrity

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. 131 Industrial Way does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

Summary

As the property at 131 Industrial Way does not meet any of the California Register criteria, ESA recommends it to be ineligible for listing in the California Register.

\*B12. **References:** (Continued from page 2)



CONTINUATION SHEET

Trinomial

Page 5 of 6

\*Resource Name or # 131 Industrial Way

\*Recorded by: Amber Grady, ESA

\*Date: February 2023

☒ Continuation

☐ Update

Carey & Company. *Visitacion Valley Redevelopment EIR Historic Resources Technical Report*. April 29, 2008.

County of San Mateo. *About San Mateo County*. Accessed February 15, 2023. <https://www.smcgov.org/about-san-mateo-county>

Oral History Associates, Inc. *Brisbane City of Stars: The First Twenty-Five Years 1961-1986*. 1989

Paleowest. *Cultural Resource Technical Report for the Brisbane Data Gap Investigation Project in the City of Brisbane, San Mateo County, California*. April 13, 2022.

Public Works Manual. United States, Public Works Journal Corporation. 1993.



## LOCATION MAP

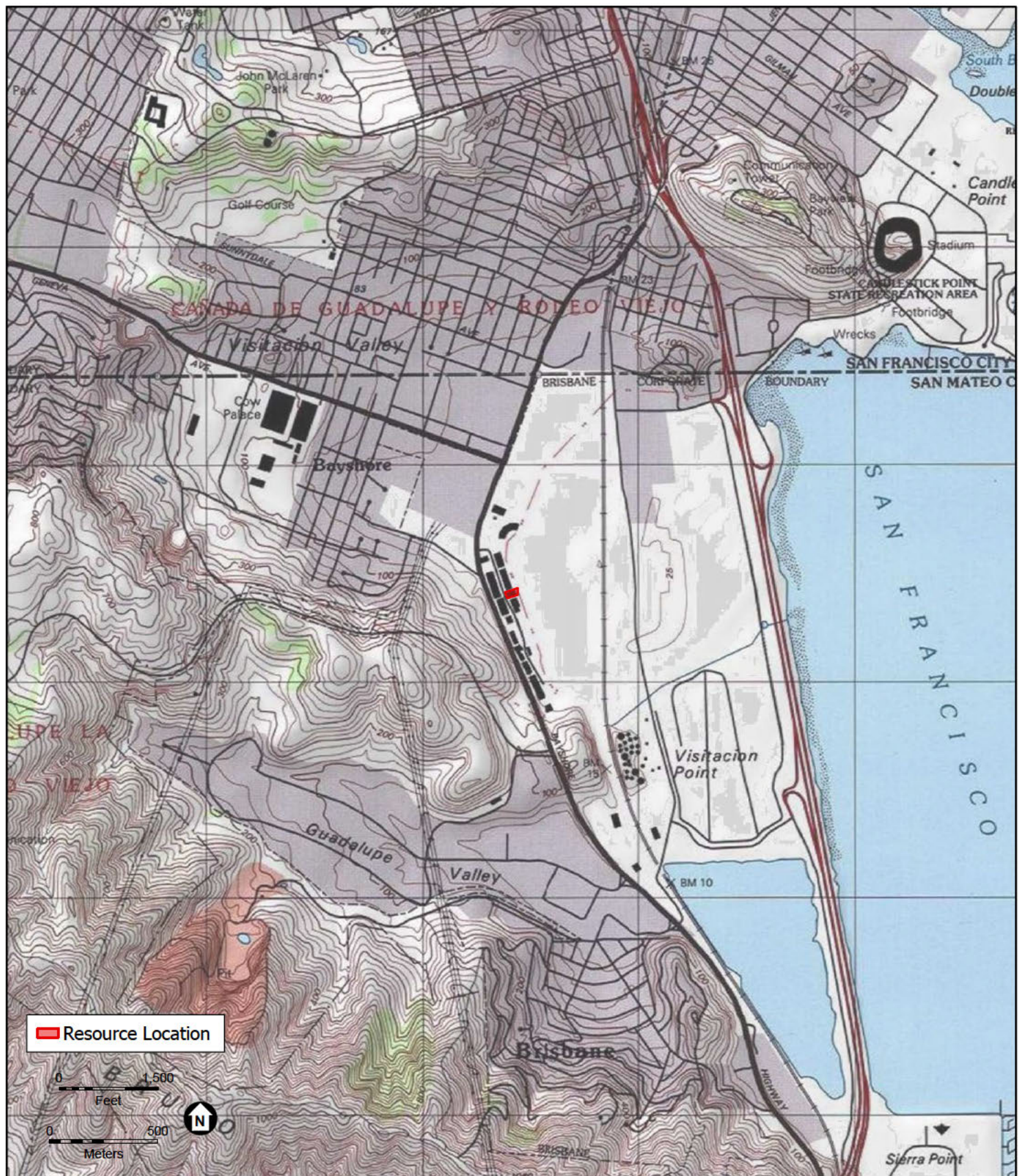
Page 6 of 6

\*Resource Name or Number: 131 Industrial

\*Map name: South San Francisco, CA

\*Scale: 1:24000

\*Date of Map: 1981





State of California — The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary  
HRI #  
Trinomial  
**NRHP Status Code**

Other Listings  
Review Code

Reviewer

Date

Page 1 of 6

**\*Resource Name or #:** 140-190 Industrial Way

**P1. Other Identifier:** 140 Industrial Way, 150 Industrial Way, 180 Industrial Way, 190 Industrial Way

**\*P2. Location:** ☐ Not for Publication ☒ Unrestricted

**\*a. County** San Mateo

**\*b. USGS 7.5' Quad** San Francisco South

**Date** 1981 Within unsectioned portion of Cañada de Guadalupe land grant

c. Address 151 Industrial Way

City Brisbane

Zip 94005

d. UTM: Zone 10N, 552270 mE/ 4172540 mN; NAD 83

e. Other Locational Data: APN 005-311-100

**\*P3a. Description:**

The subject property is a 1.033-acre lot that includes a large warehouse building. The building has an irregular footprint, a concrete slab foundation, and a low pitched, side gabled roof. The roof extends out of the east (primary) façade creating a wide eave. The walls are clad in standing seam metal sheets. The areas surrounding the building are paved with the area between the building and Industrial way stripped for parking.

The primary (east) façade includes three planes; the south end of the building is the widest (west to east) with the walls stepped back in each of the two segments to the north. The southern-most section includes six doors and one window; four of the doors are large metal roll-up style and two are solid pedestrian style. The middle section includes two pedestrian doors, one solid and one glazed, and two aluminum slider windows. The northern-most section includes two large metal roll-up doors and two pedestrian doors; this section of the façade is symmetrical with one each of these types of doors at each end.

The north façade has no door or window openings. The south façade has three windows that appear to have aluminum sashes. The west façade was not visible from the right-of-way.

**\*P3b. Resource Attributes:** HP6. 1-3 story commercial building

**\*P4. Resources Present:** ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo:

**\*P6. Date Constructed/Age and Source:**  
1971 (assessor data)

☒ Historic ☐ Prehistoric ☐ Both

**\*P7. Owner and Address:**

Sierra Hotel Management Corp  
150 Executive Park Blvd Ste 42  
San Francisco CA 94134

**P8. Recorded by:**

Becky Urbano and Kathy Cleveland, ESA  
2600 Capitol Avenue, #200  
Sacramento, CA 95816

**\*P9. Date Recorded:**

February 7, 2023

**\*P10. Survey Type:**

Intensive

**\*P11. Report Citation:** Sims, Cleveland, and Grady, *Baylands Specific Plan, Cultural Resources Technical Report*, Prepared by Environmental Science Associates, Prepared for Metis Environmental Group, 2023.

**\*Attachments:** ☐ NONE ☒ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record  
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record  
☐ Artifact Record ☐ Photograph Record ☐ Other (List):

## BUILDING, STRUCTURE, AND OBJECT RECORD

\*Resource Name or # (Assigned by recorder) 140-190 Industrial Way \*NRHP Status Code 6Z

Page 2 of 6

B1. Historic Name: none  
B2. Common Name: none  
B3. Original Use: Light industrial B4. Present Use: Light industrial  
\*B5. Architectural Style: Utilitarian  
\*B6. Construction History: (Construction date, alterations, and date of alterations)  
1971 original construction

\*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date: n/a Original Location: n/a

\*B8. Related Features:  
none

B9a. Architect: unknown b. Builder: unknown  
\*B10. Significance: Theme Mid-Century Industrial Development Area Brisbane  
Period of Significance 1970s Property Type Industrial Applicable Criteria n/a  
(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

### Brisbane

As the subject property is in the City of Brisbane, which was not incorporated until the 1960s, the early history of the area is best understood from the history of San Mateo County. "San Mateo County was formed in April 1856 out of the southern portion of then-San Francisco County with a population of fewer than 5,000 people and little in the way of commerce." (County of San Mateo, 2023) It is 455 square miles and is bordered by the Pacific Ocean to the west, San Francisco Bay and Santa Clara County to the east, the City and County of San Francisco to the north, and Santa Cruz County to the south (County of San Mateo, 2023). A large portion of the County is occupied by open space including state parks while population centers are primarily located along the two coast lines. Brisbane is located at the northern end of the county between South San Francisco and Millbrae.

[Continued on page 3]

B11. Additional Resource Attributes: (List attributes and codes) none

\*B12. References:

B13. Remarks:  
none

(Sketch Map with north arrow required.)  
See location map

\*B14. Evaluator: Amber Grady, ESA  
\*Date of Evaluation: February 8, 2023

(This space reserved for official comments.)



**\*B10. Significance:** (Continued from page 2)

*Southern Pacific Railroad*

The subject property is located on land that was previously owned and occupied by the Southern Pacific Railroad. The following context is an excerpt from Carey & Company's technical report that was prepared for the Visitation Valley Redevelopment EIR (Carey & Company, 2008):

The Southern Pacific Railroad Company was one of the most powerful and prominent of American railroad companies in the nineteenth and twentieth centuries. Founded in 1865 by San Francisco businessmen, with the aim to construct a line between San Francisco and San Diego, California, it eventually merged with Central Pacific Railroad Company in 1870, expanding the reach of the western portion of the First Transcontinental Railroad in the United States. In 1904, the Southern Pacific Railroad Company had begun filling in land along the Bay and building their tunnel, still extant, at the corner of Blanken and Tunnel avenues. Southern Pacific provided commuter service to Visitation Valley on the line called the "Bayshore cutoff," which was known as the most important access route for the San Francisco Peninsula.

The creation of the Cutoff was the product of massive efforts to fill, level and dig through the land in order to connect the burgeoning industrial factory district of the South Bay to the heart of the city and spur development. In 1907, the cutoff was complete, and gave Southern Pacific trains a quick and more direct route into San Francisco. Shop facilities for repairing cars and locomotives were constructed along the Cutoff. A freight yard was established adjacent to the tracks and was noted as the major classification yard for the San Francisco terminal. Freight traffic in the Bay Area experienced a gradual decline in the 1970s, and by 1979, the Bayshore freight yard began moving yard activity to South San Francisco. In 1996, Southern Pacific Railroad Company was succeeded by Union Pacific Railroad. The former lots belonging to Southern Pacific are currently under the ownership of Universal Paragon, and the buildings are now occupied by various commercial organizations.

*Bayshore Boulevard*

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. "According to the 1913-1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse" (Carey & Company, 2008). "The Schlage Lock Factory was well established by this time, with numerous industrial buildings lining the eastern side of Bayshore Boulevard" (Carey & Company, 2008).

*Incorporation*

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. San Francisco to the north Daly City to the northwest and South San Francisco to the south each could have annexed the area. Development pressure was also felt from the Southern Pacific railroad, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a subsidiary of the Crocker Estate Company (Oral History Associates, Inc., 1989). Prior to incorporation, the area was under the control of San Mateo County (Oral History Associates, Inc., 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased the threat of annexation as the development would need to be annexed into only one of the surrounding cities in order to provide services such as police, fire, water, and sewer (Oral History Associates, Inc., 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc., 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted to incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc., 1989). The subject property is north of the originally incorporated city.

*Crocker Land Company*

The Crocker Land Company, a subsidiary of Crocker Estate Company, owned properties throughout California (Oral History Associates, Inc., 1989). The owner of the Crocker Estate Company was Charles Crocker, founder of Crocker Bank (Oral History Associates, Inc., 1989). "Its San Mateo County holdings included all of the undeveloped property on San Bruno Mountain and a large tract of tidelands on the east side of the Bayshore Freeway" (Oral History Associates, Inc., 1989). "The town of Brisbane was almost completely surrounded by Crocker-owned property" (Oral History Associates, Inc., 1989).

*Industrial Development*

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both Southern Pacific Railroad and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History

\*Recorded by: Amber Grady, ESA

\*Date: February 2023

☒ Continuation

☐ Update

Associates, Inc., 1989). Some of Southern Pacific Railroad's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc., 1989). The subject property is located on what was, in the early 1960s, lands owned by Southern Pacific Railroad (Oral History Associates, Inc., 1989). Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the Southern Pacific ceased operations.

#### Subject Property

The following is an excerpt from Paleowest's Cultural Resource Technical Report (Paleowest, 2022):

The original bay shoreline is approximately followed by Bayshore Boulevard. Construction of the Southern Pacific Railroad began in the early 1900s, extending the railroad tracks across the Bay. Following the 1906 San Francisco earthquake, the area west of the rail corridor was filled in primarily with demolition rubble.

On the east side of the rail corridor was a municipal landfill site which was established in the 1930s. [The subject property is to the west of the rail corridor and was developed prior to 1946 (Historicaerials.com, 1946). Deposition began in the north and it progressed southward until expansion of the landfill was terminated in the 1960s at the edge of what is now Brisbane Lagoon. The construction of US Highway 101 in the mid-1950s established the easternmost boundary of the Bay fill. The Brisbane Landfill site is 364 acres and is bounded by the Union Pacific/Joint Powers Board railroad corridor (Caltrain tracks) to the west, US Highway 101 to the east, and Brisbane Lagoon to the south. After the landfill operation was discontinued in 1967, approximately 20 to 30 ft of soil was placed over the site. Since the 1940s, the landfill has been developed into an industrial area and the existing lumberyards and warehouse buildings having been built on the oldest part of the landfill.

The subject property is developed with a large, light industrial warehouse that allows for four separate tenants. It was constructed in 1971.

#### Occupants

2011 to 2021 - Sunset Garage (150-160 Industrial Way) (Google Streetview)

2013 to 2023 - Fire in the Ring Boxing (180 Industrial Way)

2017 to 2023 – Premier Stone & Tile (190 Industrial Way) (Google Streetview)

2022 to 2023 – Zak's Auto Collision Body Shop & Paint (150-160 Industrial Way) (Google Streetview)

#### Evaluation

ESA staff evaluated the subject property for its potential historical significance under California Register of Historical Resources Criteria 1 through 4. The subject property at 160 Industrial Way is a light industrial building that has housed several businesses since it was constructed. The subject property and surrounding land was originally occupied by the Southern Pacific Railroad. Industrial Way was subsequently redeveloped with a variety of light industrial buildings after Southern Pacific no longer occupied the site. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. 160 Industrial Way is a utilitarian building with no distinct architectural style. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property at 160 Industrial Way would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

#### Integrity

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. 160 Industrial Way does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

#### Summary

As the property at 160 Industrial Way does not meet any of the California Register criteria, ESA recommends it to be ineligible for listing in the California Register.

Page 5 of 6

\*Resource Name or # 140-190 Industrial Way

\*Recorded by: Amber Grady, ESA

\*Date: February 2023

☒ Continuation

☐ Update

**\*B12. References:** (Continued from page 2)

Carey & Company. *Visitacion Valley Redevelopment EIR Historic Resources Technical Report*. April 29, 2008.

County of San Mateo. *About San Mateo County*. Accessed February 15, 2023. <https://www.smcgov.org/about-san-mateo-county>

Directory, Japanese-affiliated Companies in USA & Canada. Japan, Japan External Trade Organization, 1991.

Oral History Associates, Inc. *Brisbane City of Stars: The First Twenty-Five Years 1961-1986*. 1989

Paleowest. *Cultural Resource Technical Report for the Brisbane Data Gap Investigation Project in the City of Brisbane, San Mateo County, California*. April 13, 2022.

Public Works Manual. United States, Public Works Journal Corporation. 1993.



## LOCATION MAP

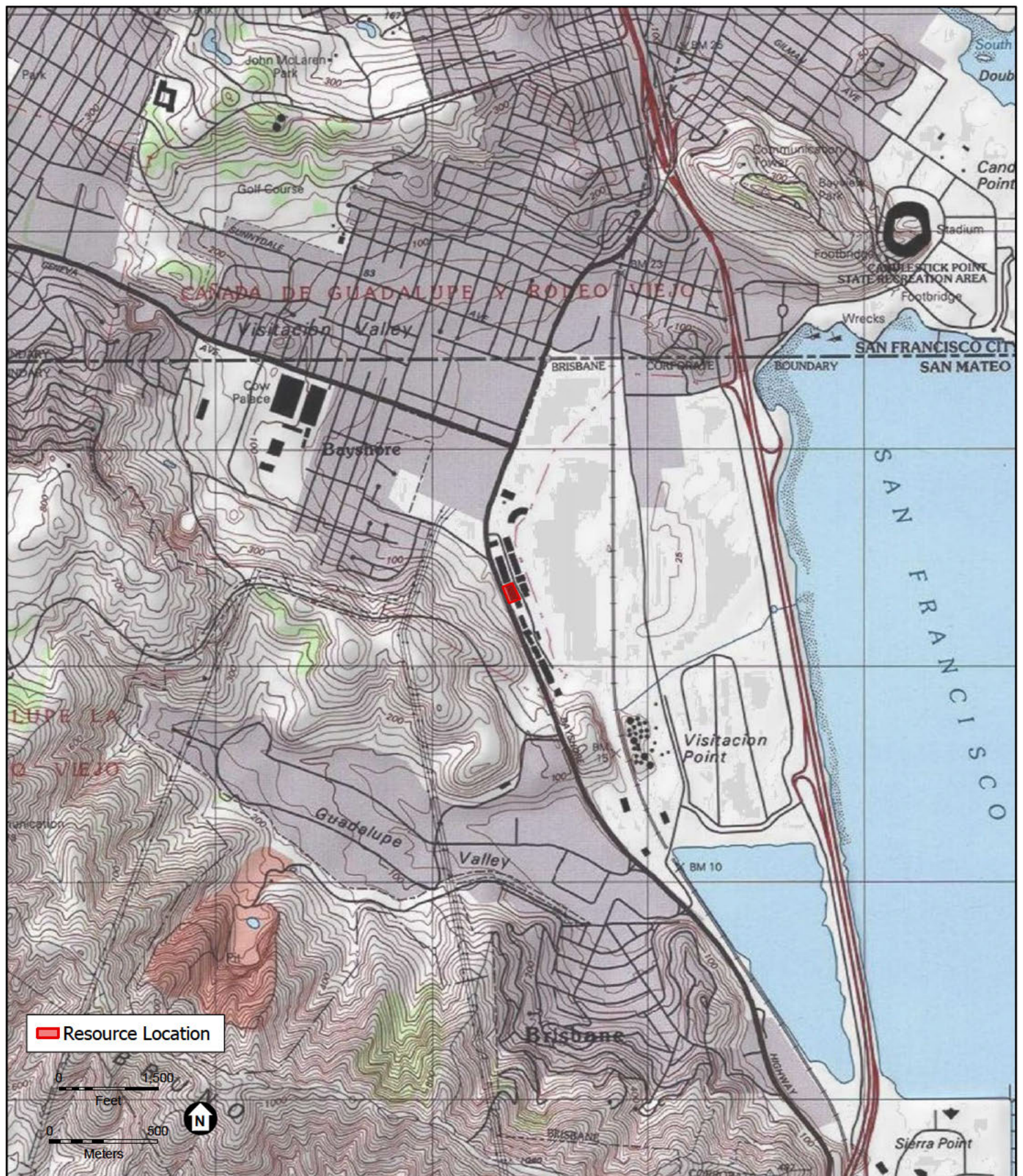
Page 6 of 6

\*Resource Name or Number: 140-190 Industrial

\*Map name: South San Francisco, CA

\*Scale: 1:24000

\*Date of Map: 1981





State of California — The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary  
HRI #  
Trinomial  
**NRHP Status Code**

Other Listings  
Review Code

Reviewer

Date

Page 1 of 6

**\*Resource Name or #:** 151-171 Industrial Way

**P1. Other Identifier:** 161 Industrial Way, 171 Industrial Way

**\*P2. Location:** ☐ Not for Publication ☒ Unrestricted

**\*a. County** San Mateo

**\*b. USGS 7.5' Quad** San Francisco South

**Date** 1981 Within unsectioned portion of Cañada de Guadalupe land grant

c. Address 151 Industrial Way

City Brisbane

Zip 94005

d. UTM: Zone 10N, 552330 mE/ 4172553 mN; NAD 83

e. Other Locational Data: APN 005-312-110

**\*P3a. Description:**

The subject property is a 0.771-acre lot that includes a large warehouse building. The building has an irregular footprint, a concrete slab foundation, and a low pitched, side gabled roof. The walls are clad in standing seam metal sheeting. The areas that surround the building to the north, west, and south are paved with the area between the building and Industrial Way to the east stripped for parking. The area to the east is a combination of paved area and vegetation.

The primary (west) façade includes the large metal roll-up doors, four pedestrian doors and six aluminum sliders windows. The north facade does not appear to have any window or door openings. The east (rear) façade is not visible from the right-of-way.

**\*P3b. Resource Attributes:** HP6. 1-3 story commercial building

**\*P4. Resources Present:** ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo: facing northeast

**\*P6. Date Constructed/Age and Source:**

1971 (assessor data)

☒ Historic ☐ Prehistoric ☐ Both

**\*P7. Owner and Address:**

Sierra Hotel Management Corp  
150 Executive Park Blvd Ste 42  
San Francisco CA 94134

**P8. Recorded by:**

Becky Urbano and Kathy Cleveland, ESA  
2600 Capitol Avenue, #200  
Sacramento, CA 95816

**\*P9. Date Recorded:**

February 7, 2023

**\*P10. Survey Type:**

Intensive

**\*P11. Report Citation:** Sims, Cleveland, and Grady, *Baylands Specific Plan, Cultural Resources Technical Report*, Prepared by Environmental Science Associates, Prepared for Metis Environmental Group, 2023.

**\*Attachments:** ☐ NONE ☒ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record  
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record  
☐ Artifact Record ☐ Photograph Record ☐ Other (List):

## BUILDING, STRUCTURE, AND OBJECT RECORD

\*Resource Name or # (Assigned by recorder) 151-171 Industrial Way \*NRHP Status Code 6Z

Page 2 of 6

B1. Historic Name: none  
B2. Common Name: none  
B3. Original Use: Light industrial B4. Present Use: Light industrial  
\*B5. Architectural Style: Utilitarian  
\*B6. Construction History: (Construction date, alterations, and date of alterations)  
1971 original construction

\*B7. Moved? ☒No ☐Yes ☐Unknown Date: n/a Original Location: n/a

\*B8. Related Features:  
none

B9a. Architect: unknown b. Builder: unknown

\*B10. Significance: Theme Mid-Century Industrial Development Area Brisbane  
Period of Significance 1970s Property Type Industrial Applicable Criteria n/a  
(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

### Brisbane

As the subject property is in the City of Brisbane, which was not incorporated until the 1960s, the early history of the area is best understood from the history of San Mateo County. "San Mateo County was formed in April 1856 out of the southern portion of then-San Francisco County with a population of fewer than 5,000 people and little in the way of commerce." (County of San Mateo, 2023) It is 455 square miles and is bordered by the Pacific Ocean to the west, San Francisco Bay and Santa Clara County to the east, the City and County of San Francisco to the north, and Santa Cruz County to the south (County of San Mateo, 2023). A large portion of the County is occupied by open space including state parks while population centers are primarily located along the two coast lines. Brisbane is located at the northern end of the county between South San Francisco and Millbrae.

[Continued on page 3]

B11. Additional Resource Attributes: (List attributes and codes) none

\*B12. References:

B13. Remarks:  
none

(Sketch Map with north arrow required.)  
See location map

\*B14. Evaluator: Amber Grady, ESA  
\*Date of Evaluation: February 8, 2023

(This space reserved for official comments.)

**\*B10. Significance:** (Continued from page 2)

*Southern Pacific Railroad*

The subject property is located on land that was previously owned and occupied by the Southern Pacific Railroad. The following context is an excerpt from Carey & Company's technical report that was prepared for the Visitation Valley Redevelopment EIR (Carey & Company, 2008):

The Southern Pacific Railroad Company was one of the most powerful and prominent of American railroad companies in the nineteenth and twentieth centuries. Founded in 1865 by San Francisco businessmen, with the aim to construct a line between San Francisco and San Diego, California, it eventually merged with Central Pacific Railroad Company in 1870, expanding the reach of the western portion of the First Transcontinental Railroad in the United States. In 1904, the Southern Pacific Railroad Company had begun filling in land along the Bay and building their tunnel, still extant, at the corner of Blanken and Tunnel avenues. Southern Pacific provided commuter service to Visitation Valley on the line called the "Bayshore cutoff," which was known as the most important access route for the San Francisco Peninsula.

The creation of the Cutoff was the product of massive efforts to fill, level and dig through the land in order to connect the burgeoning industrial factory district of the South Bay to the heart of the city and spur development. In 1907, the cutoff was complete, and gave Southern Pacific trains a quick and more direct route into San Francisco. Shop facilities for repairing cars and locomotives were constructed along the Cutoff. A freight yard was established adjacent to the tracks and was noted as the major classification yard for the San Francisco terminal. Freight traffic in the Bay Area experienced a gradual decline in the 1970s, and by 1979, the Bayshore freight yard began moving yard activity to South San Francisco. In 1996, Southern Pacific Railroad Company was succeeded by Union Pacific Railroad. The former lots belonging to Southern Pacific are currently under the ownership of Universal Paragon, and the buildings are now occupied by various commercial organizations.

*Bayshore Boulevard*

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. "According to the 1913-1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse" (Carey & Company, 2008). "The Schlage Lock Factory was well established by this time, with numerous industrial buildings lining the eastern side of Bayshore Boulevard" (Carey & Company, 2008).

*Incorporation*

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. San Francisco to the north Daly City to the northwest and South San Francisco to the south each could have annexed the area. Development pressure was also felt from the Southern Pacific railroad, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a subsidiary of the Crocker Estate Company (Oral History Associates, Inc., 1989). Prior to incorporation, the area was under the control of San Mateo County (Oral History Associates, Inc., 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased the threat of annexation as the development would need to be annexed into only one of the surrounding cities in order to provide services such as police, fire, water, and sewer (Oral History Associates, Inc., 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc., 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted to incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc., 1989). The subject property is north of the originally incorporated city.

*Crocker Land Company*

The Crocker Land Company, a subsidiary of Crocker Estate Company, owned properties throughout California (Oral History Associates, Inc., 1989). The owner of the Crocker Estate Company was Charles Crocker, founder of Crocker Bank (Oral History Associates, Inc., 1989). "Its San Mateo County holdings included all of the undeveloped property on San Bruno Mountain and a large tract of tidelands on the east side of the Bayshore Freeway" (Oral History Associates, Inc., 1989). "The town of Brisbane was almost completely surrounded by Crocker-owned property" (Oral History Associates, Inc., 1989).

*Industrial Development*

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both Southern Pacific Railroad and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History

\*Recorded by: Amber Grady, ESA

\*Date: February 2023

☒ Continuation

☐ Update

Associates, Inc., 1989). Some of Southern Pacific Railroad's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc., 1989). The subject property is located on what was, in the early 1960s, lands owned by Southern Pacific Railroad (Oral History Associates, Inc., 1989). Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the Southern Pacific ceased operations.

*Subject Property*

The following is an excerpt from Paleowest's Cultural Resource Technical Report (Paleowest, 2022):

The original bay shoreline is approximately followed by Bayshore Boulevard. Construction of the Southern Pacific Railroad began in the early 1900s, extending the railroad tracks across the Bay. Following the 1906 San Francisco earthquake, the area west of the rail corridor was filled in primarily with demolition rubble.

On the east side of the rail corridor was a municipal landfill site which was established in the 1930s. [The subject property is to the west of the rail corridor and was developed prior to 1946 (Historicaerials.com, 1946). Deposition began in the north and it progressed southward until expansion of the landfill was terminated in the 1960s at the edge of what is now Brisbane Lagoon. The construction of US Highway 101 in the mid-1950s established the easternmost boundary of the Bay fill. The Brisbane Landfill site is 364 acres and is bounded by the Union Pacific/Joint Powers Board railroad corridor (Caltrain tracks) to the west, US Highway 101 to the east, and Brisbane Lagoon to the south. After the landfill operation was discontinued in 1967, approximately 20 to 30 ft of soil was placed over the site. Since the 1940s, the landfill has been developed into an industrial area and the existing lumberyards and warehouse buildings having been built on the oldest part of the landfill.

The subject property is developed with a large, light industrial warehouse that is divided to accommodate three separate businesses. It was constructed in 1971.

*Occupants*

1981-1998 – Advance Carbon Products, Inc (171 Industrial Way) (Cooper/Lang Communications, 1981) (U.S. Industrial Directory, 1997) (Omnigraphics, 1998)

1983-1988 – Nomura & Company (beer wholesaler) (151 Industrial) (Beverage Marketing Corporation, 1988) (Thomas Publishing Company, 1983)

2005 – Try-R-Steel Fabrication Inc. (151 Industrial Way) (Times Mirror Press, 2005.)

2011 to 2019 - Allied Ropes Co. (171 Industrial Way) (Google Streetview)

2011 to 2023 – R&H Floor Equipment Repairs (161 Industrial Way) (Google Streetview)

2011 to 2023 – Florian Industries: Crafted Architectural Metal Products (151 Industrial Way) (Google Streetview)

Evaluation

ESA staff evaluated the subject property for its potential historical significance under California Register of Historical Resources Criteria 1 through 4. The subject property at 151 Industrial Way is a light industrial building that has housed several businesses since it was constructed. The subject property and surrounding land was originally occupied by the Southern Pacific Railroad. Industrial Way was subsequently redeveloped with a variety of light industrial buildings after Southern Pacific no longer occupied the site. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. 151 Industrial Way is a utilitarian building with no distinct architectural style. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property at 151 Industrial Way would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

Integrity



Page 5 of 6

\*Resource Name or # 151-171 Industrial Way

\*Recorded by: Amber Grady, ESA

\*Date: February 2023

☒ Continuation

☐ Update

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. 151 Industrial Way does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

Summary

As the property at 151 Industrial Way does not meet any of the California Register criteria, ESA recommends it to be ineligible for listing in the California Register.

**\*B12. References:** (Continued from page 2)

African/American Directory. United States, Cooper/Lang Communications, 1981.

California Manufacturers Register. United States, Times Mirror Press, 2005.

Carey & Company. *Visitation Valley Redevelopment EIR Historic Resources Technical Report*. April 29, 2008.

County of San Mateo. *About San Mateo County*. Accessed February 15, 2023. <https://www.smcgov.org/about-san-mateo-county>

National Beverage Marketing Directory. United States, Beverage Marketing Corporation, 1988.

Oral History Associates, Inc. *Brisbane City of Stars: The First Twenty-Five Years 1961-1986*. 1989

Paleowest. *Cultural Resource Technical Report for the Brisbane Data Gap Investigation Project in the City of Brisbane, San Mateo County, California*. April 13, 2022.

Public Works Manual. United States, Public Works Journal Corporation. 1993.

Toll-free Phone Book USA: A Directory of Toll-free Telephone Numbers for Businesses and Organizations Nationwide. United States, Omnigraphics, 1998.

Thomas Grocery Register. United States, Thomas Publishing Company, 1983.

U.S. Industrial Directory. United States, Cahnners, 1997.



## LOCATION MAP

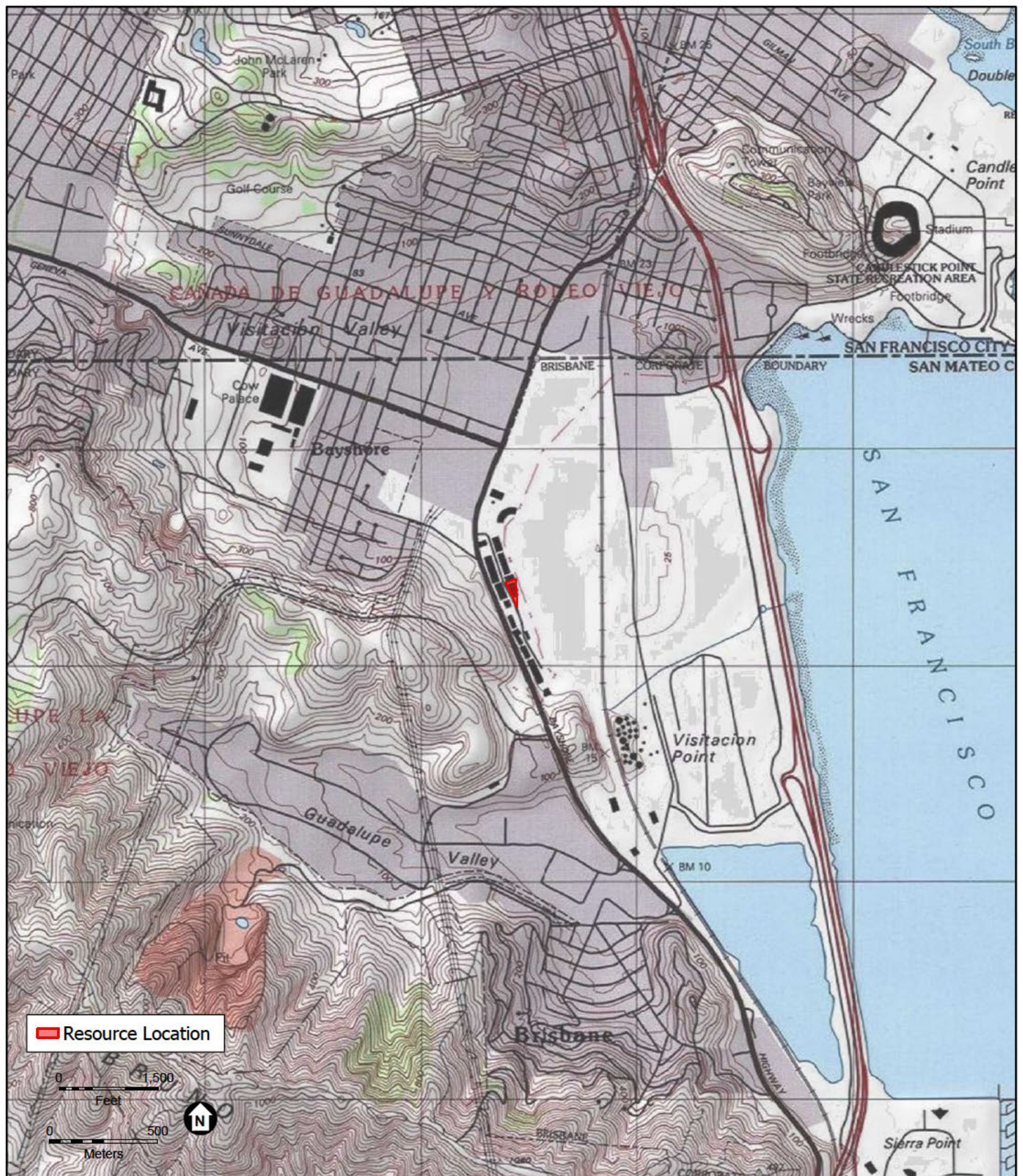
Page 6 of 6

\*Resource Name or Number: 151-171 Industrial

\*Map name: South San Francisco, CA

\*Scale: 1:24000

\*Date of Map: 1981





State of California — The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary  
HRI #  
Trinomial  
**NRHP Status Code**

Other Listings  
Review Code

Reviewer

Date

Page 1 of 6

\*Resource Name or #: 200 Industrial Way

**P1. Other Identifier:**

\*P2. Location: ☐ Not for Publication ☒ Unrestricted

\*a. County San Mateo

\*b. USGS 7.5' Quad San Francisco South Date 1981 Within unsectioned portion of Cañada de Guadalupe land grant  
City Brisbane Zip 94005

c. Address 200 Industrial Way

d. UTM: Zone 10N, 552303 mE/ 4172480 mN; NAD 83

e. Other Locational Data: APN 005-311-020

**\*P3a. Description:**

The subject property is a 0.619-acre lot that includes a large warehouse building. The building is of concrete tilt-up construction with a rectangular footprint. The building has a bow truss roof behind a parapet wall. The areas surrounding the building to the north, west, and south are paved. There is a landscape strip between the building and the sidewalk nearly the full length of the east façade.

The primary (east) façade include five bays that are divided by pilasters. The southern-most bay includes 12 openings in four rows, the top and bottoms rows included colored solid panels with a row of fixed windows in the middle row. The next bay to the north includes 11 openings, the top four and bottom three include colored solid panels with a glazed door and three fixed windows. From south to north the remaining three bays include a solid pedestrian door, a large metal roll-up door, and a blank wall.

The north façade includes four bays that are divided by pilasters; one large opening is in the second bay from the northeast corner. The south façade includes four bays divided by pilasters; the westernmost bay includes a large opening, the second bay from the southwest corner is blank, and the two easternmost bays each include 12 openings in four rows, the top and bottoms rows included colored solid panels with a row of fixed windows in the middle row. Awnings are present over the windows on the south façade. The west façade is not visible from the right-of-way.

\*P3b. Resource Attributes: HP6. 1-3 story commercial building

\*P4. Resources Present: ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo: facing northwest

\*P6. Date Constructed/Age and Source:  
1965 (assessor data)

☒ Historic ☐ Prehistoric ☐ Both

**\*P7. Owner and Address:**

Sierra Hotel Management Corp  
150 Executive Park Blvd Ste 42  
San Francisco CA 94134

**P8. Recorded by:**

Becky Urbano and Kathy Cleveland, ESA  
2600 Capitol Avenue, #200  
Sacramento, CA 95816

**\*P9. Date Recorded:**

February 7, 2023

**\*P10. Survey Type:**

Intensive

\*P11. Report Citation: Sims, Cleveland, and Grady, *Baylands Specific Plan, Cultural Resources Technical Report*, Prepared by Environmental Science Associates, Prepared for Metis Environmental Group, 2023.

\*Attachments: ☐ NONE ☒ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record  
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record  
☐ Artifact Record ☐ Photograph Record ☐ Other (List):

## BUILDING, STRUCTURE, AND OBJECT RECORD

\*Resource Name or # (Assigned by recorder) 200 Industrial Way \*NRHP Status Code 6Z

Page 2 of 6

B1. Historic Name: none

B2. Common Name: none

B3. Original Use: unknown B4. Present Use: Retail Sales

\*B5. Architectural Style:

\*B6. Construction History: (Construction date, alterations, and date of alterations)

1965 original construction

\*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date: n/a Original Location: n/a

\*B8. Related Features:

none

B9a. Architect: unknown b. Builder:

\*B10. Significance: Theme Mid-Century Industrial Development Area Brisbane

Period of Significance 1960s Property Type Industrial Applicable Criteria n/a

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

### Brisbane

As the subject property is in the City of Brisbane, which was not incorporated until the 1960s, the early history of the area is best understood from the history of San Mateo County. "San Mateo County was formed in April 1856 out of the southern portion of then-San Francisco County with a population of fewer than 5,000 people and little in the way of commerce." (County of San Mateo, 2023) It is 455 square miles and is bordered by the Pacific Ocean to the west, San Francisco Bay and Santa Clara County to the east, the City and County of San Francisco to the north, and Santa Cruz County to the south (County of San Mateo, 2023). A large portion of the County is occupied by open space including state parks while population centers are primarily located along the two coast lines. Brisbane is located at the northern end of the county between South San Francisco and Millbrae.

[Continued on page 3]

B11. Additional Resource Attributes: (List attributes and codes) none

\*B12. References:

B13. Remarks:

none

(Sketch Map with north arrow required.)  
See location map

\*B14. Evaluator: Amber Grady, ESA

\*Date of Evaluation: February 22, 2023

(This space reserved for official comments.)



**\*B10. Significance:** (Continued from page 2)

*Southern Pacific Railroad*

The subject property is located on land that was previously owned and occupied by the Southern Pacific Railroad. The following context is an excerpt from Carey & Company's technical report that was prepared for the Visitation Valley Redevelopment EIR (Carey & Company, 2008):

The Southern Pacific Railroad Company was one of the most powerful and prominent of American railroad companies in the nineteenth and twentieth centuries. Founded in 1865 by San Francisco businessmen, with the aim to construct a line between San Francisco and San Diego, California, it eventually merged with Central Pacific Railroad Company in 1870, expanding the reach of the western portion of the First Transcontinental Railroad in the United States. In 1904, the Southern Pacific Railroad Company had begun filling in land along the Bay and building their tunnel, still extant, at the corner of Blanken and Tunnel avenues. Southern Pacific provided commuter service to Visitation Valley on the line called the "Bayshore cutoff," which was known as the most important access route for the San Francisco Peninsula.

The creation of the Cutoff was the product of massive efforts to fill, level and dig through the land in order to connect the burgeoning industrial factory district of the South Bay to the heart of the city and spur development. In 1907, the cutoff was complete, and gave Southern Pacific trains a quick and more direct route into San Francisco. Shop facilities for repairing cars and locomotives were constructed along the Cutoff. A freight yard was established adjacent to the tracks and was noted as the major classification yard for the San Francisco terminal. Freight traffic in the Bay Area experienced a gradual decline in the 1970s, and by 1979, the Bayshore freight yard began moving yard activity to South San Francisco. In 1996, Southern Pacific Railroad Company was succeeded by Union Pacific Railroad. The former lots belonging to Southern Pacific are currently under the ownership of Universal Paragon, and the buildings are now occupied by various commercial organizations.

*Bayshore Boulevard*

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. "According to the 1913-1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse" (Carey & Company, 2008). "The Schlage Lock Factory was well established by this time, with numerous industrial buildings lining the eastern side of Bayshore Boulevard" (Carey & Company, 2008).

*Incorporation*

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. San Francisco to the north Daly City to the northwest and South San Francisco to the south each could have annexed the area. Development pressure was also felt from the Southern Pacific railroad, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a subsidiary of the Crocker Estate Company (Oral History Associates, Inc., 1989). Prior to incorporation, the area was under the control of San Mateo County (Oral History Associates, Inc., 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased the threat of annexation as the development would need to be annexed into only one of the surrounding cities in order to provide services such as police, fire, water, and sewer (Oral History Associates, Inc., 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc., 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted to incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc., 1989). The subject property is north of the originally incorporated city.

*Crocker Land Company*

The Crocker Land Company, a subsidiary of Crocker Estate Company, owned properties throughout California (Oral History Associates, Inc., 1989). The owner of the Crocker Estate Company was Charles Crocker, founder of Crocker Bank (Oral History Associates, Inc., 1989). "Its San Mateo County holdings included all of the undeveloped property on San Bruno Mountain and a large tract of tidelands on the east side of the Bayshore Freeway" (Oral History Associates, Inc., 1989). "The town of Brisbane was almost completely surrounded by Crocker-owned property" (Oral History Associates, Inc., 1989).

*Industrial Development*

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both Southern Pacific Railroad and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History

\*Recorded by: Amber Grady, ESA

\*Date: February 2023

☒ Continuation

☐ Update

Associates, Inc., 1989). Some of Southern Pacific Railroad's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc., 1989). The subject property is located on what was, in the early 1960s, lands owned by Southern Pacific Railroad (Oral History Associates, Inc., 1989). Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the Southern Pacific ceased operations.

Subject Property

The following is an excerpt from Paleowest's Cultural Resource Technical Report (Paleowest, 2022):

The original bay shoreline is approximately followed by Bayshore Boulevard. Construction of the Southern Pacific Railroad began in the early 1900s, extending the railroad tracks across the Bay. Following the 1906 San Francisco earthquake, the area west of the rail corridor was filled in primarily with demolition rubble.

On the east side of the rail corridor was a municipal landfill site which was established in the 1930s. [The subject property is to the west of the rail corridor and was developed prior to 1946 (Historicaerials.com, 1946). Deposition began in the north and it progressed southward until expansion of the landfill was terminated in the 1960s at the edge of what is now Brisbane Lagoon. The construction of US Highway 101 in the mid-1950s established the easternmost boundary of the Bay fill. The Brisbane Landfill site is 364 acres and is bounded by the Union Pacific/Joint Powers Board railroad corridor (Caltrain tracks) to the west, US Highway 101 to the east, and Brisbane Lagoon to the south. After the landfill operation was discontinued in 1967, approximately 20 to 30 ft of soil was placed over the site. Since the 1940s, the landfill has been developed into an industrial area and the existing lumberyards and warehouse buildings having been built on the oldest part of the landfill.

The subject property is developed with a large, light industrial warehouse that was constructed in 1965.

Occupants

2005 – D. Zelinsky & Sons Inc. (Times Mirror Press, 2005.)

2019 to 2023 – Florian Industries (Google Streetview)

Evaluation

ESA staff evaluated the subject property for its potential historical significance under California Register of Historical Resources Criteria 1 through 4. The subject property at 200 Industrial Way is a light industrial building that has housed several businesses since it was constructed. The subject property and surrounding land was originally occupied by the Southern Pacific Railroad. Industrial Way was subsequently redeveloped with a variety of light industrial buildings after Southern Pacific no longer occupied the site. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. 200 Industrial Way is a utilitarian building with no distinct architectural style. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property at 200 Industrial Way would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

Integrity

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. 200 Industrial Way does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

Summary

As the property at 200 Industrial Way does not meet any of the California Register criteria, ESA recommends it to be ineligible for listing in the California Register.

\*B12. **References:** (Continued from page 2)

Page 5 of 6

\*Resource Name or # 200 Industrial Way

\*Recorded by: Amber Grady, ESA

\*Date: February 2023

☒ Continuation

☐ Update

California Manufacturers Register. United States, Times Mirror Press, 2005.

Carey & Company. *Visitacion Valley Redevelopment EIR Historic Resources Technical Report*. April 29, 2008.

County of San Mateo. *About San Mateo County*. Accessed February 15, 2023. <https://www.smcgov.org/about-san-mateo-county>

Oral History Associates, Inc. *Brisbane City of Stars: The First Twenty-Five Years 1961-1986*. 1989

Paleowest. *Cultural Resource Technical Report for the Brisbane Data Gap Investigation Project in the City of Brisbane, San Mateo County, California*. April 13, 2022.

Public Works Manual. United States, Public Works Journal Corporation. 1993.



## LOCATION MAP

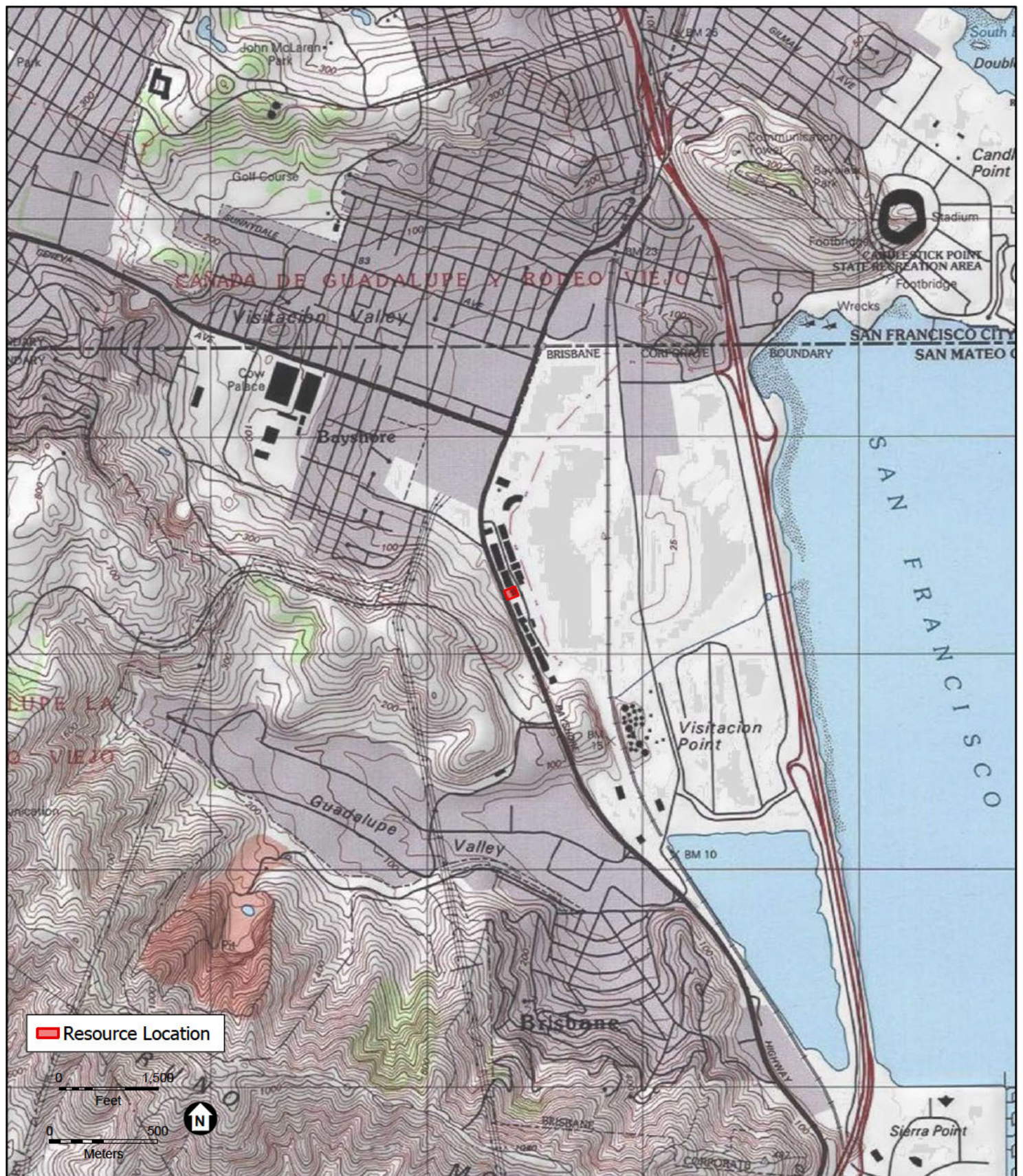
Page 6 of 6

\*Resource Name or Number: 200 Industrial

\*Map name: South San Francisco, CA

\*Scale: 1:24000

\*Date of Map: 1981





State of California — The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary  
HRI #  
Trinomial  
**NRHP Status Code**

Other Listings  
Review Code      Reviewer      Date

Page 1 of 6

**\*Resource Name or #:** 250-256 Industrial Way

**P1. Other Identifier:** 250 Industrial Way

**\*P2. Location:** ☐ Not for Publication ☒ Unrestricted

**\*a. County** San Mateo

**\*b. USGS 7.5' Quad** San Francisco South      **Date** 1981      Within unsectioned portion of Cañada de Guadalupe land grant

c. Address 256 Industrial Way      City Brisbane      Zip 94005

d. UTM: Zone 10N, 552320 mE/ 4172406 mN; NAD 83

e. Other Locational Data: APN 005-311-010

**\*P3a. Description:**

The subject property is a 1.227-acre lot that includes a large warehouse building. The building has a rectangular footprint and a side gambled roof. The walls are clad in corrugated metal sheets on approximately the top ¼ of the walls with stucco cladding below.

The primary (east) façade consists of 10 bays; with five of the bays including large, metal roll-up doors and the bay on the northernmost end of the façade has a glazed, aluminum framed door with a large fixed window on the left side and a fixed transom window above. The north façade includes seven aluminum framed windows. The south façade is partially blocked by the building to the south and the west façade is not visible from the right-of-way.

**\*P3b. Resource Attributes:** HP6. 1-3 story commercial building

**\*P4. Resources Present:** ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo: facing southwest

**\*P6. Date Constructed/Age and Source:**

1920 (assessor data)

☒ Historic ☐ Prehistoric ☐ Both

**\*P7. Owner and Address:**

Sierra Hotel Management Corp  
150 Executive Park Blvd Ste 42  
San Francisco CA 94134

**P8. Recorded by:**

Becky Urbano and Kathy Cleveland, ESA  
2600 Capitol Avenue, #200  
Sacramento, CA 95816

**\*P9. Date Recorded:**

February 7, 2023

**\*P10. Survey Type:**

Intensive

**\*P11. Report Citation:** Sims, Cleveland, and Grady, *Baylands Specific Plan, Cultural Resources Technical Report*, Prepared by Environmental Science Associates, Prepared for Metis Environmental Group, 2023.

**\*Attachments:** ☐ NONE ☒ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record

☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record

☐ Artifact Record ☐ Photograph Record ☐ Other (List):

## BUILDING, STRUCTURE, AND OBJECT RECORD

\*Resource Name or # (Assigned by recorder) 250-256 Industrial Way \*NRHP Status Code 6Z

Page 2 of 6

B1. Historic Name: none  
B2. Common Name: none  
B3. Original Use: Light industrial B4. Present Use: Light industrial  
\*B5. Architectural Style: \_\_\_\_\_  
\*B6. Construction History: (Construction date, alterations, and date of alterations)  
1920 original construction

\*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date: n/a Original Location: n/a

\*B8. Related Features:  
none

B9a. Architect: unknown b. Builder: unknown  
\*B10. Significance: Theme Early 20th Industrial Development Area Brisbane  
Period of Significance 1920s Property Type Industrial Applicable Criteria n/a  
(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

### Brisbane

As the subject property is in the City of Brisbane, which was not incorporated until the 1960s, the early history of the area is best understood from the history of San Mateo County. "San Mateo County was formed in April 1856 out of the southern portion of then-San Francisco County with a population of fewer than 5,000 people and little in the way of commerce." (County of San Mateo, 2023) It is 455 square miles and is bordered by the Pacific Ocean to the west, San Francisco Bay and Santa Clara County to the east, the City and County of San Francisco to the north, and Santa Cruz County to the south (County of San Mateo, 2023). A large portion of the County is occupied by open space including state parks while population centers are primarily located along the two coast lines. Brisbane is located at the northern end of the county between South San Francisco and Millbrae.

[Continued on page 3]

B11. Additional Resource Attributes: (List attributes and codes) none

\*B12. References:

B13. Remarks:  
none

(Sketch Map with north arrow required.)  
See location map

\*B14. Evaluator: Amber Grady, ESA  
\*Date of Evaluation: February 27, 2023

(This space reserved for official comments.)

**\*B10. Significance:** (Continued from page 2)

*Southern Pacific Railroad*

The subject property is located on land that was previously owned and occupied by the Southern Pacific Railroad. The following context is an excerpt from Carey & Company's technical report that was prepared for the Visitation Valley Redevelopment EIR (Carey & Company, 2008):

The Southern Pacific Railroad Company was one of the most powerful and prominent of American railroad companies in the nineteenth and twentieth centuries. Founded in 1865 by San Francisco businessmen, with the aim to construct a line between San Francisco and San Diego, California, it eventually merged with Central Pacific Railroad Company in 1870, expanding the reach of the western portion of the First Transcontinental Railroad in the United States. In 1904, the Southern Pacific Railroad Company had begun filling in land along the Bay and building their tunnel, still extant, at the corner of Blanken and Tunnel avenues. Southern Pacific provided commuter service to Visitation Valley on the line called the "Bayshore cutoff," which was known as the most important access route for the San Francisco Peninsula.

The creation of the Cutoff was the product of massive efforts to fill, level and dig through the land in order to connect the burgeoning industrial factory district of the South Bay to the heart of the city and spur development. In 1907, the cutoff was complete, and gave Southern Pacific trains a quick and more direct route into San Francisco. Shop facilities for repairing cars and locomotives were constructed along the Cutoff. A freight yard was established adjacent to the tracks and was noted as the major classification yard for the San Francisco terminal. Freight traffic in the Bay Area experienced a gradual decline in the 1970s, and by 1979, the Bayshore freight yard began moving yard activity to South San Francisco. In 1996, Southern Pacific Railroad Company was succeeded by Union Pacific Railroad. The former lots belonging to Southern Pacific are currently under the ownership of Universal Paragon, and the buildings are now occupied by various commercial organizations.

The following is an excerpt from the National Register nomination of the SPRR Roundhouse (Brandi, 2009):

During the next eight years (1901-1909), Harriman spent \$247 million on the Southern Pacific Railroad, twenty times more than was spent in the previous eight years. As part of a campaign to rehabilitate and modernize the Southern Pacific and other railroads under his control, Harriman built new sidings, expanded block signals, laid millions of tons of ballast, installed heavier rail, built stronger bridges, and bought real estate for new terminals. He also completed three, multimillion dollar projects: the Lucin Cutoff across the Great Salt Lake in Utah (opened 1904), the Montavlo cut-off south of Santa Barbara (opened 1904), and the Bayshore cut-off (opened 1907). [...]

The Bayshore Cut-off involved rerouting the line east from San Bruno and then north along the bay. This new route avoided the narrow right of way, sharp curves, steep grades and San Franciscans' bitter opposition to trains running through their backyards of the original route. The Bayshore route required filling the bay at several places, boring five tunnels, and building a long trestle, but it increased capacity and reduced operating costs.

As part of the project, the cove at Visitation Valley was filled in with tailings from the tunnels. More than 200 acres were reclaimed from the bay to make room for the railroad right of way and for the construction of a major marshalling yard with extensive maintenance facilities. This yard would supplant the cramped San Francisco machine shops at 16<sup>th</sup> Street and the car repair and roundhouse at Mariposa Street (Mission Bay). The new yard, called Bayshore, was 8,400 feet long and included a roundhouse, machine shop, car shops, and a hump for making up train consists, the second to be built on the Pacific coast.

Bayshore was one of several yards and roundhouses that were located along the route from San Francisco to Santa Barbara. [...] The Southern Pacific used the Bayshore yard for major railcar rehabilitation and locomotive maintenance from 1914 to 1983. [...]

The bulk of the construction took place from 1907 through 1920, although additions were made in the later years. The roundhouse was under construction from about 1908 and was completed by the end of 1910, making it the first, or one of the first, buildings built in the yard [...].

By 1920, the yard contained a freight car repair shop, passenger coach repair shop, machine shop, store, and two 150-ton track scales. Under construction was a tank and boiler shop. In 1921, Bayshore yard's repair and heavy locomotive repair shops were handling 42,000 cars a month on

64 outbound and inbound tracks and 21 repair tracks. By the mid[-nineteen] twenties the yard employed 1,196 employees.[...]

The Bayshore yard declined as San Francisco became less important as a freight terminal after WWII. The Western and Coast divisions were combined in 1964 with Oakland becoming the hub of operations in the Bay Area. Further consolidations occurred along the line during the 1960s with closing of Santa Barbara and San Luis Obispo yards. In the spring of 1979, Bayshore yard was phased out and facilities were dismantled over the next several years.

#### *Bayshore Boulevard*

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. "According to the 1913-1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse" (Carey & Company, 2008). "The Schlage Lock Factory was well established by this time, with numerous industrial buildings lining the eastern side of Bayshore Boulevard" (Carey & Company, 2008).

#### *Incorporation*

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. San Francisco to the north Daly City to the northwest and South San Francisco to the south each could have annexed the area. Development pressure was also felt from the Southern Pacific railroad, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a subsidiary of the Crocker Estate Company (Oral History Associates, Inc., 1989). Prior to incorporation, the area was under the control of San Mateo County (Oral History Associates, Inc., 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased the threat of annexation as the development would need to be annexed into only one of the surrounding cities in order to provide services such as police, fire, water, and sewer (Oral History Associates, Inc., 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc., 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted to incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc., 1989). The subject property is north of the originally incorporated city.

#### *Industrial Development*

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both Southern Pacific Railroad and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History Associates, Inc., 1989). Some of Southern Pacific Railroad's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc., 1989). The subject property is located on what was, in the early 1960s, lands owned by Southern Pacific Railroad (Oral History Associates, Inc., 1989). Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the Southern Pacific ceased operations.

#### *Subject Property*

The following is an excerpt from Paleowest's Cultural Resource Technical Report (Paleowest, 2022):

The original bay shoreline is approximately followed by Bayshore Boulevard. Construction of the Southern Pacific Railroad began in the early 1900s, extending the railroad tracks across the Bay. Following the 1906 San Francisco earthquake, the area west of the rail corridor was filled in primarily with demolition rubble.

On the east side of the rail corridor was a municipal landfill site which was established in the 1930s. [The subject property is to the west of the rail corridor and was developed prior to 1946 (Historicaerials.com, 1946). Deposition began in the north and it progressed southward until expansion of the landfill was terminated in the 1960s at the edge of what is now Brisbane Lagoon. The construction of US Highway 101 in the mid-1950s established the easternmost boundary of the Bay fill. The Brisbane Landfill site is 364 acres and is bounded by the Union Pacific/Joint Powers Board railroad corridor (Caltrain tracks) to the west, US Highway 101 to the east, and Brisbane Lagoon to the south. After the landfill operation was discontinued in 1967, approximately 20 to 30 ft of soil was placed over the site. Since the 1940s, the landfill has been developed into an industrial area and the existing lumberyards and warehouse buildings having been built on the oldest part of the landfill.

The subject property is developed with a large, light industrial warehouse that was constructed in 1920. It appears to be the only building along Industrial Way that pre-dates the construction of the road. Although it is not mentioned in the



\*Recorded by: Amber Grady, ESA

\*Date: February 2023

☒ Continuation

☐ Update

documentation of the other remaining SPRR buildings, historic aerials indicate that it was originally part of the complex of buildings and structures at the railyard. It appears to have been one of the last buildings constructed during the bulk of initial construction which occurred between 1907-1920. How the building was used during its time as part of the Bayshore yard is unknown.

**Occupants**

1920 – Southern Pacific Railroad

2009 – J and B Delivery Service, Inc. (interstate contract carrier) (U.S. Department of Transportation, 2009.)

**Evaluation**

ESA staff evaluated the subject property for its potential historical significance under California Register of Historical Resources Criteria 1 through 4. The subject property at 256 Industrial Way is a large industrial building, constructed in 1920, that was originally part of the Southern Pacific Railroad complex of buildings and has housed several businesses since Southern Pacific Railroad vacated the property. Industrial Way was subsequently developed with a variety of light industrial buildings and the context of the area has changed significantly. The building does not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. 256 Industrial Way is a utilitarian building with no distinct architectural style. It does not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property at 256 Industrial Way would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

**Integrity**

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. 256 Industrial Way does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

**Summary**

As the property at 256 Industrial Way does not meet any of the California Register criteria, ESA recommends it to be ineligible for listing in the California Register.

**\*B12. References:** (Continued from page 2)

Brandi, Richard. *Historic Nomination National Register of Historic Places: Southern Pacific Railroad's Bayshore Roundhouse, Brisbane, California*. 2009.

California Manufacturers Register. United States, Times Mirror Press, 2005.

Carey & Company. *Visitation Valley Redevelopment EIR Historic Resources Technical Report*. April 29, 2008.

County of San Mateo. *About San Mateo County*. Accessed February 15, 2023. <https://www.smcgov.org/about-san-mateo-county>

Oral History Associates, Inc. *Brisbane City of Stars: The First Twenty-Five Years 1961-1986*. 1989

Paleowest. *Cultural Resource Technical Report for the Brisbane Data Gap Investigation Project in the City of Brisbane, San Mateo County, California*. April 13, 2022.

Public Works Manual. United States, Public Works Journal Corporation. 1993.

U.S. Department of Transportation Federal Motor Carrier Safety Administration Register. United States, The Administration, 2009.



## LOCATION MAP

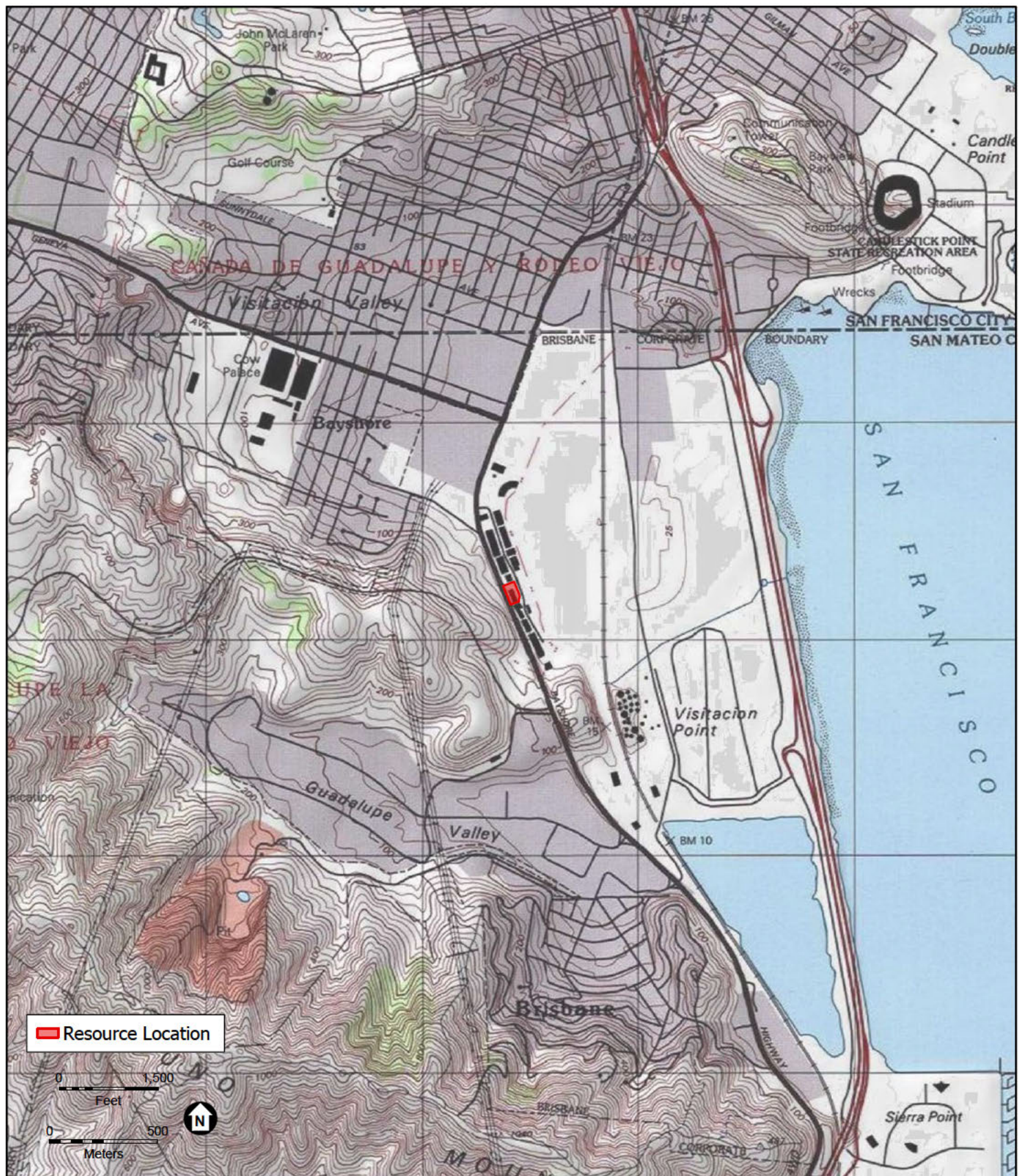
Page 6 of 6

\*Resource Name or Number: 250-256 Industrial

\*Map name: South San Francisco, CA

\*Scale: 1:24000

\*Date of Map: 1981





State of California — The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary  
HRI #  
Trinomial  
**NRHP Status Code**

Other Listings  
Review Code

Reviewer

Date

Page 1 of 6      \*Resource Name or #: 290-29   300-31   340-37   and   0 Industr I Way

**P1. Other Identifier:** 290 Industrial Way, 296 Industrial Way

**\*P2. Location:** ☐ Not for Publication ☒ Unrestricted

\*a. County San Mateo

\*b. USGS 7.5' Quad San Francisco South      Date 1981    Within unsectioned portion of Cañada de Guadalupe land grant

c. Address 294 Industrial Way      City Brisbane      Zip 94005

d. UTM: Zone 10N, 552408 mE/ 4172220 mN; NAD 83

e. Other Locational Data: APN 005-320-020

**\*P3a. Description:**

The subject property is a 5.662-acre lot that includes four buildings.

Building #1 is addressed as 290-296 Industrial Way and is an L-shaped with a low pitched side gabled roof. The walls are clad in standing seam metal sheets. The areas surrounding the building to the south and east are paved and the areas to the north and west include a variety of vegetation, but no purposeful landscaping. The northernmost portion of the building is the widest and is the "bottom" of the "L;" it includes two large roll-up doors, a small pedestrian door, and an aluminum framed window on the east facade. The southern portion of the primary (east) façade includes three large roll-up doors and three large openings, that likely previously included roll-up doors, that have been filled in with windows and pedestrian doors. The north and south facades do not appear to have any window or door openings. The west façade was not visible from the right-of-way.

(Continued on page 3)

**\*P3b. Resource Attributes:** HP6. 1-3 story commercial building

**\*P4. Resources Present:** ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo: facing northwest

**\*P6. Date Constructed/Age and Source:**

1971 (assessor data)

☒ Historic    ☐ Prehistoric    ☐ Both

**\*P7. Owner and Address:**

Sierra Hotel Management Corp  
150 Executive Park Blvd Ste 42  
San Francisco CA 94134

**P8. Recorded by:**

Becky Urbano and Kathy Cleveland, ESA  
2600 Capitol Avenue, #200  
Sacramento, CA 95816

**\*P9. Date Recorded:**

February 7, 2023

**\*P10. Survey Type:**

Intensive

**\*P11. Report Citation:** Sims, Cleveland, and Grady, *Baylands Specific Plan, Cultural Resources Technical Report*, Prepared by Environmental Science Associates, Prepared for Metis Environmental Group, 2023.

**\*Attachments:** ☐ NONE ☒ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record  
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record  
☐ Artifact Record ☐ Photograph Record ☐ Other (List):

## BUILDING, STRUCTURE, AND OBJECT RECORD

\*Resource Name or # (Assigned by recorder) 290-296, 300-312, 340-374, and 380 Industrial Way \*NRHP Status Code 6Z  
Page 2 of 6

B1. Historic Name: none  
B2. Common Name: none  
B3. Original Use: Light industrial B4. Present Use: Light industrial  
\*B5. Architectural Style: Utilitarian  
\*B6. Construction History: (Construction date, alterations, and date of alterations)  
1971 original construction

\*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date: n/a Original Location: n/a

\*B8. Related Features:  
none

B9a. Architect: unknown b. Builder: unknown  
\*B10. Significance: Theme Mid-Century Industrial Development Area Brisbane  
Period of Significance 1970s Property Type Industrial Applicable Criteria n/a  
(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

### Brisbane

As the subject property is in the City of Brisbane, which was not incorporated until the 1960s, the early history of the area is best understood from the history of San Mateo County. "San Mateo County was formed in April 1856 out of the southern portion of then-San Francisco County with a population of fewer than 5,000 people and little in the way of commerce." (County of San Mateo, 2023) It is 455 square miles and is bordered by the Pacific Ocean to the west, San Francisco Bay and Santa Clara County to the east, the City and County of San Francisco to the north, and Santa Cruz County to the south (County of San Mateo, 2023). A large portion of the County is occupied by open space including state parks while population centers are primarily located along the two coast lines. Brisbane is located at the northern end of the county between South San Francisco and Millbrae.

[Continued on page 3]

B11. Additional Resource Attributes: (List attributes and codes) none

\*B12. References:

B13. Remarks:  
none

(Sketch Map with north arrow required.)  
See location map

\*B14. Evaluator: Amber Grady, ESA  
\*Date of Evaluation: February 8, 2023

(This space reserved for official comments.)



**\*P3a. Description:** (Continued from page 1)

Building #2 is addressed as 300-312 Industrial Way, has a rectangular footprint, and a low-pitched side gabled roof. The walls are clad in standing seam metal sheets. The areas surrounding the building to the east, north, and south are paved and the area to the west includes a variety of vegetation, but no purposeful landscaping. The primary (east) façade includes six large metal roll-up doors, five solid pedestrian doors, and four slider windows. The north and south facades do not appear to have any window or door openings. The west façade was not visible from the right-of-way.

Building #3 is addressed as 340-374 Industrial Way, has an irregular footprint, and a low-pitched side gabled roof. The walls are clad in standing seam metal sheets. The areas surrounding the building to the east, north, and south are paved and the area to the west includes some paving and a variety of vegetation, but no purposeful landscaping. The primary (east) façade includes The north façade includes two tripart windows. The south facade do not appear to have any window or door openings. The west façade was not visible from the right-of-way.

Building #4 is addressed as 380 Industrial Way, has a square footprint, and a low pitched side gabled roof. The walls are clad in standing seam metal sheets. The areas surrounding the building are a mix of pavement and bare earth. The roof extends out over the north façade.

**\*B10. Significance:** (Continued from page 2)

*Southern Pacific Railroad*

The subject property is located on land that was previously owned and occupied by the Southern Pacific Railroad. The following context is an excerpt from Carey & Company's technical report that was prepared for the Visitation Valley Redevelopment EIR (Carey & Company, 2008):

The Southern Pacific Railroad Company was one of the most powerful and prominent of American railroad companies in the nineteenth and twentieth centuries. Founded in 1865 by San Francisco businessmen, with the aim to construct a line between San Francisco and San Diego, California, it eventually merged with Central Pacific Railroad Company in 1870, expanding the reach of the western portion of the First Transcontinental Railroad in the United States. In 1904, the Southern Pacific Railroad Company had begun filling in land along the Bay and building their tunnel, still extant, at the corner of Blanken and Tunnel avenues. Southern Pacific provided commuter service to Visitation Valley on the line called the "Bayshore cutoff," which was known as the most important access route for the San Francisco Peninsula.

The creation of the Cutoff was the product of massive efforts to fill, level and dig through the land in order to connect the burgeoning industrial factory district of the South Bay to the heart of the city and spur development. In 1907, the cutoff was complete, and gave Southern Pacific trains a quick and more direct route into San Francisco. Shop facilities for repairing cars and locomotives were constructed along the Cutoff. A freight yard was established adjacent to the tracks and was noted as the major classification yard for the San Francisco terminal. Freight traffic in the Bay Area experienced a gradual decline in the 1970s, and by 1979, the Bayshore freight yard began moving yard activity to South San Francisco. In 1996, Southern Pacific Railroad Company was succeeded by Union Pacific Railroad. The former lots belonging to Southern Pacific are currently under the ownership of Universal Paragon, and the buildings are now occupied by various commercial organizations.

*Bayshore Boulevard*

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. "According to the 1913-1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse" (Carey & Company, 2008). "The Schlage Lock Factory was well established by this time, with numerous industrial buildings lining the eastern side of Bayshore Boulevard" (Carey & Company, 2008).

*Incorporation*

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. San Francisco to the north Daly City to the northwest and South San Francisco to the south each could have annexed the area. Development pressure was also felt from the Southern Pacific railroad, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a subsidiary of the Crocker Estate Company (Oral History Associates, Inc., 1989). Prior to incorporation, the area was under the control of San Mateo County (Oral History Associates, Inc., 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased

\*Recorded by: Amber Grady, ESA

\*Date: February 2023

☒ Continuation

☐ Update

the threat of annexation as the development would need to be annexed into only one of the surrounding cities in order to provide services such as police, fire, water, and sewer (Oral History Associates, Inc., 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc., 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted to incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc., 1989). The subject property is north of the originally incorporated city.

#### *Crocker Land Company*

The Crocker Land Company, a subsidiary of Crocker Estate Company, owned properties throughout California (Oral History Associates, Inc., 1989). The owner of the Crocker Estate Company was Charles Crocker, founder of Crocker Bank (Oral History Associates, Inc., 1989). "Its San Mateo County holdings included all of the undeveloped property on San Bruno Mountain and a large tract of tidelands on the east side of the Bayshore Freeway" (Oral History Associates, Inc., 1989). "The town of Brisbane was almost completely surrounded by Crocker-owned property" (Oral History Associates, Inc., 1989).

#### *Industrial Development*

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both Southern Pacific Railroad and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History Associates, Inc., 1989). Some of Southern Pacific Railroad's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc., 1989). The subject property is located on what was, in the early 1960s, lands owned by Southern Pacific Railroad (Oral History Associates, Inc., 1989). Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the Southern Pacific ceased operations.

#### *Subject Property*

The following is an excerpt from Paleowest's Cultural Resource Technical Report (Paleowest, 2022):

The original bay shoreline is approximately followed by Bayshore Boulevard. Construction of the Southern Pacific Railroad began in the early 1900s, extending the railroad tracks across the Bay. Following the 1906 San Francisco earthquake, the area west of the rail corridor was filled in primarily with demolition rubble.

On the east side of the rail corridor was a municipal landfill site which was established in the 1930s. [The subject property is to the west of the rail corridor and was developed prior to 1946 (Historicaerials.com, 1946). Deposition began in the north and it progressed southward until expansion of the landfill was terminated in the 1960s at the edge of what is now Brisbane Lagoon. The construction of US Highway 101 in the mid-1950s established the easternmost boundary of the Bay fill. The Brisbane Landfill site is 364 acres and is bounded by the Union Pacific/Joint Powers Board railroad corridor (Caltrain tracks) to the west, US Highway 101 to the east, and Brisbane Lagoon to the south. After the landfill operation was discontinued in 1967, approximately 20 to 30 ft of soil was placed over the site. Since the 1940s, the landfill has been developed into an industrial area and the existing lumberyards and warehouse buildings having been built on the oldest part of the landfill.

The subject property is developed with four large, light industrial warehouses that were constructed in 1971.

#### *Occupants*

2019 – SF Paratransit (Building #1, 290 Industrial Way)

2023 – L&D Truck & Equipment Repair (Building #3, 374 Industrial Way), Super Tech Body Shop (Building #3, 370 Industrial Way), Shambhala Landscapes (Building #3, 366 Industrial Way), Virage Racing (Building #3, 340 Industrial Way), Atlantis Towing (Building #2, 312 Industrial Way), Golden West Specialty Foods (Building #2, 300 Industrial Way), Laprom Moving (Building #1, 294 Industrial Way).

#### Evaluation

ESA staff evaluated the subject property for its potential historical significance under California Register of Historical Resources Criteria 1 through 4. The subject property includes four light industrial buildings that have housed several businesses since they were constructed. The subject property and surrounding land was originally occupied by the Southern Pacific Railroad. Industrial Way was subsequently redeveloped with a variety of light industrial buildings after Southern Pacific

Page 5 of 6

\*Resource Name or # 290–296, 300–312, 340–374, and 380 Industrial Way

\*Recorded by: Amber Grady, ESA

\*Date: February 2023

☒ Continuation

☐ Update

no longer occupied the site. The buildings do not appear to have contributed to broad patterns of local or regional history or the cultural heritage of California, and for this reason is not recommended eligible for listing under Criterion 1. No one associated with the subject property was found to have been important to local, California, or national history. Research did not reveal associations with any significant persons. It therefore is not recommended eligible for listing under Criterion 2. The buildings on the subject property are all utilitarian building with no distinct architectural style. They do not embody distinctive characteristics of a type, region, or method of construction, represent the work of a master, or possess high artistic value, and are not recommended eligible for listing under Criterion 3. Research did not reveal that the subject property would provide important information relevant to history or pre-history that was not already known. For these reasons, the subject property is not recommended eligible for listing under Criterion 4.

Integrity

For a property to be eligible for the California Register it must be eligible under at least one of the four criteria and retain integrity. The subject property does not appear to be eligible under any of the four significance criteria; therefore, an analysis of integrity is not required.

Summary

As the property does not meet any of the California Register criteria, ESA recommends it to be ineligible for listing in the California Register.

**\*B12. References:** (Continued from page 2)

California Manufacturers Register. United States, Times Mirror Press, 2005.

Carey & Company. *Visitacion Valley Redevelopment EIR Historic Resources Technical Report*. April 29, 2008.

County of San Mateo. *About San Mateo County*. Accessed February 15, 2023. <https://www.smcgov.org/about-san-mateo-county>

Google Streetview, February 2023, May 2022, March 2019

Oral History Associates, Inc. *Brisbane City of Stars: The First Twenty-Five Years 1961-1986*. 1989

Paleowest. *Cultural Resource Technical Report for the Brisbane Data Gap Investigation Project in the City of Brisbane, San Mateo County, California*. April 13, 2022.

Public Works Manual. United States, Public Works Journal Corporation. 1993.



## LOCATION MAP

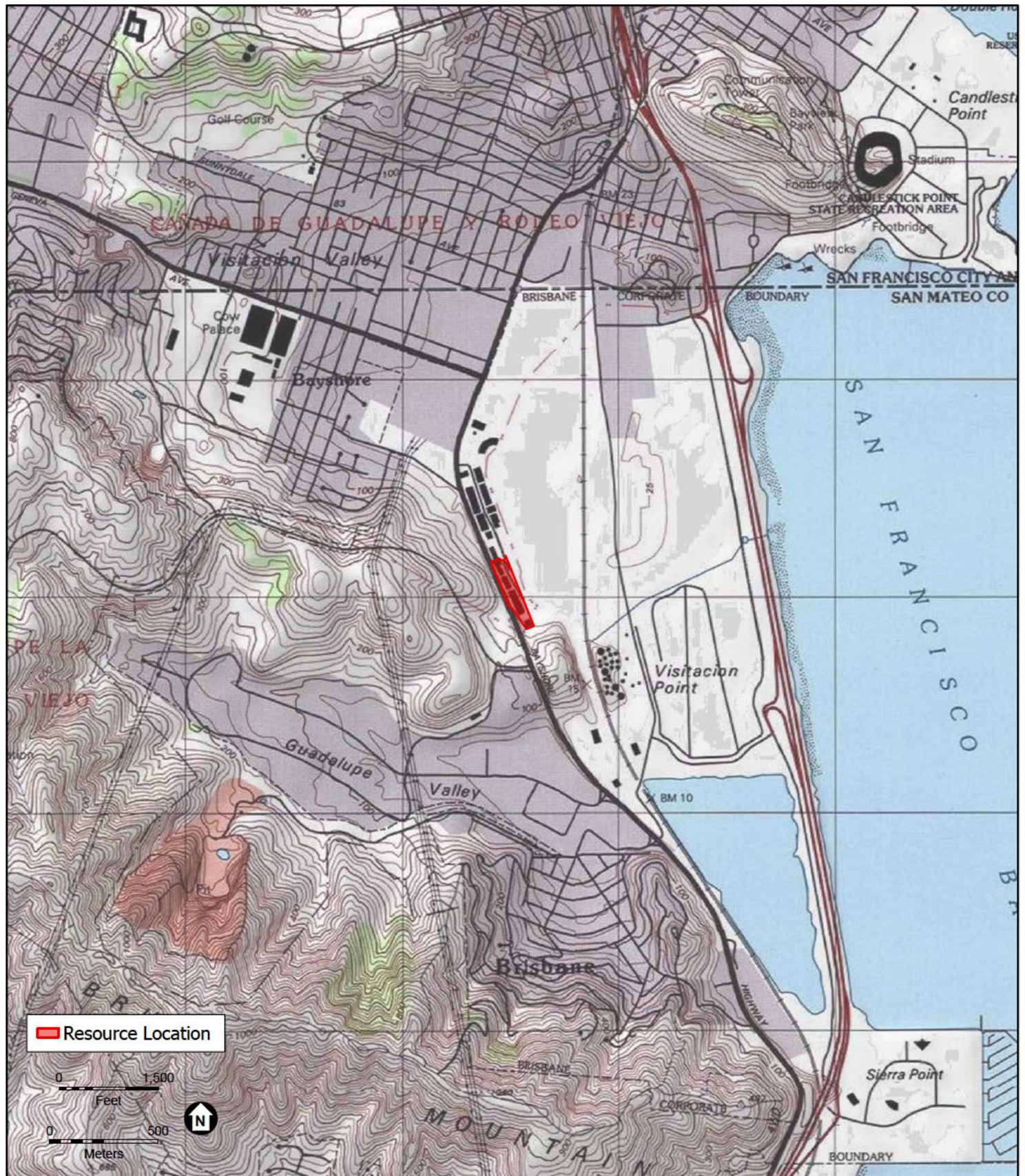
Page 6 of 6

\*Resource Name or Number: 290-296, 300-312, 340-374, and 380 Industrial

\*Map name: South San Francisco, CA

\*Scale: 1:24000

\*Date of Map: 1981





State of California — The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary [Abstract]  
HRI #  
Trinomial  
**NRHP Status Code**

Other Listings  
Review Code

Reviewer

Date

Page 1 of 7

**\*Resource Name or #:** 10 Industrial Way

**P1. Other Identifier:** none

**\*P2. Location:** ☐ Not for Publication ☒ Unrestricted

**\*a. County** San Mateo

**\*b. USGS 7.5' Quad** South San Francisco **Date** 1981 Within unsectioned portion of Cañada de Guadalupe land grant

**c. Address** 10 Industrial Way **City** Brisbane **Zip** 94005

**d. UTM: Zone** 10S, 552181.85 mE/ 4172759.45 mN; NAD 83

**e. Other Locational Data:** APN 005-311-090

**\*P3a. Description:** 10 Industrial Way is 8,400 square foot light industrial warehouse building located on a 0.528-acre lot in an industrial area of the City of Brisbane. It is of wood-frame construction and features a rectangular footprint measuring approximately 70 feet wide and 120 feet long. The building is clad primarily in standing seam metal panel and wood board siding and is capped with a low-pitched roof. The primary (east) façade features two wood-frame, unglazed wood doors situated beneath a wood slat overhang supported by brick posts. A black key box is affixed to the southernmost entrance. A roll-up, metal, garage door is located at the north end of the façade. The south façade features a flush metal door and three small, metal sash windows covered with wrought iron bars. The west façade is not visible from the public right-of-way. The north façade features a secondary entrance situated beneath a wood slat overhang supported by brick posts. It provides access to a concrete lot enclosed by a metal, chain-link fence and currently used as storage for a fleet of charter buses.

**\*P3b. Resource Attributes:** HP6. 1-3 story commercial building

**\*P4. Resources Present:** ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo: East façade of 10 Industrial Way, view facing west. ESA, 2024.

**\*P6. Date Constructed/Age and Source:**

☒ Historic ☐ Prehistoric

☐ Both

Ca. 1968-1980. Source: Historicaerials.com

**\*P7. Owner and Address:**

Ivana Panontin (Trust) and Dino & Marlene L. Belluomini (Trust)  
1766 Walnut Drive  
Mountain View, CA 994040-3659

**P8. Recorded by:**

Amy Langford, ESA  
550 Kearny Street  
San Francisco, CA 94108

**\*P9. Date Recorded:** June 13, 2024.

**\*P10. Survey Type:**

Intensive

**\*P11. Report Citation:** Sims, Cleveland, and Grady, *Baylands Specific Plan, Cultural Resources Technical Report*, prepared by Environmental Science Associates, prepared for Metis Environmental Group, 2023.

**\*Attachments:** ☐ NONE ☐ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record  
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record  
☐ Artifact Record ☐ Photograph Record ☐ Other (List):

## BUILDING, STRUCTURE, AND OBJECT RECORD

\*Resource Name or # 10 Industrial Way \*NRHP Status Code 6Z

Page 2 of 7

B1. Historic Name: none

B2. Common Name: none

B3. Original Use: Light Industrial

B4. Present Use: Light Industrial

\*B5. Architectural Style: Utilitarian

\*B6. Construction History: (Construction date, alterations, and date of alterations)

Constructed between 1968 and 1980. Seen Table 1 on page 4 for more information.

\*B7. Moved? ☐ No ☐ Yes ☐ Unknown Date:

Original Location:

\*B8. Related Features:

B9a. Architect: Unknown

b. Builder: Unknown

\*B10. Significance: Theme Mid-Century Industrial Development Area Brisbane

Period of Significance N/A

Property Type Industrial

Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

Brisbane

As the subject property is in the City of Brisbane, which was not incorporated until the 1960s, the early history of the area is best understood from the history of San Mateo County. "San Mateo County was formed in April 1856 out of the southern portion of then-San Francisco County with a population of fewer than 5,000 people and little in the way of commerce." (County of San Mateo, 2023) It is 455 square miles and is bordered by the Pacific Ocean to the west, San Francisco Bay and Santa Clara County to the east, the City and County of San Francisco to the north, and Santa Cruz County to the south (County of San Mateo, 2023). A large portion of the County is occupied by open space including state parks while population centers are primarily located along the two coast lines. Brisbane is located at the northern end of the county between South San Francisco and Millbrae. (Continued on page 3)

B11. Additional Resource Attributes: none

\*B12. References:

See page 7.

B13. Remarks: none

\*B14. Evaluator: Amy Langford, ESA

\*Date of Evaluation: June 2024



\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

\*B10. **Significance:** (Continued from page 2)

#### *Southern Pacific Railroad*

The subject property is located on land that was previously owned and occupied by the Southern Pacific Railroad. The following context is an excerpt from Carey & Company's technical report that was prepared for the Visitacion Valley Redevelopment EIR (Carey & Company, 2008):

The Southern Pacific Railroad Company was one of the most powerful and prominent of American railroad companies in the nineteenth and twentieth centuries. Founded in 1865 by San Francisco businessmen, with the aim to construct a line between San Francisco and San Diego, California, it eventually merged with Central Pacific Railroad Company in 1870, expanding the reach of the western portion of the First Transcontinental Railroad in the United States. In 1904, the Southern Pacific Railroad Company had begun filling in land along the Bay and building their tunnel, still extant, at the corner of Blanken and Tunnel avenues. Southern Pacific provided commuter service to Visitacion Valley on the line called the "Bayshore cutoff," which was known as the most important access route for the San Francisco Peninsula.

The creation of the Cutoff was the product of massive efforts to fill, level and dig through the land in order to connect the burgeoning industrial factory district of the South Bay to the heart of the city and spur development. In 1907, the cutoff was complete, and gave Southern Pacific trains a quick and more direct route into San Francisco. Shop facilities for repairing cars and locomotives were constructed along the Cutoff. A freight yard was established adjacent to the tracks and was noted as the major classification yard for the San Francisco terminal. Freight traffic in the Bay Area experienced a gradual decline in the 1970s, and by 1979, the Bayshore freight yard began moving yard activity to South San Francisco. In 1996, Southern Pacific Railroad Company was succeeded by Union Pacific Railroad. The former lots belonging to Southern Pacific are currently under the ownership of Universal Paragon, and the buildings are now occupied by various commercial organizations.

#### *Bayshore Boulevard*

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. "According to the 1913-1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse" (Carey & Company, 2008). "The Schlage Lock Factory was well established by this time, with numerous industrial buildings lining the eastern side of Bayshore Boulevard" (Carey & Company, 2008).

#### *Incorporation*

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. San Francisco to the north Daly City to the northwest and South San Francisco to the south each could have annexed the area. Development pressure was also felt from the Southern Pacific railroad, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a subsidiary of the Crocker Estate Company (Oral History Associates, Inc., 1989). Prior to incorporation, the area was under the control of San Mateo County (Oral History Associates, Inc., 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased the threat of annexation as the development would need to be annexed into only one of the surrounding cities in order to provide services such as police, fire, water, and sewer (Oral History Associates, Inc., 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc., 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted to incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc., 1989). The subject property is north of the originally incorporated city.

#### *Crocker Land Company*

The Crocker Land Company, a subsidiary of Crocker Estate Company, owned properties throughout California (Oral History Associates, Inc., 1989). The owner of the Crocker Estate Company was Charles Crocker, founder of Crocker Bank (Oral History Associates, Inc., 1989). "Its San Mateo County holdings included all of the undeveloped property on San Bruno Mountain and a large tract of tidelands on the east side of the Bayshore Freeway" (Oral History Associates, Inc., 1989). "The town of Brisbane was almost completely surrounded by Crocker-owned property" (Oral History Associates, Inc., 1989).

#### *Industrial Development*

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both Southern



\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

Pacific Railroad and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History Associates, Inc., 1989). Some of Southern Pacific Railroad's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc., 1989). The subject property is located on what was, in the early 1960s, lands owned by Southern Pacific Railroad (Oral History Associates, Inc., 1989). Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the Southern Pacific ceased operations.

### Ownership and Occupancy

#### Subject Property

The following is an excerpt from Paleowest's Cultural Resource Technical Report (Paleowest, 2022):

The original bay shoreline is approximately followed by Bayshore Boulevard. Construction of the Southern Pacific Railroad began in the early 1900s, extending the railroad tracks across the Bay. Following the 1906 San Francisco earthquake, the area west of the rail corridor was filled in primarily with demolition rubble.

On the east side of the rail corridor was a municipal landfill site which was established in the 1930s. [The subject property is to the west of the rail corridor and was developed prior to 1946 (Historicaerials.com, 1946). Deposition began in the north and it progressed southward until expansion of the landfill was terminated in the 1960s at the edge of what is now Brisbane Lagoon. The construction of US Highway 101 in the mid-1950s established the easternmost boundary of the Bay fill. The Brisbane Landfill site is 364 acres and is bounded by the Union Pacific/Joint Powers Board railroad corridor (Caltrain tracks) to the west, US Highway 101 to the east, and Brisbane Lagoon to the south. After the landfill operation was discontinued in 1967, approximately 20 to 30 ft of soil was placed over the site. Since the 1940s, the landfill has been developed into an industrial area and the existing lumberyards and warehouse buildings having been built on the oldest part of the landfill.

The subject property is developed with a light industrial building that was constructed between 1968 and 1980 (Figure 1). A review of historic aerial photographs indicates that the office building retains the same overall massing and footprint of its original design and construction.

A record of known alterations to the subject property is listed below in Table 1. Known owners and occupants are listed in Table 2.

**Table 1: Property & Building History for 10 Industrial Way**

Date	Alternation / Change	Source
1965	Historic aerial photograph shows that the subject property at 10 Industrial Way is developed with a parking lot and a small outbuilding to the north. A light industrial building to the southeast (40 Industrial Way) is present.	CAS_65-130 (May 1, 1965)
1968	Historic aerial photograph shows the same spatial configuration as the 1965 aerial photograph.	Historicaerials.com
1980	The small outbuilding is no longer present. A second, larger building has been constructed on the lot.	Historicaerials.com
1993	Historic aerial photograph shows the same spatial configuration of the subject property and adjacent parcels captured in 1980 and consistent with present conditions.	Historicaerials.com

**Table 2: Ownership & Use History for 10 Industrial Way**

Year(s) of Occupancy	Occupant(s)/Business	Source
Ca.2000-2015	Roseo and Ivana Panontin (Trust)	San Mateo County Assessor, Document #2015-055676; #2000-048406



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\*Resource Name or # 10 Industrial Way

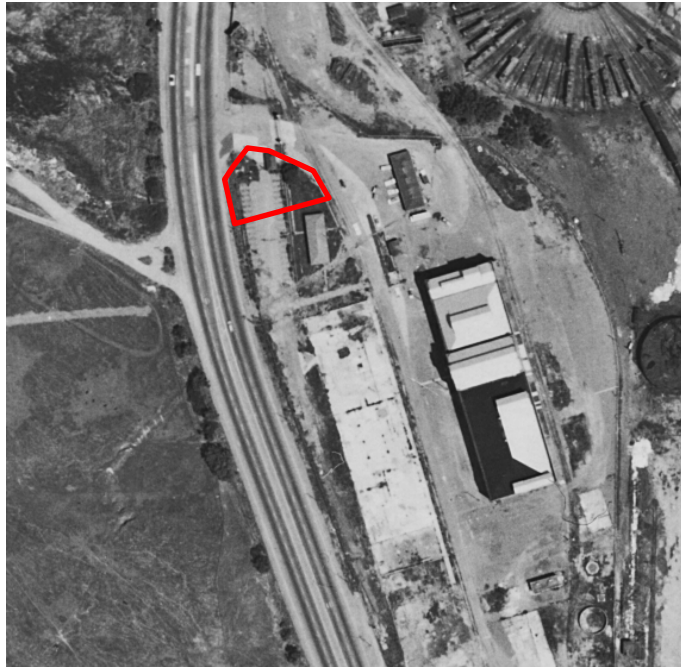
\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

2015-2024	Ivana Panontin (Trust) and Dino & Marlene Belluomini (Trust)	San Mateo County Assessor, Document #2015-055676
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Source: UC Santa Barbara Library, Framefinder Aerial Imagery, CAS\_65-130, Frame 1-82, 1:20,000, May 1, 1965, accessed May 24, 2024.  
**Figure 1.** 1965 Aerial Photograph of subject property (outlined in red). 40 Industrial Way is present to the south.

## Regulatory Framework

### *National Register of Historic Places*

A property is eligible for listing in the National Register of Historic Places (National Register) if it meets the National Register listing criteria at 36 CFR 60.4, as stated below:

The quality of significance in American history, architecture, archaeology, engineering, and culture is present in districts, sites, buildings, structures, and objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association and that:

- A) Are associated with events that have made a significant contribution to the broad patterns of our history, or
- B) Are associated with the lives of persons significant in our past, or
- C) Embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction, or
- D) Have yielded, or may be likely to yield, information important in prehistory or history

### *California Register of Historical Resources*

To be eligible for the California Register of Historical Resources (California Register) a historical resource must be significant under one or more of the following criteria:

- 1. Is associated with events that have made a significant contribution to the broad patterns of California's history and cultural heritage;
- 2. Is associated with the lives of persons important in our past;
- 3. Embodies the distinctive characteristics of a type, period, region, or method of construction, represents the work of an important creative individual, or possesses high artistic values; or
- 4. Has yielded, or may be likely to yield, information important to prehistory or history.

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\*Resource Name or # 10 Industrial Way

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

### Significance Evaluation

The subject property at 10 Industrial Way is evaluated below for potential historic significance according to National Register of Historic Places (National Register) Criteria A through D and California Register of Historical Resources (California Register) Criteria 1 through 4.

#### Criteria A/1 – Events

Research does not indicate that there are any significant associations between the building at 10 Industrial Way and important events or patterns in history. The extant light industrial building was constructed between 1968 and 1980 and appears to have continuously functioned as a commercial facility since its construction date. As a commercial property, 10 Industrial Way reflects an important period of economic and commercial growth in Brisbane after the city's incorporation in 1961, but not in an individually significant way. Archival research failed to indicate that the building is significant to the City of Brisbane or is associated with significant trends in local or regional development. For these reasons, 10 Industrial Way is recommended ineligible for individual listing under Criteria A/1.

#### Criteria B/2 – People

Preliminary research failed to uncover significant associations between 10 Industrial Way and significant persons or businesses. The subject property has likely been occupied by multiple businesses since its construction. However, the building's association with these various occupants does not appear to rise to the level of significance. As research does not indicate that 10 Industrial Way is significantly associated with the productive life of any significant person or business, it is recommended ineligible for individual listing under Criteria B/2.

#### Criteria C/3 – Design/Engineering

The subject property at 10 Industrial Way is not significant for its design or engineering. The property was developed between 1968 and 1980 and is a common example of a light industrial property type constructed during this period. It does not appear to be the work of a master architect. For these reasons, 10 Industrial Way is recommended ineligible for individual listing under Criteria C/3.

#### Criteria D/4 – Information Potential

While most often applied to archaeological districts and sites, Criteria D/4 can also apply to buildings, structures, and objects that contain important information. For these types of properties to be eligible under Criteria D/4, they themselves must be, or must have been, the principal source of the important information, and the information must be considered important. 10 Industrial Way is representative of a light industrial property type constructed during the mid-twentieth century. While it reflects an important period of industrial development in early Brisbane, the subject building was constructed of standard materials (wood, metal, and concrete) and with standard methodologies. Furthermore, the construction and manufacturing methodologies of light industrial buildings are well documented. There is no evidence to suggest that 10 Industrial Way would yield significant information that would expand our current knowledge or theories of design, methods of construction, or other information that is not already known. As such, it is recommended ineligible for listing under Criteria D/4.

#### Integrity Analysis

In addition to being eligible for listing under at least one of the National Register or California Register criteria, a property must also retain sufficient integrity to convey its historical significance. There are seven aspects to consider when evaluating the integrity of a property: location, design, setting, materials, workmanship, feeling, and association. As discussed above, the commercial building at 10 Industrial Way is not recommended as eligible for listing under any criteria; therefore, a further assessment of integrity is not presented.

### Summary

In summary, ESA recommends 10 Industrial Way ineligible for listing in the National Register or California Register under any criteria.

#### \*B12. References: (Continued from page 2)

Carey & Company. *Visitacion Valley Redevelopment EIR Historic Resources Technical Report*. April 29, 2008.

County of San Mateo. *About San Mateo County*. Accessed February 15, 2023. <https://www.smcgov.org/about-san-mateo-county>.

Historicaerials.com. Historic aerial photographs for 1968, 1980, and 1993. Accessed June 2024.

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

Oral History Associates, Inc. *Brisbane City of Stars: The First Twenty-Five Years 1961-1986*. 1989

Paleowest. *Cultural Resource Technical Report for the Brisbane Data Gap Investigation Project in the City of Brisbane, San Mateo County, California*. April 13, 2022.

Public Works Manual. United States, Public Works Journal Corporation. 1993.

San Mateo County Assessor. Document #2015-055676. 2015.

San Mateo County Assessor. Document #2000-048406. 2000.

UC Santa Barbara Library. Framefinder Aerial Imagery. CAS\_65-130, Frame 1-82. 1:20,000. May 1, 1965. Accessed May 24, 2024.

State of California — The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary  
HRI #  
Trinomial  
**NRHP Status Code**

Other Listings  
Review Code

Reviewer

Date

Page 1 of 9

**\*Resource Name or #:** 36 Industrial Way

**P1. Other Identifier:** Bayshore Sanitary District's Eric H. Carlyle Pump Station

**\*P2. Location:** ☐ Not for Publication ☒ Unrestricted

**\*a. County** San Mateo

**\*b. USGS 7.5' Quad** San Francisco South **Date** 1981 Within unsectioned portion of Cañada de Guadalupe land grant

**c. Address** 36 Industrial Way **City** Brisbane **Zip** 94005

**d. UTM:** Zone 10, 552206.91 mE/ 4172754.97 mN; NAD 83

**e. Other Locational Data:** APN 005-311-120

**\*P3a. Description:** 36 Industrial Way is a one-story public utility building situated on a 0.193-acre lot in an industrial and commercial area of the City of Brisbane. The facility is accessed via a paved, gated entrance at the north end of Industrial Way and surrounded by metal fencing and landscaping consisting of mature bushes. It is of wood-frame construction and features a rectangular footprint measuring approximately 70 feet long and 25 feet wide. The building is clad in brick siding and capped with a flat roof covered with what appears to be lightweight structural concrete. The primary (west) façade is clad with brick siding featuring a diamond-like design. It features a recessed entry enclosed by a metal gate. Within the entry, two wood-frame, unglazed wood doors provide access to what appears to be an office space and multi-purpose facility room (**Figure 1**). A sign reading "Bayshore Sanitary District" is mounted to the north of the recessed entry. A wood and concrete partition/storage area is located along the north portion of the façade and a cluster of mature bushes lines the south. The rear (south) façade is clad in brick siding and devoid of fenestration (**Figure 2**). The east façade is likewise devoid of fenestration and appears to feature a wood and metal utility access door (**Figure 3**). On the building's north façade, metal and plastic utility boxes are located to the east and west of a roll-up, metal, garage door. A poured concrete driveway leads to the garage entrance at a moderate incline. Additional large metal utility storage structures frame the driveway to the east and west (**Figure 4**). (Continued on page 3)

**\*P3b. Resource Attributes:** HP9. Public utility building

**\*P4. Resources Present:** ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo: North and west façades of 36 Industrial Way, view facing southeast. ESA, 2024.

**\*P6. Date Constructed/Age and Source:**

☒ Historic ☐ Prehistoric

☐ Both

1972. Source: Bayshore Sanitary District, 2024.

**\*P7. Owner and Address:**

Bayshore Sanitary District  
36 Industrial Way  
Brisbane, CA 94005-1002

**P8. Recorded by:**

Amy Langford, ESA  
550 Kearny Street  
San Francisco, CA 94108

**\*P9. Date Recorded:** June 13, 2024

**\*P10. Survey Type:**

Intensive

**\*P11. Report Citation:** Sims, Cleveland, and Grady, *Baylands Specific Plan, Cultural Resources Technical Report*, prepared by Environmental Science Associates, prepared for Metis Environmental Group, 2023.

**\*Attachments:** ☐ NONE ☐ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record  
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record  
☐ Artifact Record ☐ Photograph Record ☐ Other (List):



## BUILDING, STRUCTURE, AND OBJECT RECORD

\*Resource Name or # 36 Industrial Way \*NRHP Status Code 6Z

Page 2 of 9

- B1. Historic Name: Eric H. Carlyle Pump Station  
B2. Common Name: Eric H. Carlyle Pump Station  
B3. Original Use: Pump Station B4. Present Use: Pump Station/Bayshore Sanitary District Headquarters  
\*B5. Architectural Style: Utilitarian  
\*B6. Construction History: (Construction date, alterations, and date of alterations)  
Constructed between 1968 and 1980. See Table 1 on page 5 for more information.

\*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date: N/A Original Location: N/A

\*B8. Related Features: none

B9a. Architect: Unknown b. Builder: Unknown  
\*B10. Significance: Theme Mid-Century Industrial Development Area Brisbane  
Period of Significance N/A Property Type Public Utility Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

### Brisbane

As the subject property is in the City of Brisbane, which was not incorporated until the 1960s, the early history of the area is best understood from the history of San Mateo County. "San Mateo County was formed in April 1856 out of the southern portion of then-San Francisco County with a population of fewer than 5,000 people and little in the way of commerce." (County of San Mateo, 2023) It is 455 square miles and is bordered by the Pacific Ocean to the west, San Francisco Bay and Santa Clara County to the east, the City and County of San Francisco to the north, and Santa Cruz County to the south (County of San Mateo, 2023). A large portion of the County is occupied by open space including state parks while population centers are primarily located along the two coast lines. Brisbane is located at the northern end of the county between South San Francisco and Millbrae. (Continued on page 3)

B11. Additional Resource Attributes: none

\*B12. References:

See page 9.

B13. Remarks: none

\*B14. Evaluator: Amy Langford, ESA

\*Date of Evaluation: June 2024



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\*Resource Name or # 36 Industrial Way

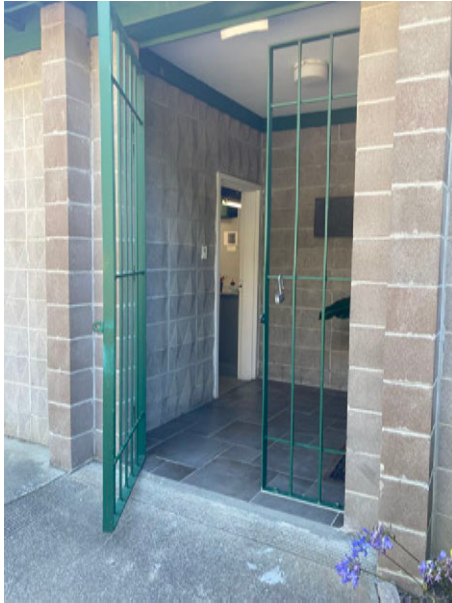
\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

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\*P3a. Description: (Continued from page 1)



Recessed entry, view facing northeast (**Figure 1**) and rear (south) façade, view facing northeast (**Figure 2**).



East façade, view facing southwest (**Figure 3**) and north façade, view facing south (**Figure 4**).

\*B10. Significance: (Continued from page 2)

#### *Southern Pacific Railroad*

The subject property is located on land that was previously owned and occupied by the Southern Pacific Railroad. The following context is an excerpt from Carey & Company's technical report that was prepared for the Visitation Valley Redevelopment EIR (Carey & Company, 2008):

The Southern Pacific Railroad Company was one of the most powerful and prominent of American railroad companies in the nineteenth and twentieth centuries. Founded in 1865 by San Francisco businessmen, with the aim to construct a line between San Francisco and San Diego, California, it eventually merged with Central Pacific Railroad Company in 1870, expanding the reach of the western portion of the First Transcontinental Railroad in the United States. In 1904, the Southern Pacific Railroad Company had begun filling in land along the Bay and building their tunnel, still extant, at the corner of Blanken and Tunnel



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\*Date: June 2024

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avenues. Southern Pacific provided commuter service to Visitacion Valley on the line called the "Bayshore cutoff," which was known as the most important access route for the San Francisco Peninsula.

The creation of the Cutoff was the product of massive efforts to fill, level and dig through the land in order to connect the burgeoning industrial factory district of the South Bay to the heart of the city and spur development. In 1907, the cutoff was complete, and gave Southern Pacific trains a quick and more direct route into San Francisco. Shop facilities for repairing cars and locomotives were constructed along the Cutoff. A freight yard was established adjacent to the tracks and was noted as the major classification yard for the San Francisco terminal. Freight traffic in the Bay Area experienced a gradual decline in the 1970s, and by 1979, the Bayshore freight yard began moving yard activity to South San Francisco. In 1996, Southern Pacific Railroad Company was succeeded by Union Pacific Railroad. The former lots belonging to Southern Pacific are currently under the ownership of Universal Paragon, and the buildings are now occupied by various commercial organizations.

#### *Bayshore Boulevard*

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. "According to the 1913-1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse" (Carey & Company, 2008). "The Schlage Lock Factory was well established by this time, with numerous industrial buildings lining the eastern side of Bayshore Boulevard" (Carey & Company, 2008).

#### *Incorporation*

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. San Francisco to the north Daly City to the northwest and South San Francisco to the south each could have annexed the area. Development pressure was also felt from the Southern Pacific railroad, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a subsidiary of the Crocker Estate Company (Oral History Associates, Inc., 1989). Prior to incorporation, the area was under the control of San Mateo County (Oral History Associates, Inc., 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased the threat of annexation as the development would need to be annexed into only one of the surrounding cities in order to provide services such as police, fire, water, and sewer (Oral History Associates, Inc., 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc., 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted to incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc., 1989). The subject property is north of the originally incorporated city.

#### *Crocker Land Company*

The Crocker Land Company, a subsidiary of Crocker Estate Company, owned properties throughout California (Oral History Associates, Inc., 1989). The owner of the Crocker Estate Company was Charles Crocker, founder of Crocker Bank (Oral History Associates, Inc., 1989). "Its San Mateo County holdings included all of the undeveloped property on San Bruno Mountain and a large tract of tidelands on the east side of the Bayshore Freeway" (Oral History Associates, Inc., 1989). "The town of Brisbane was almost completely surrounded by Crocker-owned property" (Oral History Associates, Inc., 1989).

#### *Industrial Development*

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both Southern Pacific Railroad and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History Associates, Inc., 1989). Some of Southern Pacific Railroad's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc., 1989). The subject property is located on what was, in the early 1960s, lands owned by Southern Pacific Railroad (Oral History Associates, Inc., 1989). Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the Southern Pacific ceased operations.

#### *Bayshore Sanitary District*

The Bayshore Sanitary District is an independent special district<sup>1</sup> located in San Mateo County, California. In April 1925, the San Mateo County Board of Supervisors voted to approve a petition for the formation of the Bayshore

<sup>1</sup> A local government entity established by local community groups to provide specialized services not offered by a local city or

## CONTINUATION SHEET

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\*Resource Name or # 36 Industrial Way

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation☐ Update

Sanitary district to serve the residents of Visitation Valley (*Redwood City Standard*, 1925). The following May, residents voted for the formation of the district, which was duly established according to Section 6400 et seq. of the California Health and Safety Code (San Mateo County, 2024). The District subsequently constructed a collection system that discharged wastewater directly to the San Francisco Bay (Yeager, 2018).

The District underwent a second period of expansion during the 1960s and 1970s. In 1961, the City of Brisbane incorporated and included a segment of the District's 1060-acre region. In 1963, the remaining portion of the District outside of Brisbane's city limits was annexed by Daly City. During the 1960s, a wastewater pump station and force main were constructed along Brisbane's Tunnel Avenue. From there, wastewater was collected and pumped to San Francisco for treatment (Yeager, 2018). In 1971, the District constructed a new wastewater collection and storm drainage system. Said phase of construction included a second pump station. Completed in 1972, the Carlyle Pump Station (CPS) and associated force main remains in operation. According to the Bayshore Sanitary District, the "majority of the District's wastewater flows into this station and is pumped into the City & County of San Francisco's southeast treatment plant where it receives secondary treatment" (Bayshore Sanitary District, 2024). As of 2024, the facility also houses the Bayshore Sanitary District's District Board Room (Bayshore Sanitary District, 2024).

The district currently provides sewage collection and disposal services to a total of 1546 parcels located in the City of Brisbane and Daly City, "including the Bayshore neighborhood south of the San Francisco County line, from the San Francisco collection depot on Tunnel Road south to the Guadalupe Canal and the industrial area south of Geneva Avenue (San Mateo County, 2024) (Figure 5).

**Ownership and Occupancy***Subject Property*

The following is an excerpt from Paleowest's Cultural Resource Technical Report (Paleowest, 2022):

The original bay shoreline is approximately followed by Bayshore Boulevard. Construction of the Southern Pacific Railroad began in the early 1900s, extending the railroad tracks across the Bay. Following the 1906 San Francisco earthquake, the area west of the rail corridor was filled in primarily with demolition rubble.

On the east side of the rail corridor was a municipal landfill site which was established in the 1930s. [The subject property is to the west of the rail corridor and was developed prior to 1946 (Historicaerials.com, 1946). Deposition began in the north and it progressed southward until expansion of the landfill was terminated in the 1960s at the edge of what is now Brisbane Lagoon. The construction of US Highway 101 in the mid-1950s established the easternmost boundary of the Bay fill. The Brisbane Landfill site is 364 acres and is bounded by the Union Pacific/Joint Powers Board railroad corridor (Caltrain tracks) to the west, US Highway 101 to the east, and Brisbane Lagoon to the south. After the landfill operation was discontinued in 1967, approximately 20 to 30 ft of soil was placed over the site. Since the 1940s, the landfill has been developed into an industrial area and the existing lumberyards and warehouse buildings having been built on the oldest part of the landfill.

The subject property is developed with a pump station that was constructed in 1972. It currently serves as the District Headquarters of the Bayshore Sanitary District (San Mateo County, 2024). A review of historic aerial photographs indicates that the building retains the same overall massing and footprint of its original design and construction. Preliminary research did not identify, aside from the Bayshore Sanitary District, any previous owners or occupants of the building at 36 Industrial Way.

A record of known alterations to the subject property is listed below in Table 1.

**Table 1: Property & Building History for 36 Industrial Way**

Date	Alternation / Change	Source
1965	Historic aerial photograph shows that the subject property at 36 Industrial Way is undeveloped. A light industrial building to the east (40 Industrial Way) is present.	CAS_65-130 (May 1, 1965) (Figure 6)
1968	Historic aerial photograph shows that the subject property at 36 Industrial Way is undeveloped.	Historicaerials.com
1980	A light industrial building with a rectangular footprint is present at 36 Industrial Way. A second, larger building has been constructed on a lot to the immediate north of the subject property (10 Industrial Way).	Historicaerials.com

county.



\*Recorded by: Amy Langford, ESA

\*Date: June 2024

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1993	Historic aerial photograph shows the same spatial configuration of the subject property and adjacent parcels captured in 1980 and consistent with present conditions.	Historicaerials.com
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Source: San Mateo County, Local Agency Formation Commission.  
**Figure 5.** Boundary of the Bayshore Sanitary District (bolded black).



Source: UC Santa Barbara Library, Framefinder Aerial Imagery, CAS\_65-130, Frame 1-82, 1:20,000, May 1, 1965, accessed May 24, 2024.  
**Figure 6.** 1965 Aerial Photograph of subject property (outlined in red). 40 Industrial Way is present to the east.

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

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### Regulatory Framework

#### *National Register of Historic Places*

A property is eligible for listing in the National Register of Historic Places (National Register) if it meets the National Register listing criteria at 36 CFR 60.4, as stated below:

The quality of significance in American history, architecture, archaeology, engineering, and culture is present in districts, sites, buildings, structures, and objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association and that:

- A) Are associated with events that have made a significant contribution to the broad patterns of our history, or
- B) Are associated with the lives of persons significant in our past, or
- C) Embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction, or
- D) Have yielded, or may be likely to yield, information important in prehistory or history

#### *California Register of Historical Resources*

To be eligible for the California Register of Historical Resources (California Register) a historical resource must be significant under one or more of the following criteria:

- 1. Is associated with events that have made a significant contribution to the broad patterns of California's history and cultural heritage;
- 2. Is associated with the lives of persons important in our past;
- 3. Embodies the distinctive characteristics of a type, period, region, or method of construction, represents the work of an important creative individual, or possesses high artistic values; or
- 4. Has yielded, or may be likely to yield, information important to prehistory or history.

### Significance Evaluation

The subject property at 36 Industrial Way is evaluated below for potential historic significance according to National Register of Historic Places (National Register) Criteria A through D and California Register of Historical Resources (California Register) Criteria 1 through 4.

#### *Criteria A/1 – Events*

Research does not indicate that there are any significant associations between the building at 36 Industrial Way and important events or patterns in history. The Carlyle Pump Station provides a necessary public service (i.e., pumping untreated wastewater), and it is not exceptional or significant for the role that it plays in San Mateo County. Constructed in 1972, the Station replaced an earlier water treatment facility on Tunnel Avenue and, as such, has historically been a key component of a larger regional wastewater infrastructure managed by the Bayshore Sanitary District, one of the only remaining special districts established prior to the incorporation of the City of Brisbane in 1961. While the Station plays an essential role in public health and sanitation, it is not significantly associated with events or thematic activities that have made a significant contribution to the broader patterns of local or regional history, nor the cultural heritage of California or the United States. For these reasons, 36 Industrial Way is recommended ineligible for individual listing under Criteria A/1.

#### *Criteria B/2 – People*

Preliminary research failed to uncover significant associations between 36 Industrial Way and significant persons or businesses. The Station bears the name of Eric H. Carlyle, who was presumably a longtime employee or associate of the Bayshore Sanitary District. However, preliminary research failed to uncover Carlyle's professional contributions to the District or San Mateo County government. It appears that none of Carlyle's productive life (i.e., the time period when he achieved significance) was spent at the Station, as it was constructed a year after his death (Find a Grave, 2024). Additionally, Criteria 2/1 "is generally restricted to those properties that illustrate (rather than commemorate) a person's important achievements" (National Park Service, 1997).

Research also failed to identify any other individuals associated with the Brisbane Sanitary district that would meet the aforementioned threshold for significance. Since its construction, numerous people have been employed at the Station over the years. Preliminary archival research does not indicate that the Station is significantly associated with any people who were/are important to local, California, or national history. As research does not indicate that 36 Industrial Way is significantly

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

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associated with the productive life of any significant person, it is recommended ineligible for individual listing under Criteria B/2.

*Criteria C/3 – Design/Engineering*

The subject property at 36 Industrial Way is not significant for its design or engineering. The Carlyle Pump Station was originally designed as a wastewater pump station. The building was designed in a utilitarian style with the incorporation of diamond-like details on the building's concrete brick cladding that indicates some appeal to aesthetics. However, the building does not exemplify a distinctive architectural style or embody the distinctive characteristics of a type, period, region, or method of construction. The property was constructed in 1972 and is a common example of a utilitarian property type constructed during this period. It does not appear to be the work of a master architect. Preliminary research did not identify an architect or architectural firm responsible for the design of the Carlyle Pump Station, and it is likely that a staff architect of the Brisbane Sanitary District was involved in its construction. The building served as a modernized replacement for an earlier pump station located on Tunnel Avenue, however, the Station does not possess or represent any engineering innovation that rises to the level of significance. Finally, the design of the Station does not articulate a particular concept of design so fully that it expresses an aesthetic ideal, and it therefore does not possess high artistic value. For these reasons, 36 Industrial Way is recommended ineligible for individual listing under Criteria C/3.

*Criteria D/4 – Information Potential*

While most often applied to archaeological districts and sites, Criteria D/4 can also apply to buildings, structures, and objects that contain important information. For these types of properties to be eligible under Criteria D/4, they themselves must be, or must have been, the principal source of the important information, and the information must be considered important. 36 Industrial Way is representative of a light industrial property type constructed during the mid-twentieth century. While it reflects an important period of industrial development in early Brisbane, the subject building was constructed of standard materials (wood, metal, and concrete) and with standard methodologies. Furthermore, the construction and manufacturing methodologies of light industrial buildings are well documented. There is no evidence to suggest that 36 Industrial Way would yield significant information that would expand our current knowledge or theories of design, methods of construction, or other information that is not already known. As such, it is recommended ineligible for listing under Criteria D/4.

*Integrity Analysis*

In addition to being eligible for listing under at least one of the National Register or California Register criteria, a property must also retain sufficient integrity to convey its historical significance. There are seven aspects to consider when evaluating the integrity of a property: location, design, setting, materials, workmanship, feeling, and association. As discussed above, the commercial building at 36 Industrial Way is not recommended as eligible for listing under any criteria; therefore, a further assessment of integrity is not presented.

**Summary**

In summary, ESA recommends 36 Industrial Way ineligible for listing in the National Register or California Register under any criteria.

**\*B12. References:** (Continued from page 2)

Carey & Company. *Visitacion Valley Redevelopment EIR Historic Resources Technical Report*. April 29, 2008.

County of San Mateo. *About San Mateo County*. Accessed February 15, 2023. <https://www.smcgov.org/about-san-mateo-county>.

County of San Mateo. "Bayshore Sanitary District." Accessed June 10, 2024. <https://www.smcgov.org/lafco/bayshore-sanitary-district>.

Find a Grave. "Eric Harry Carlyle, 1989-1971." Accessed June 17, 2024. <https://www.findagrave.com/memorial/153775254/eric-harry-carlyle>.

Historicaerials.com. Historic aerial photographs for 1968, 1980, and 1993. Accessed June 2024.

National Park Service. *National Register Bulletin 15: How to Apply the National Register Criteria for Evaluation*. 1997. Accessed June 17, 2024. [https://www.nps.gov/subjects/nationalregister/upload/NRB-15\\_web508.pdf](https://www.nps.gov/subjects/nationalregister/upload/NRB-15_web508.pdf).

Oral History Associates, Inc. *Brisbane City of Stars: The First Twenty-Five Years 1961-1986*. 1989

Paleowest. *Cultural Resource Technical Report for the Brisbane Data Gap Investigation Project in the City of Brisbane, San Mateo County, California*. April 13, 2022.

**CONTINUATION SHEET**

Trinomial

Page 9 of 9

\*Resource Name or # 36 Industrial Way

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

Public Works Manual. United States, Public Works Journal Corporation. 1993.

"Supervisors Meet." *Redwood City Standard*. April 9, 1925.

UC Santa Barbara Library. Framefinder Aerial Imagery. CAS\_65-130, Frame 1-82, 1:20,000. May 1, 1965. Accessed May 24, 2024.

Yeager, Thomas E. *Sewer System Management Plan (SSMP) Revision 3*. Prepared for Bayshore Sanitary District. November 2018.



State of California — The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary [Abstract]  
HRI #  
Trinomial  
**NRHP Status Code**

Other Listings  
Review Code

Reviewer

Date

Page 1 of 9

**\*Resource Name or #:** 40 Industrial Way

**P1. Other Identifier:** none

**\*P2. Location:** ☐ Not for Publication ☒ Unrestricted

**\*a. County** San Mateo

**\*b. USGS 7.5' Quad** South San Francisco **Date** 1981 Within unsectioned portion of Cañada de Guadalupe land grant

**c. Address** 40 Industrial Way **City** Brisbane **Zip** 94005

**d. UTM: Zone** 10S, 552202.59 **mE/** 4172729.48 **mN;** NAD 83

**e. Other Locational Data:** APN 005-311-110

**\*P3a. Description:** 40 Industrial way is a 2,123 square foot building located on a 0.289-acre lot in an industrial area of the City of Brisbane. The subject property is enclosed by metal fencing and appears to contain a cluster of metal storage containers and trailers. The building is of wood-frame construction and features a rectangular footprint measuring approximately 30 feet wide and 75 feet long. It is clad with brick siding and capped with a tiled hip roof. The north façade is the only portion of the building fully visible to the public right-of-way. It features what appears to have been three large windows that have been covered with cement cladding. A metal mail slot has been affixed to the former window at the west end of the façade. The west façade appears to feature two metal sash, sliding windows with transom lights, a pair of metal doors, and six metal sash, sliding windows (**Figure 1**). A wood and metal lean-to appears to be attached to the building's south façade. At least five metal sash windows are partially visible on the building's east façade.

**\*P3b. Resource Attributes:** HP6. 1-3 story commercial building

**\*P4. Resources Present:** ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



**P5b. Description of Photo:** North façade of 40 Industrial Way, view facing southwest. ESA, 2024.

**\*P6. Date Constructed/Age and Source:**

☒ Historic ☐ Prehistoric

☐ Both

Ca. 1956-1958. Source: Historicaerials.com; San Mateo County Assessor, 2024.

**\*P7. Owner and Address:**

Conal Gallagher  
1408 Wawona Street  
San Francisco, CA 94116-2951

**P8. Recorded by:**

Amy Langford, ESA  
550 Kearny Street  
San Francisco, CA 94108

**\*P9. Date Recorded:** June 13, 2024

**\*P10. Survey Type:**

Intensive

**\*P11. Report Citation:** Sims, Cleveland, and Grady, *Baylands Specific Plan, Cultural Resources Technical Report*, prepared by Environmental Science Associates, prepared for Metis Environmental Group, 2023.

**\*Attachments:** ☐ NONE ☐ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record  
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record  
☐ Artifact Record ☐ Photograph Record ☐ Other (List):

## BUILDING, STRUCTURE, AND OBJECT RECORD

Page 3 of 9

Resource Name or # 40 Industrial Way

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

\*Resource Name or # 40 Industrial Way

\*NRHP Status Code 6Z

Page 3 of 9

B1. Historic Name: none

B2. Common Name: none

B3. Original Use: Light Industrial

B4. Present Use: Light Industrial

\*B5. Architectural Style: Vernacular

\*B6. Construction History: (Construction date, alterations, and date of alterations)

Constructed ca. 1956-1958. Seen Table 1 on page 6 for more information.

\*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date: N/A Original Location: N/A

\*B8. Related Features: none

B9a. Architect: Unknown

b. Builder: Unknown

\*B10. Significance: Theme Mid-Century Industrial Development Area Brisbane

Period of Significance N/A

Property Type Industrial

Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

Brisbane

As the subject property is in the City of Brisbane, which was not incorporated until the 1960s, the early history of the area is best understood from the history of San Mateo County. "San Mateo County was formed in April 1856 out of the southern portion of then-San Francisco County with a population of fewer than 5,000 people and little in the way of commerce." (County of San Mateo, 2023) It is 455 square miles and is bordered by the Pacific Ocean to the west, San Francisco Bay and Santa Clara County to the east, the City and County of San Francisco to the north, and Santa Cruz County to the south (County of San Mateo, 2023). A large portion of the County is occupied by open space including state parks while population centers are primarily located along the two coast lines. Brisbane is located at the northern end of the county between South San Francisco and Millbrae. (Continued on page 3)

B11. Additional Resource Attributes: none

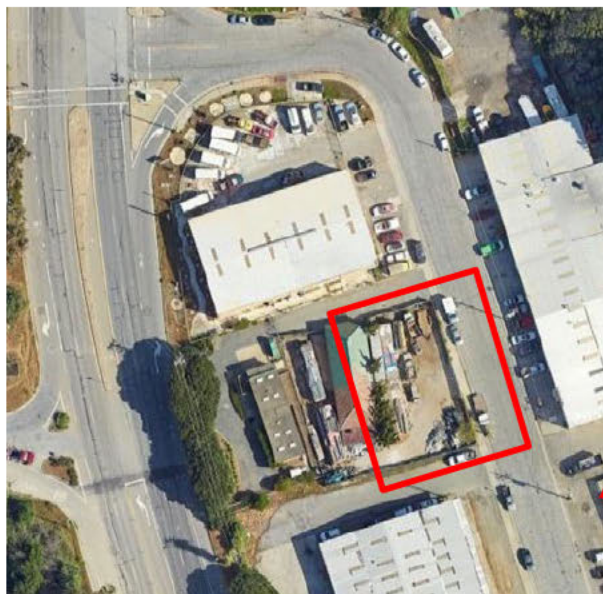
\*B12. References:

See page 9.

B13. Remarks: none

\*B14. Evaluator: Amy Langford, ESA

\*Date of Evaluation: June 2024





\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

\*P3a. Description: (Continued from page 1)



Figure 1. West façade of 40 Industrial Way, view facing southeast.

\*B10. Significance: (Continued from page 2)

#### *Southern Pacific Railroad*

The subject property is located on land that was previously owned and occupied by the Southern Pacific Railroad. The following context is an excerpt from Carey & Company's technical report that was prepared for the Visitacion Valley Redevelopment EIR (Carey & Company, 2008):

The Southern Pacific Railroad Company was one of the most powerful and prominent of American railroad companies in the nineteenth and twentieth centuries. Founded in 1865 by San Francisco businessmen, with the aim to construct a line between San Francisco and San Diego, California, it eventually merged with Central Pacific Railroad Company in 1870, expanding the reach of the western portion of the First Transcontinental Railroad in the United States. In 1904, the Southern Pacific Railroad Company had begun filling in land along the Bay and building their tunnel, still extant, at the corner of Blanken and Tunnel avenues. Southern Pacific provided commuter service to Visitacion Valley on the line called the "Bayshore cutoff," which was known as the most important access route for the San Francisco Peninsula.

The creation of the Cutoff was the product of massive efforts to fill, level and dig through the land in order to connect the burgeoning industrial factory district of the South Bay to the heart of the city and spur development. In 1907, the cutoff was complete, and gave Southern Pacific trains a quick and more direct route into San Francisco. Shop facilities for repairing cars and locomotives were constructed along the Cutoff. A freight yard was established adjacent to the tracks and was noted as the major classification yard for the San Francisco terminal. Freight traffic in the Bay Area experienced a gradual decline in the 1970s, and by 1979, the Bayshore freight yard began moving yard activity to South San Francisco. In 1996, Southern Pacific Railroad Company was succeeded by Union Pacific Railroad. The former lots belonging to Southern Pacific are currently under the ownership of Universal Paragon, and the buildings are now occupied by various commercial organizations.

#### *Bayshore Boulevard*

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. "According to the 1913-1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse" (Carey

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

& Company, 2008). "The Schlage Lock Factory was well established by this time, with numerous industrial buildings lining the eastern side of Bayshore Boulevard" (Carey & Company, 2008).

#### *Incorporation*

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. San Francisco to the north Daly City to the northwest and South San Francisco to the south each could have annexed the area. Development pressure was also felt from the Southern Pacific railroad, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a subsidiary of the Crocker Estate Company (Oral History Associates, Inc., 1989). Prior to incorporation, the area was under the control of San Mateo County (Oral History Associates, Inc., 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased the threat of annexation as the development would need to be annexed into only one of the surrounding cities in order to provide services such as police, fire, water, and sewer (Oral History Associates, Inc., 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc., 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted to incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc., 1989). The subject property is north of the originally incorporated city.

#### *Crocker Land Company*

The Crocker Land Company, a subsidiary of Crocker Estate Company, owned properties throughout California (Oral History Associates, Inc., 1989). The owner of the Crocker Estate Company was Charles Crocker, founder of Crocker Bank (Oral History Associates, Inc., 1989). "Its San Mateo County holdings included all of the undeveloped property on San Bruno Mountain and a large tract of tidelands on the east side of the Bayshore Freeway" (Oral History Associates, Inc., 1989). "The town of Brisbane was almost completely surrounded by Crocker-owned property" (Oral History Associates, Inc., 1989).

#### *Industrial Development*

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both Southern Pacific Railroad and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History Associates, Inc., 1989). Some of Southern Pacific Railroad's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc., 1989). The subject property is located on what was, in the early 1960s, lands owned by Southern Pacific Railroad (Oral History Associates, Inc., 1989). Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the Southern Pacific ceased operations.

#### **Ownership and Occupance**

##### *Subject Property*

The following is an excerpt from Paleowest's Cultural Resource Technical Report (Paleowest, 2022):

The original bay shoreline is approximately followed by Bayshore Boulevard. Construction of the Southern Pacific Railroad began in the early 1900s, extending the railroad tracks across the Bay. Following the 1906 San Francisco earthquake, the area west of the rail corridor was filled in primarily with demolition rubble.

On the east side of the rail corridor was a municipal landfill site which was established in the 1930s. [The subject property is to the west of the rail corridor and was developed prior to 1946 (Historicaerials.com, 1946). Deposition began in the north and it progressed southward until expansion of the landfill was terminated in the 1960s at the edge of what is now Brisbane Lagoon. The construction of US Highway 101 in the mid-1950s established the easternmost boundary of the Bay fill. The Brisbane Landfill site is 364 acres and is bounded by the Union Pacific/Joint Powers Board railroad corridor (Caltrain tracks) to the west, US Highway 101 to the east, and Brisbane Lagoon to the south. After the landfill operation was discontinued in 1967, approximately 20 to 30 ft of soil was placed over the site. Since the 1940s, the landfill has been developed into an industrial area and the existing lumberyards and warehouse buildings having been built on the oldest part of the landfill.

The subject property is developed with a light industrial building that appears to have been constructed ca. 1956. A review of historic aerial photographs indicates that a small rectangular building was present at the location of the subject property by 1941 (**Figure 2**). A review of subsequent historic aerials and San Mateo County assessor records indicate that the building present in 1941 was replaced with a building with a larger footprint by 1956 (**Figure 3**). The massing and footprint of the extant



\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

building appears to be largely unchanged from its original design. ESA staff observed that several original windows have been replaced and/or removed.

A record of known alterations to the subject property is listed below in **Table 1**. Known owners and occupants are listed in **Table 2**.

**Table 1: Property & Building History for 40 Industrial Way**

Date	Alternation / Change	Source
1941	Historic aerial photograph shows a rectangular building at the location of the subject property at 40 Industrial Way.	c-666 (March 23, 1941)
1956	San Mateo County assessor records indicate that the extant building was constructed in 1958. However, a historic aerial photograph shows a larger, rectangular building has been constructed at the location of the subject property at 40 Industrial Way by 1956.	Historicaerials.com; San Mateo County Assessor (2024)
1963	Historic aerial photograph shows the same building captured in the 1956 aerial photograph.	CAS_SM (January 1, 1963)
1968	Historic aerial photograph shows the same building captured in the 1956 and 1963 aerial photograph.	Historicaerials.com
1980	Two light industrial buildings have been constructed to the north (10 Industrial Way) and west (36 Industrial Way). A light industrial building to the east of the subject property has been replaced with a large industrial building.	Historicaerials.com
1993	Historic aerial photograph shows the same spatial configuration of the subject property and adjacent parcels captured in 1980 and consistent with present conditions.	Historicaerials.com

**Table 2: Ownership & Use History for 40 Industrial Way**

Year(s) of Occupancy	Owner/Occupant(s)/Business	Source
Unknown-2013	Linda and Wayne Goulding (trust) and Michael and Robert Cristofani (trust) (owners)	San Mateo County Assessor, Document #2013-126963 (2013)
2013-2024	Conal Gallagher (owner)	San Mateo County Assessor, Document #2013-126963 (2013)
2024	EnviroServices Brisbane (occupant)	Google maps (2024)



Source: UC Santa Barbara Library, Framefinder Aerial Imagery, c-666, Frame 25, 1:7,200, March 23, 1941, accessed May 24, 2024.

**Figure 2.** 1941 Aerial Photograph of subject property. A building in the location of 40 Industrial Way, outlined in red.



Source: UC Santa Barbara Library, Framefinder Aerial Imagery, CAS\_SM, Frame 1-81, 1:20,000, January 1, 1963, accessed May 24, 2024.

**Figure 3.** 1963 Aerial Photograph of subject property. 40 Industrial Way is outlined in red.

## Regulatory Framework

### *National Register of Historic Places*

A property is eligible for listing in the National Register of Historic Places (National Register) if it meets the National Register listing criteria at 36 CFR 60.4, as stated below:

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\*Date: June 2024

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The quality of significance in American history, architecture, archaeology, engineering, and culture is present in districts, sites, buildings, structures, and objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association and that:

- A) Are associated with events that have made a significant contribution to the broad patterns of our history, or
- B) Are associated with the lives of persons significant in our past, or
- C) Embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction, or
- D) Have yielded, or may be likely to yield, information important in prehistory or history

#### *California Register of Historical Resources*

To be eligible for the California Register of Historical Resources (California Register) a historical resource must be significant under one or more of the following criteria:

- 1. Is associated with events that have made a significant contribution to the broad patterns of California's history and cultural heritage;
- 2. Is associated with the lives of persons important in our past;
- 3. Embodies the distinctive characteristics of a type, period, region, or method of construction, represents the work of an important creative individual, or possesses high artistic values; or
- 4. Has yielded, or may be likely to yield, information important to prehistory or history.

#### **Significance Evaluation**

The subject property at 40 Industrial Way is evaluated below for potential historic significance according to National Register of Historic Places (National Register) Criteria A through D and California Register of Historical Resources (California Register) Criteria 1 through 4.

##### *Criteria A/1 – Events*

Research does not indicate that there are any significant associations between the building at 40 Industrial Way and important events or patterns in history. The extant light industrial building was constructed ca. 1956 and appears to have continuously functioned as a commercial facility since its construction date. As a commercial property, 40 Industrial Way reflects an important period of economic and commercial growth in Brisbane after the city's incorporation in 1961, but not in an individually significant way. Archival research failed to indicate that the building is significant to the City of Brisbane or is associated with significant trends in local or regional development. For these reasons, 40 Industrial Way is recommended ineligible for individual listing under Criteria A/1.

##### *Criteria B/2 – People*

Preliminary research failed to uncover significant associations between 40 Industrial Way and significant persons or businesses. The subject property has likely been occupied by multiple businesses since its construction. However, the building's association with these various occupants does not appear to rise to the level of significance. As research does not indicate that 40 Industrial Way is significantly associated with the productive life of any significant person or business, it is recommended ineligible for individual listing under Criteria B/2.

##### *Criteria C/3 – Design/Engineering*

The subject property at 40 Industrial Way is not significant for its design or engineering. The property was developed ca. 1956 and is a common example of a light industrial property type constructed during this period. It does not appear to be the work of a master architect. For these reasons, 40 Industrial Way is recommended ineligible for individual listing under Criteria C/3.

##### *Criteria D/4 – Information Potential*

While most often applied to archaeological districts and sites, Criteria D/4 can also apply to buildings, structures, and objects that contain important information. For these types of properties to be eligible under Criteria D/4, they themselves must be, or must have been, the principal source of the important information, and the information must be considered important. 40 Industrial Way is representative of a light industrial property type constructed during the mid-twentieth century. While it reflects an important period of industrial development in early Brisbane, the subject building was constructed of standard materials (wood, metal, and concrete) and with standard methodologies. Furthermore, the construction and manufacturing methodologies of light industrial buildings are well documented. There is no evidence to suggest that 40 Industrial Way would yield significant information that would expand our current knowledge or theories of design, methods of construction, or other information that is not already known. As such, it is recommended ineligible for listing under Criteria D/4.

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

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*Integrity Analysis*

In addition to being eligible for listing under at least one of the National Register or California Register criteria, a property must also retain sufficient integrity to convey its historical significance. There are seven aspects to consider when evaluating the integrity of a property: location, design, setting, materials, workmanship, feeling, and association. As discussed above, the commercial building at 40 Industrial Way is not recommended as eligible for listing under any criteria; therefore, a further assessment of integrity is not presented.

**Summary**

In summary, ESA recommends 40 Industrial Way ineligible for listing in the National Register or California Register under any criteria.

**\*B12. References:** (Continued from page 2)

Carey & Company. *Visitacion Valley Redevelopment EIR Historic Resources Technical Report*. April 29, 2008.

County of San Mateo. *About San Mateo County*. Accessed February 15, 2023. <https://www.smcgov.org/about-san-mateo-county>

Oral History Associates, Inc. *Brisbane City of Stars: The First Twenty-Five Years 1961-1986*. 1989.

Historicaerials.com. Historic aerial photographs for 1968, 1980, and 1993. Accessed June 2024.

Paleowest. *Cultural Resource Technical Report for the Brisbane Data Gap Investigation Project in the City of Brisbane, San Mateo County, California*. April 13, 2022.

Public Works Manual. United States, Public Works Journal Corporation. 1993.

San Mateo County Assessor. Document #2013-126963. 2013.

San Mateo County Assessor. Parcelquest property information. 2024.

UC Santa Barbara Library. Framefinder Aerial Imagery. CAS\_SM, Frame 1-81, 1:20,000. January 1, 1963. Accessed May 24, 2024.

UC Santa Barbara Library. Framefinder Aerial Imagery. c-666, Frame 25, 1:7,200. March 23, 1941. Accessed May 24, 2024.



State of California — The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary [Abstract]  
HRI #  
Trinomial  
**NRHP Status Code**

Other Listings  
Review Code

Reviewer

Date

Page 1 of 11

**\*Resource Name or #:** 2629 - 2635 Bayshore Boulevard

**P1. Other Identifier:** A. Silvestri Co.

**\*P2. Location:** ☐ Not for Publication ☒ Unrestricted

**\*a. County** San Mateo

**\*b. USGS 7.5' Quad** San Francisco South **Date** 1981 Within unsectioned portion of Cañada de Guadalupe land grant

**c. Address** 2629 and 2635 Bayshore Boulevard **City** Brisbane **Zip** 94005

**d. UTM:** Zone 10S, 552455.55 mE/ 4173621.79 mN; NAD 83

**e. Other Locational Data:** APN 005-340-110 (2629 Bayshore Boulevard) and APN 005-340-120 (2635 Bayshore Boulevard)

**\*P3a. Description:**

The subject property is located on two lots (APN 005-340-110 and APN 005-340-120) measuring a combined 11,468 square feet located on the boundary of the City of Brisbane and Daly City. 2635 Bayshore Boulevard (APN 005-340-110) is a one-story commercial building of wood-frame construction featuring an irregular footprint. It is primarily clad in wood board siding and capped with a double gabled roof covered with asphalt shingles. The primary (west) façade features two structural bays (**Figure 1**). It is clad primarily with a horizontal log veneer, with vertical logs beneath each gable peak and mortared stone cladding at the base of the façade. (Continued on page 3)

**\*P3b. Resource Attributes:** HP6. 1-3 story commercial building

**\*P4. Resources Present:** ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



**P5b. Description of Photo:** Primary façade of 2629-2635 Bayshore Boulevard, view facing east. ESA, 2024.

**\*P6. Date Constructed/Age and Source:**

☒ Historic ☐ Prehistoric

☐ Both

Ca. 1930s –1947 (2635 Bayshore Blvd) and ca. 1950 – 1965 (2629 Bayshore Blvd). Source: Carey & Co., Inc., 2008; San Mateo County Assessor, 2024; Cas-65-130\_1-81 (May 1, 1965).

**\*P7. Owner and Address:**

2629 Bayshore Boulevard:  
Alfeo & Paola Silvestri  
2635 Bayshore Boulevard  
San Francisco, CA 94134-2905

2635 Bayshore Boulevard:  
Paola Silvestri Trust, et al; Silvestri Family LP  
149 South Linden Avenue  
South San Francisco, CA 94080-6410

**P8. Recorded by:**

Amy Langford, ESA  
550 Kearny Street  
San Francisco, CA 94108

**\*P9. Date Recorded:** June 13, 2024

**\*P10. Survey Type:**

Intensive

**\*P11. Report Citation:** Sims, Cleveland, and Grady, *Baylands Specific Plan, Cultural Resources Technical Report*, prepared by Environmental Science Associates, prepared for Metis Environmental Group, 2023.

**\*Attachments:** ☐ NONE ☐ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record  
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record  
☐ Artifact Record ☐ Photograph Record ☐ Other (List):

## BUILDING, STRUCTURE, AND OBJECT RECORD

\*Resource Name or # 2629 - 2635 Bayshore Boulevard \*NRHP Status Code 6Z

Page 2 of 11

B1. Historic Name: George's Log Cabin/A. Silvestri Co.

B2. Common Name: A. Silvestri Co.

B3. Original Use: Saloon/Restaurant

B4. Present Use: Commercial/Garden Center

\*B5. Architectural Style: Vernacular commercial

\*B6. Construction History: (Construction date, alterations, and date of alterations)

A contiguous saloon and office building are present on the subject property by 1914. The extant building with its log veneer façade appears to have been an expansion of the original saloon building and appears to have been present by the 1930s. For more information, see Table 1 on page 8.

\*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date: N/A Original Location: N/A

\*B8. Related Features: none

B9a. Architect: Unknown

b. Builder: Unknown

\*B10. Significance: Theme Mid-Century Commercial Development Area Brisbane

Period of Significance N/A Property Type Commercial Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

### Brisbane

As the subject property is in the City of Brisbane, which was not incorporated until the 1960s, the early history of the area is best understood from the history of San Mateo County. "San Mateo County was formed in April 1856 out of the southern portion of then-San Francisco County with a population of fewer than 5,000 people and little in the way of commerce." (County of San Mateo, 2023). It is 455 square miles and is bordered by the Pacific Ocean to the west, San Francisco Bay and Santa Clara County to the east, the City and County of San Francisco to the north, and Santa Cruz County to the south (County of San Mateo, 2023). A large portion of the County is occupied by open space including state parks while population centers are primarily located along the two coast lines. Brisbane is located at the northern end of the county between South San Francisco and Millbrae. (Continued on page 4)

B11. Additional Resource Attributes: none

\*B12. References:

See page 11.

B13. Remarks: none

\*B14. Evaluator: Amy Langford, ESA

\*Date of Evaluation: June 2024





\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

\*P3a. Description: (Continued from page 1)

The north bay of the building features three metal sash, fixed windows secured with wrought iron bars. The south bay features a recessed entry with a metal-frame, partially glazed wood door. A metal and canvas awning is located at the entrance. To the south of the entrance are three metal sash, fixed windows secured with wrought iron bars. A small, fixed diamond-shaped window is located beneath the south gable. The north façade features a metal sash window secured with wrought iron bars and a wooden door that provides access to a neighboring lot enclosed by a metal fence that appears to function as statuary storage. An addition with a flat roof has been attached to the eastern portion of the north façade. Multiple additions are present on the east (rear) façade. These were inaccessible to ESA staff during a June 2024 pedestrian survey. The west façade (**Figure 2**) features a small multi-lite, fixed window and a partially-glazed wood door that provides access to the southern lot (APN 005-340-120) which is enclosed by a metal fence.



West façade (**Figure 1**) and south façade (**Figure 2**) of 2635 Bayshore Boulevard. Views facing east and northeast, respectively.

A flat roof addition currently addressed 2629 Bayshore Boulevard has been attached to the south façade of 2635 Bayshore Boulevard. The building appears to be primarily clad in stucco and wood board. The primary (west) façade features two large, uncovered entrances to what appears to be an outdoor statuary showroom. The south façade features a metal and wood awning supported by wood posts.



\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

**Figure 3.** West façade of 2629 Bayshore Boulevard. View facing southeast.

**\*B10. Significance:** (Continued from page 2)

*Southern Pacific Railroad*

The subject property is located on land that was previously owned and occupied by the Southern Pacific Railroad. The following context is an excerpt from Carey & Company's technical report that was prepared for the Visitacion Valley Redevelopment EIR (Carey & Company, 2008):

The Southern Pacific Railroad Company was one of the most powerful and prominent of American railroad companies in the nineteenth and twentieth centuries. Founded in 1865 by San Francisco businessmen, with the aim to construct a line between San Francisco and San Diego, California, it eventually merged with Central Pacific Railroad Company in 1870, expanding the reach of the western portion of the First Transcontinental Railroad in the United States. In 1904, the Southern Pacific Railroad Company had begun filling in land along the Bay and building their tunnel, still extant, at the corner of Blanken and Tunnel avenues. Southern Pacific provided commuter service to Visitacion Valley on the line called the "Bayshore cutoff," which was known as the most important access route for the San Francisco Peninsula.

The creation of the Cutoff was the product of massive efforts to fill, level and dig through the land in order to connect the burgeoning industrial factory district of the South Bay to the heart of the city and spur development. In 1907, the cutoff was complete, and gave Southern Pacific trains a quick and more direct route into San Francisco. Shop facilities for repairing cars and locomotives were constructed along the Cutoff. A freight yard was established adjacent to the tracks and was noted as the major classification yard for the San Francisco terminal. Freight traffic in the Bay Area experienced a gradual decline in the 1970s, and by 1979, the Bayshore freight yard began moving yard activity to South San Francisco. In 1996, Southern Pacific Railroad Company was succeeded by Union Pacific Railroad. The former lots belonging to Southern Pacific are currently under the ownership of Universal Paragon, and the buildings are now occupied by various commercial organizations.

*Bayshore Boulevard*

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. "According to the 1913-1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse" (Carey & Company, 2008). "The Schlage Lock Factory was well established by this time, with numerous industrial buildings lining the eastern side of Bayshore Boulevard" (Carey & Company, 2008).

*Incorporation*

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. San Francisco to the north Daly City to the northwest and South San Francisco to the south each could have annexed the area. Development pressure was also felt from the Southern Pacific railroad, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a subsidiary of the Crocker Estate Company (Oral History Associates, Inc., 1989). Prior to incorporation, the area was under the control of San Mateo County (Oral History Associates, Inc., 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased the threat of annexation as the development would need to be annexed to provide services such as police, fire, water, and sewer (Oral History Associates, Inc., 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc., 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted in incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc., 1989). The subject property is north of the originally incorporated city.

*Crocker Land Company*

The Crocker Land Company, a subsidiary of Crocker Estate Company, owned properties throughout California (Oral History Associates, Inc., 1989). The owner of the Crocker Estate Company was Charles Crocker, founder of Crocker Bank (Oral History Associates, Inc., 1989). "Its San Mateo County holdings included all of the undeveloped property on San Bruno Mountain and a large tract of tidelands on the east side of the Bayshore Freeway" (Oral



\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

History Associates, Inc., 1989). "The town of Brisbane was almost completely surrounded by Crocker-owned property" (Oral History Associates, Inc., 1989).

#### *Industrial Development*

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both Southern Pacific Railroad and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History Associates, Inc., 1989). Some of Southern Pacific Railroad's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc., 1989). The subject property is located on what was, in the early 1960s, lands owned by Southern Pacific Railroad (Oral History Associates, Inc., 1989). Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the Southern Pacific ceased operations.

#### **Ownership and Occupancy**

The subject property is developed with a multi-component commercial property that spans the San Francisco-San Mateo County Line. Archival research failed to conclusively identify the construction date of the extant building. However, a review of historic maps and aerial photographs suggests that the subject building may have developed from a contiguous saloon and office that appears on a 1914 Sanborn map (**Figure 4**). A 1941 aerial photograph indicates that the building had been expanded from its earlier 1914 footprint by the early 1940s (**Figure 5**). According to a subsequent 1950 Sanborn map, the saloon building had been converted into a restaurant and addressed 2629 Bayshore Boulevard (**Figure 6**). A review of San Francisco City Directories indicate that 2629 Bayshore Boulevard was occupied by a series of restaurants and bars until the mid-1970s.

A review of San Francisco city directories indicate that the southern portion of the subject building has been addressed 2635 Bayshore Boulevard since 1960. Between 1960 and 1963, 3635 Bayshore Boulevard was occupied by the S&H Upholstery company. In 1964, the outdoor statuary importer and garden supply center, A. Silvestri Co., is listed as the occupant for 2635 Bayshore Boulevard. Subsequent city directory records and assessor records indicate that A. Silvestri Co. has continuously occupied 3635 Bayshore Boulevard since that date (**Figure 8**). A previous recordation of the building indicates that the company operated a sister retail establishment across the street from the subject property at 2630 Bayshore Boulevard in 2008 (Carey & Co., 2008). ESA staff observations made during a June 2024 intensive survey indicates that A. Silvestri Co. currently owns and occupies commercial spaces at both 2629 and 2635 Bayshore Boulevard (ESA, 2024). A portion of the garden supply center operates out of a single-story building located to the southeast of the original building that first appears in a 1965 aerial photograph (**Figure 7**).

#### *A. Silvestri Company*

The A. Silvestri Company is a statuary and garden center business that has been owned and operated by members of the Silvestri Family for six generations. The company was first established by Arcangelo Silvestri and his two brothers after they settled in San Francisco in 1900 (A. Silvestri Co., 2024). The trio began manufacturing ornamental figurines and mouldings out of plaster of Paris, which they then peddled door to door in various San Francisco neighborhoods. The Silvestri brothers continued to manufacture and sell statuary until the onset of the Great Depression, whereupon they returned to their family home, the Tuscan village Bagni di Lucca. In 1956, Archangelo's son and grandson, Adorno and Alfeo (Luigi) Silvestri, followed their patriarch's footsteps and settled in San Francisco. The pair subsequently founded the A. Silvestri Company Fine Statuary and began manufacturing and reproducing garden ornaments in precast concrete (A. Silvestri Co., 2024). Publicly available immigration records confirm that Adorno Silvestri immigrated to the United States in 1956 (Ancestry, *Index to Alien Case Files*, 2022). Adorno Silvestri first appears in a 1959 San Francisco City Directory as an "importer" living on San Francisco's Mission Street (R.L. Polk & Co., 1959). Between 1961 and 1962, Adorno appears in city directories as the proprietor of Silvestri Garden Ornaments on San Francisco's Bosworth Street (R.L. Polk & Co., 1961-62). In 1963, the business remained at the same location on Bosworth Street but had been renamed the A. Silvestri Co. (R.L. Polk & Co., 1963). As previously noted, the A. Silvestri Co. was first listed as the occupant of 3635 Bayshore Boulevard in 1964 and appears to have continuously owned and occupied the subject building since. Luigi Silvestri continued to run the business until his death in 2013 (A. Silvestri Co., 2024). The business is currently run by Silvestri's wife, Paola Silvestri, and their daughters, Sandra and Rita (A. Silvestri Co., 2024).

A record of known alterations to the subject property is listed below in **Table 1**. Known owners and occupants are listed in **Table 2**.

**Table 1: Property & Building History for 2629 and 2635 Bayshore Boulevard**

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

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Date	Alternation / Change	Source
1914	A 1914 Sanborn Fire Insurance map shows a contiguous office and saloon on the subject property, which is situated on the boundary line for San Francisco and San Mateo Counties. The saloon is located in San Mateo County.	Sanborn Fire Insurance Co. (1914)
1941	An aerial photograph shows a building with the same massing as the saloon identified in the 1914 Sanborn map. A shed or outbuilding has been added to the building's southeast corner. A second, smaller building is located on the southeast corner of the subject property.	c-6660_319 (March 23, 1941)
1950	A 1950 Sanborn Fire Insurance map shows that the saloon building has been converted to a restaurant (addressed as 2629 Bayshore Boulevard). The original saloon building has been expanded, encompassing the original contiguous office and featuring what appears to be an additions to the building's north and east facades. The extant building at the southeast corner of the subject property does not appear on the map.	Sanborn Fire Insurance Co. (1950)
1965	An aerial photograph shows 2629-2635 Bayshore Boulevard with the same massing as its current configuration. A one-story building is attached to the southeast corner of the original building.	Cas-65-130_1-81 (May 1, 1965)

**Table 2: Ownership & Use History for 2629 Bayshore Boulevard**

Year(s) of Occupancy	Occupant(s)/Business	Source
1953	George Chan	R.L. Polk & Co. (1953)
1954-56	George's Place (restaurant), George Chan	R.L. Polk & Co. (1954; 1955-56)
1957-1965	George's Log Cabin (restaurant), George Chan	R.L. Polk & Co. (1957-65)
1966-70	Polynesian Hideaway (restaurant), George Chan	R.L. Polk & Co. (1966-70)
1971	George's Log Cabin Cocktail Lounge, George Chan	R.L. Polk & Co. (1971)
1972-73	Roman's Cantina	R.L. Polk & Co. (1972-73)
1974-76	Vacant	R.L. Polk & Co. (1974-76)
1977-80	Safeway (parking lot)	R.L. Polk & Co. (1977-80)
1981-82	Vacant	R.L. Polk & Co. (1981-82)
Unknown-Present	A. Silvestri Co.	San Mateo County Assessor (2024).

**Table 3: Ownership & Use History for 2635 Bayshore Boulevard**

Year(s) of Occupancy	Occupant(s)/Business	Source
1960-63	S&H Custom Upholstery	R.L. Polk & Co. (1960-63)
1964-Present	A. Silvestri and Co.	R.L. Polk & Co. (1964-82); San Mateo County Assessor (2024).

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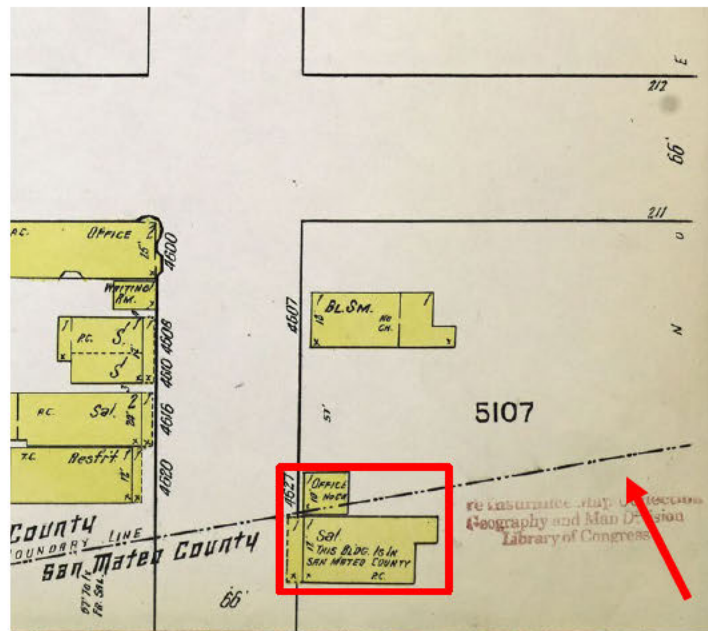
\*Resource Name or # 2629 - 2635 Bayshore Boulevard

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

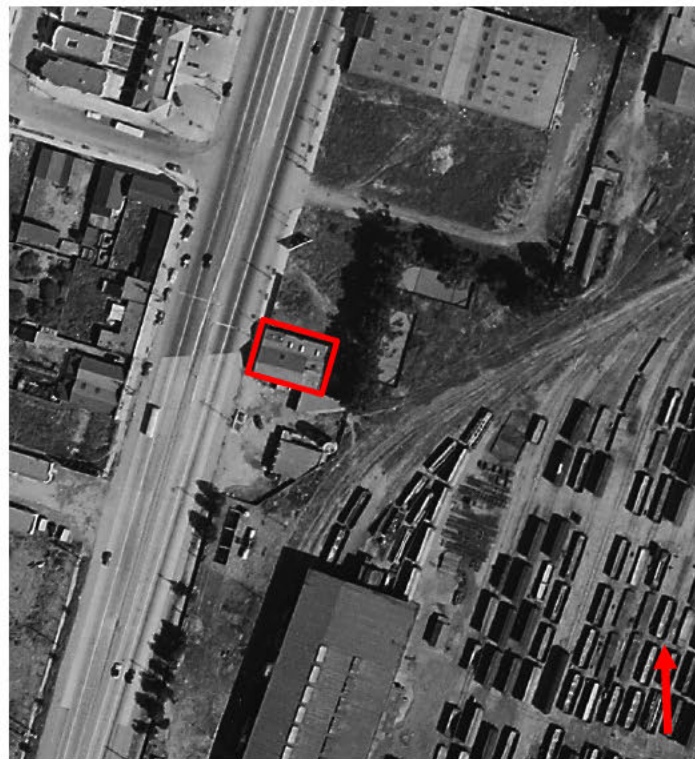
☒ Continuation

☐ Update



Source: Sanborn Fire Insurance Company, Insurance Maps of San Francisco, California, Volume 8, 1914, sheet 880.

**Figure 4.** A contiguous saloon and office building appear south of the corner of Sunnydale Avenue (east-west) and San Bruno Avenue (north-south). Subject property outlined in red.



Source: UC Santa Barbara Library, Framefinder Aerial Imagery, c-6660\_319, 1:7,200, March 23, 1941, accessed May 24, 2024.

**Figure 5.** 1941 Aerial Photograph of 2629-2635 Bayshore Boulevard. Subject property outlined in red.

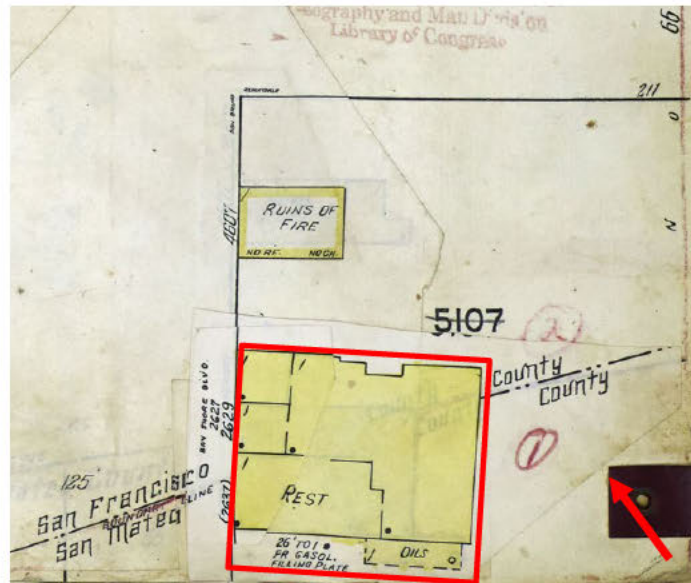


\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update



Source: Sanborn Fire Insurance Company, Insurance Maps of San Francisco, California, Volume 8, 1950, sheet 880.

Figure 6. The original building has been expanded. The original saloon has been converted into a restaurant. Note that San Bruno Avenue is now Bay Shore Boulevard. Subject property outlined in red.



Source: UC Santa Barbara Library, Framefinder Aerial Imagery, Cas-65-130\_1-81, 1:12,000, May 1, 1965, accessed May 24, 2024.

Figure 7. 1965 Aerial Photograph of 2629-2635 Bayshore Boulevard. Subject property outlined in red.



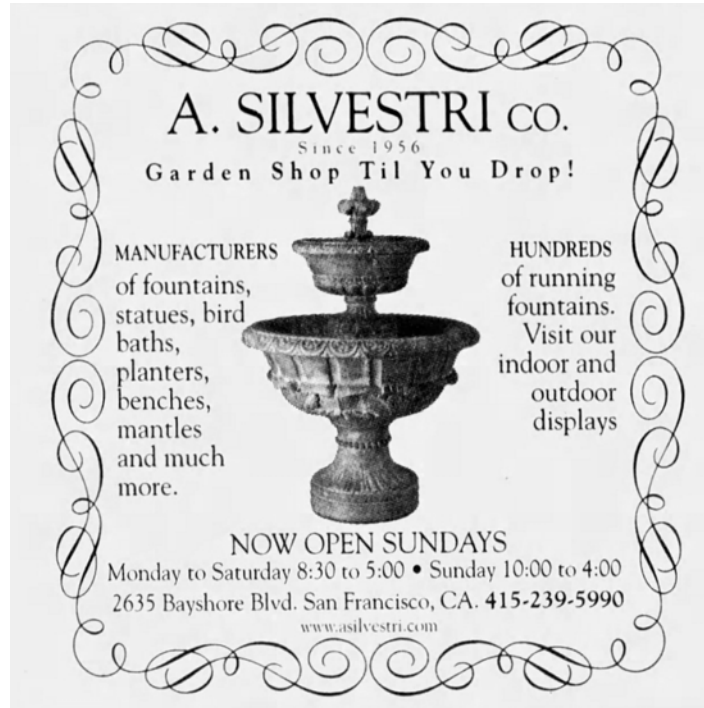


Figure 8. Source: *San Francisco Examiner*, March 10, 1999.

## Regulatory Framework

### *National Register of Historic Places*

A property is eligible for listing in the National Register of Historic Places (National Register) if it meets the National Register listing criteria at 36 CFR 60.4, as stated below:

The quality of significance in American history, architecture, archaeology, engineering, and culture is present in districts, sites, buildings, structures, and objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association and that:

- A) Are associated with events that have made a significant contribution to the broad patterns of our history, or
- B) Are associated with the lives of persons significant in our past, or
- C) Embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction, or
- D) Have yielded, or may be likely to yield, information important in prehistory or history

### *California Register of Historical Resources*

To be eligible for the California Register of Historical Resources (California Register) a historical resource must be significant under one or more of the following criteria:

- 1. Is associated with events that have made a significant contribution to the broad patterns of California's history and cultural heritage;
- 2. Is associated with the lives of persons important in our past;
- 3. Embodies the distinctive characteristics of a type, period, region, or method of construction, represents the work of an important creative individual, or possesses high artistic values; or
- 4. Has yielded, or may be likely to yield, information important to prehistory or history.

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

### Significance Evaluation

2635 Bayshore Boulevard was previously recorded by Carey & Co., Inc., in 2006 in preparation for the Visitacion Valley Redevelopment Environmental Impact Report, however, the building was not evaluated for potential historical significance for the National or California Registers (Su, 2008). The subject property at 2629-2635 Bayshore Boulevard is evaluated below for potential historic significance according to National Register of Historic Places (National Register) Criteria A through D and California Register of Historical Resources (California Register) Criteria 1 through 4.

#### *Criteria A/1 – Events*

Research does not indicate that there are any significant associations between the building at 2629-2635 Bayshore Boulevard and important events or patterns in history. Research indicates that the extant building is an expansion of a contiguous office and saloon originally located on the site at least by 1914. It has functioned as site of various local commercial enterprises—as a saloon, office, restaurant, cocktail lounge, and statuary and garden center. As such, the subject property reflects the general economic and commercial development of early Brisbane and San Mateo County, but not in an individually significant way. Archival research failed to indicate that the building is significant to the City of Brisbane or is associated with significant trends in local or regional development. For these reasons, 2629-2635 Bayshore Boulevard is recommended ineligible for individual listing under Criteria A/1.

#### *Criteria B/2 – People*

Research does not indicate that there are any associations between the building at 2629-2635 Bayshore Boulevard and significant persons. 2629 Bayshore Boulevard primarily functioned as an office, saloon, restaurant, and bar through the mid-1970s and was owned and occupied by multiple proprietors during that period. Archival research did not indicate that any of the building's earliest commercial enterprises were individually significant to broader patterns of economic, commercial, and social life in the City of Brisbane. Nor does research indicate that the building's associations with these various occupants rise to the level of significance. The building was owned and occupied for the greatest length of time by members of the Silvestri family, who established the A. Silvestri Company in San Francisco in the late 1950s. Records indicate that the Silvestri family has continuously owned and occupied a portion of the subject building since 1964, however, there is insufficient evidence to suggest that the A. Silvestri Company is a particularly noteworthy local business enterprise or that any individual member of the Silvestri family achieved historical acclaim (beyond a prolonged stewardship of the property) while occupying the building. For these reasons, 2629-2635 Bayshore Boulevard is recommended ineligible for individual listing under Criteria B/2.

#### *Criteria C/3 – Design/Engineering*

The building at 2629-2635 Bayshore Boulevard does not appear to possess architectural significance. It was constructed prior to 1914 by an unknown architect or builder and does not appear to be the work of a master architect or engineer. It is a common vernacular commercial property type constructed in San Francisco and San Mateo Counties during the early 20<sup>th</sup> century. Furthermore, the property appears to have undergone substantial modification since its original construction, including the expansion of the original building by 1941. For these reasons, 2629-2635 Bayshore Boulevard is recommended ineligible for individual listing under Criteria C/3.

#### *Criteria D/4 – Information Potential*

While most often applied to archaeological districts and sites, Criteria D/4 can also apply to buildings, structures, and objects that contain important information. For these types of properties to be eligible under Criteria D/4, they themselves must be, or must have been, the principal source of the important information, and the information must be considered important. 2629-2635 Bayshore Boulevard is representative of a commercial property type constructed during the early twentieth century. While it reflects an important period of commercial development in early Brisbane, the subject building was constructed of standard materials (wood, metal, and concrete) and with standard methodologies. Furthermore, the construction and manufacturing methodologies of vernacular commercial buildings are well documented. There is no evidence to suggest that 2629-2635 Bayshore Boulevard would yield significant information that would expand our current knowledge or theories of design, methods of construction, or other information that is not already known. As such, it is recommended ineligible for listing under Criteria D/4.

#### *Integrity Analysis*

In addition to being eligible for listing under at least one of the National Register or California Register criteria, a property must also retain sufficient integrity to convey its historical significance. There are seven aspects to consider when evaluating the integrity of a property: location, design, setting, materials, workmanship, feeling, and association. As discussed above, the commercial building at 2629-2635 Bayshore Boulevard is not recommended as eligible for listing under any criteria; therefore, a further assessment of integrity is not presented.

### Summary

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

In summary, ESA recommends 2629-2635 Bayshore Boulevard ineligible for listing in the National Register or California Register under any criteria.

**\*B12. References:** (Continued from page 2)

"A. Silvestri & Co." *San Francisco Examiner*. March 10, 1999.

A. Silvestri Co. "About A. Silvestri Co." Accessed May 23, 2024. <https://www.asilvestri.com/about-us.php>.

Ancestry.com. *U.S., Index to Ailen Case Files, 1944-2003* [database on-line]. Lehi, UT, USA: Ancestry.com Operations, Inc., 2022.

Carey & Company, Inc. *Visitation Valley Redevelopment EIR Historic Resources Technical Report, San Francisco, California*. Prepared for Wagstaff and Associates. April 29, 2008.

County of San Mateo. *About San Mateo County*. Accessed February 15, 2023. <https://www.smcgov.org/about-san-mateo-county>.

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Paleowest. *Cultural Resource Technical Report for the Brisbane Data Gap Investigation Project in the City of Brisbane, San Mateo County, California*. April 13, 2022.

Public Works Manual. United States, Public Works Journal Corporation. 1993.

R. L. Polk & Company. *Polk's San Francisco City Directory*. San Francisco, California: R.L. Polk & Co. 1953-1982.

Sanborn Fire Insurance Company. *Insurance Maps of San Francisco, California*. Volume 8, sheet 880. 1914.

Sanborn Fire Insurance Company. *Insurance Maps of San Francisco, California*. Volume 8, sheet 880. 1950.

Su, Anny. DPR523 Form Set for 2635 Bayshore Boulevard. Prepared by Carey & Company, Inc. Prepared for Wagstaff and Associates. October 20, 2006.

UC Santa Barbara Library. Framefinder Aerial Imagery. c-6660\_319, 1:7,200. March 23, 1941. Accessed May 24, 2024.

UC Santa Barbara Library. Framefinder Aerial Imagery. Cas-65-130\_1-81, 1:12,000. May 1, 1965. Accessed May 24, 2024.

State of California — The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary  
HRI #  
Trinomial  
**NRHP Status Code**

Other Listings  
Review Code

Reviewer

Date

Page 1 of 8

**\*Resource Name or #:** 595 Tunnel Avenue

**P1. Other Identifier:** none

**\*P2. Location:** ☐ Not for Publication ☒ Unrestricted

**\*a. County** San Mateo

**\*b. USGS 7.5' Quad** San Francisco South **Date** 1981 Within unsectioned portion of Cañada de Guadalupe land grant

**c. Address** 595 Tunnel Avenue **City** Brisbane **Zip** 94005

**d. UTM: Zone** 10S, 553015.17 mE/ 4173157.54 mN; NAD 83

**e. Other Locational Data:** APN 005-152-270

**\*P3a. Description:** 595 Tunnel Avenue contains two light industrial warehouses situated on a 3.515-acre lot in an industrial neighborhood in the City of Brisbane. The western portion of the subject property currently contains several clusters of large metal storage containers. The warehouses appear to be of wood-frame construction and feature a rectangular footprint measuring approximately 70 feet wide and 207 feet long. They are clad in corrugated metal siding and capped with a front gable roof covered with corrugated metal. The primary (west) façades feature large entrances with two corrugated metal sliding doors. The side facades appear to be devoid of fenestration. A review of recent aerial imagery indicates that an extension has been added to the rear façade of the north warehouse. This was inaccessible to ESA staff during a June 2024 pedestrian survey.

**\*P3b. Resource Attributes:** HP8. Industrial building

**\*P4. Resources Present:** ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo: West façade of warehouses. View facing northeast. ESA, 2024.

**\*P6. Date Constructed/Age and Source:**

☒ Historic ☐ Prehistoric

☐ Both

Ca. 1956-1963. Source: Historicaerials.com

**\*P7. Owner and Address:**

Recology Properties, Inc.  
50 California Street, 24<sup>th</sup> Floor  
San Francisco, CA 94111-4624

**P8. Recorded by:**

Amy Langford, ESA  
550 Kearny Street  
San Francisco, CA 94108

**\*P9. Date Recorded:** June 13, 2024

**\*P10. Survey Type:**

Intensive

**\*P11. Report Citation:** Sims, Cleveland, and Grady, *Baylands Specific Plan, Cultural Resources Technical Report*, prepared by Environmental Science Associates, prepared for Metis Environmental Group, 2023.

**\*Attachments:** ☐ NONE ☐ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record  
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record  
☐ Artifact Record ☐ Photograph Record ☐ Other (List):



## BUILDING, STRUCTURE, AND OBJECT RECORD

\*Resource Name or # 595 Tunnel Avenue \*NRHP Status Code 6Z

Page 2 of 8

B1. Historic Name: Van Arsdale-Harris Lumber Company

B2. Common Name: none

B3. Original Use: Warehouse

B4. Present Use: Warehouse

\*B5. Architectural Style: Utilitarian

\*B6. Construction History: (Construction date, alterations, and date of alterations)

Constructed between 1956 and 1963. For more information, see Table 1 on page 6.

\*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date: N/A Original Location: N/A

\*B8. Related Features: none

B9a. Architect: Unknown

b. Builder: Unknown

\*B10. Significance: Theme Mid-Century Industrial Development Area Brisbane

Period of Significance N/A

Property Type Industrial

Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

### Brisbane

As the subject property is in the City of Brisbane, which was not incorporated until the 1960s, the early history of the area is best understood from the history of San Mateo County. "San Mateo County was formed in April 1856 out of the southern portion of then-San Francisco County with a population of fewer than 5,000 people and little in the way of commerce." (County of San Mateo, 2023) It is 455 square miles and is bordered by the Pacific Ocean to the west, San Francisco Bay and Santa Clara County to the east, the City and County of San Francisco to the north, and Santa Cruz County to the south (County of San Mateo, 2023). A large portion of the County is occupied by open space including state parks while population centers are primarily located along the two coast lines. Brisbane is located at the northern end of the county between South San Francisco and Millbrae. (Continued on page 3)

B11. Additional Resource Attributes: none

\*B12. References:

See page 8.

B13. Remarks: none

\*B14. Evaluator: Amy Langford, ESA

\*Date of Evaluation: June 2024



\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

\*B10. **Significance:** (Continued from page 2)

#### *Southern Pacific Railroad*

The subject property is located on land that was previously owned and occupied by the Southern Pacific Railroad. The following context is an excerpt from Carey & Company's technical report that was prepared for the Visitacion Valley Redevelopment EIR (Carey & Company, 2008):

The Southern Pacific Railroad Company was one of the most powerful and prominent of American railroad companies in the nineteenth and twentieth centuries. Founded in 1865 by San Francisco businessmen, with the aim to construct a line between San Francisco and San Diego, California, it eventually merged with Central Pacific Railroad Company in 1870, expanding the reach of the western portion of the First Transcontinental Railroad in the United States. In 1904, the Southern Pacific Railroad Company had begun filling in land along the Bay and building their tunnel, still extant, at the corner of Blanken and Tunnel avenues. Southern Pacific provided commuter service to Visitacion Valley on the line called the "Bayshore cutoff," which was known as the most important access route for the San Francisco Peninsula.

The creation of the Cutoff was the product of massive efforts to fill, level and dig through the land in order to connect the burgeoning industrial factory district of the South Bay to the heart of the city and spur development. In 1907, the cutoff was complete, and gave Southern Pacific trains a quick and more direct route into San Francisco. Shop facilities for repairing cars and locomotives were constructed along the Cutoff. A freight yard was established adjacent to the tracks and was noted as the major classification yard for the San Francisco terminal. Freight traffic in the Bay Area experienced a gradual decline in the 1970s, and by 1979, the Bayshore freight yard began moving yard activity to South San Francisco. In 1996, Southern Pacific Railroad Company was succeeded by Union Pacific Railroad. The former lots belonging to Southern Pacific are currently under the ownership of Universal Paragon, and the buildings are now occupied by various commercial organizations.

#### *Bayshore Boulevard*

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. "According to the 1913-1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse" (Carey & Company, 2008). "The Schlage Lock Factory was well established by this time, with numerous industrial buildings lining the eastern side of Bayshore Boulevard" (Carey & Company, 2008).

#### *Incorporation*

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. San Francisco to the north Daly City to the northwest and South San Francisco to the south each could have annexed the area. Development pressure was also felt from the Southern Pacific railroad, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a subsidiary of the Crocker Estate Company (Oral History Associates, Inc., 1989). Prior to incorporation, the area was under the control of San Mateo County (Oral History Associates, Inc., 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased the threat of annexation as the development would need to be annexed into only one of the surrounding cities in order to provide services such as police, fire, water, and sewer (Oral History Associates, Inc., 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc., 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted to incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc., 1989). The subject property is north of the originally incorporated city.

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#### *Industrial Development*

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both Southern

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

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Pacific Railroad and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History Associates, Inc., 1989). Some of Southern Pacific Railroad's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc., 1989). The subject property is located on what was, in the early 1960s, lands owned by Southern Pacific Railroad (Oral History Associates, Inc., 1989). Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the Southern Pacific ceased operations.

*Van Arsdale-Harris Lumber Company*

The Van Arsdale-Harris Lumber Company was a large and successful San Francisco lumber company established in 1888 by prominent lumber industrialists William W. Van Arsdale and Matthew Harris (SFGate, 2006). William W. Van Arsdale was born in Missouri in 1856 (U.S. Census Bureau, 1900). By 1870, Van Arsdale was living in his parents' household in Mendocino, California (U.S. Census Bureau, 1870). While relatively little additional information could be gleaned about his early life, census records confirm that Van Arsdale was living in Siskiyou County, California, and working as a "general manager" and "rail road saw mill" operator by 1900 (U.S. Census Bureau, 1900). By this date, Harris had also established the Scott and Van Arsdale Lumber Company, which appears in an 1899 Sanborn map at the corner of Brannan and 5<sup>th</sup> Streets (**Figure 1**). Newspaper accounts indicate that the Company continued to prosper even after the 1906 San Francisco earthquake. In June of that year, it purchased another major local lumber company, the McCloud River Lumber Company, around 1906 (*Stockton Evening and Sunday Record*, 1906). At this point, Van Arsdale entered into a business partnership with fellow-lumberman Matthew Harris, when endured until his death in 1909 (*San Bernadino County Sun*, 1909).

Matthew Harris was born in Wisconsin in 1842 and moved to California at the age of ten (*Petaluma Argus Courier*, 1911). As a young man, Harris worked in mining in Calaveras County before later operating a teaming business that contracted with regional planning mills. Harris eventually established his first lumber firm, Harris & Jones. In 1895, Harris took over the management of the Scott & Van Arsdale Lumber Company in San Francisco. In 1900, Harris worked for the McCloud River Company, where he remained until 1906. After the McCloud River Company was purchased by the Van Arsdale Lumber Company, Harris became the president of the expanded and renamed Van Arsdale-Harris Lumber Company (**Figure 1**). Harris held that position until his death in 1911 (*San Francisco Chronicle*, 1911).

A review of historic newspapers indicates that the Van Arsdale-Harris Lumber Company enjoyed continued success following the death of its founders. During the early 1930s, the company became the sole manufacturer and distributor of the Brandsten International and Intercollegiate Official Springboard. The innovative diving board was invented by Ernie Brandsten, at the time the head coach of the American Olympic diving team. The board featured a adjustable fulcrum which, according to one industry publication, "adjusts the amount of spring a diver desires in a board, and the diver adjusts it by turning a wheel with his foot" (*The California Lumber Merchant*, 1932). During this time, the company also appears to have expanded its suite of services to include acoustical treatments, insulation, and sound control (*The Times*, 1939). Preliminary research uncovered relatively little information about the company's activities after 1950. However, newspaper records indicate that the Van Arsdale-Harris Lumber Company had begun to operate out of the subject property at 595 Tunnel Avenue by 1953 (*San Francisco Examiner*, 1953), where it appears to have continuously owned and/or occupied to the present (see the Owner and Occupancy section below for more information). Archival review did not indicate if the Company continued to occupy the Brannan Street



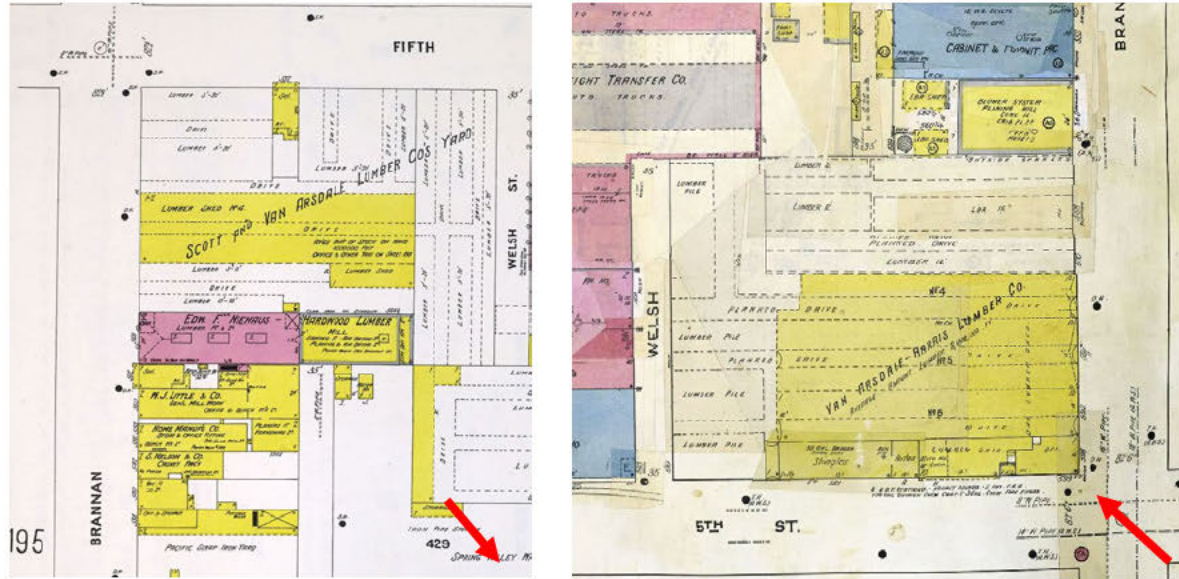
\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

location at this point. A review of recent aerial imagery indicates that the original location of the Van Arsdale-Harris Lumber Company on Brannan Street currently functions as a parking lot (Google maps, 2024).



Source: Sanborn Map Company, Insurance Maps of San Francisco, California, Vol. 2, 1899 and 1950.

Figure 1. Original location of the Van Arsdale-Harris Lumber Co. at Brannan and 5<sup>th</sup> Streets, San Francisco, as shown in 1899 (left) and 1950 (right) Sanborn maps.

### Ownership and Occupancy

The subject property is developed with a large, light industrial warehouse constructed between 1956 and 1963. A review of historic aerial photographs indicates that the property remained undeveloped at least by 1946 (Table 1). By 1953, the property had been purchased by the Van Arsdale-Harris Lumber Company. By 1956, the Company had constructed two light industrial warehouses on the western portion of the subject property. Subsequent historic aerial photographs indicate that the extant warehouse was constructed between 1956 and 1963 (Figure 2). The subject property appears to have undergone relatively few modifications until the demolition of the two original light industrial buildings between 2020 and 2024.



Source: UC Santa Barbara Library, Framefinder Aerial Imagery, Cas-sm\_1-81, 1:12,000, January 1, 1963, accessed May 24, 2024.

Figure 2. 1963 Aerial Photograph of 595 Tunnel Avenue. Subject property outlined in red.



\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

A record of known alterations to the subject property is listed below in **Table 1**. Known owners and occupants are listed in **Table 2**.

**Table 1: Property & Building History for 595 Tunnel Avenue**

Date	Alteration / Change	Source
1946	The subject property appears undeveloped in a 1946 aerial photograph.	Historicaerials.com
1956	An aerial photograph shows two light industrial buildings have been constructed on the western portion of the subject property.	Historicaerials.com
1963	An aerial photograph shows the extant warehouse has been constructed on the eastern portion of the subject property.	Cas-sm_1-81 (1963)
2020-2024	The two westernmost light industrial buildings appear to have been demolished at some point after 2020.	Historicaerials.com

**Table 2: Ownership & Use History for 595 Tunnel Avenue**

Year(s) of Occupancy	Owner/Occupant(s)/Business	Notes
1962- 2013	Van Arsdale-Harris Lumber Company (owner/occupant)	R.L. Polk & Co. (1962-82); ESA (2013).
2013-2024	Recology Properties Inc. (owner); Van Arsdale-Harris Lumber Company (occupant)	San Mateo County Assessor, Document #2013-147233; Better Business Bureau (2024).

### Regulatory Framework

#### *National Register of Historic Places*

A property is eligible for listing in the National Register of Historic Places (National Register) if it meets the National Register listing criteria at 36 CFR 60.4, as stated below:

The quality of significance in American history, architecture, archaeology, engineering, and culture is present in districts, sites, buildings, structures, and objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association and that:

- A) Are associated with events that have made a significant contribution to the broad patterns of our history, or
- B) Are associated with the lives of persons significant in our past, or
- C) Embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction, or
- D) Have yielded, or may be likely to yield, information important in prehistory or history

#### *California Register of Historical Resources*

To be eligible for the California Register of Historical Resources (California Register) a historical resource must be significant under one or more of the following criteria:

- 1. Is associated with events that have made a significant contribution to the broad patterns of California's history and cultural heritage;
- 2. Is associated with the lives of persons important in our past;
- 3. Embodies the distinctive characteristics of a type, period, region, or method of construction, represents the work of an important creative individual, or possesses high artistic values; or
- 4. Has yielded, or may be likely to yield, information important to prehistory or history.

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

### Significance Evaluation

The subject property at 595 Tunnel Avenue is evaluated below for potential historic significance according to National Register of Historic Places (National Register) Criteria A through D and California Register of Historical Resources (California Register) Criteria 1 through 4.

#### *Criteria A/1 – Events*

Research does not indicate that there are any significant associations between the building at 595 Tunnel Avenue and important events or patterns in history. Research indicates that the extant warehouse was constructed between 1956 and 1963 by the Van Arsdale-Harris Lumber Company and has continuously functioned as a commercial storage facility for the lumber industry since its construction date. While the Van Arsdale-Harris Lumber Company is associated with the development of the regional lumber industry in the years of rebuilding that followed the 1906 San Francisco earthquake, research failed to indicate that the building's association with these patterns of history rises to the level of significance. As a lumber warehouse, the subject property reflects an important period of economic and commercial growth in Brisbane after the city's incorporation in 1961, but not in an individually significant way (associations with historic businesses is discussed below under Criteria B/2). Archival research failed to indicate that the building is significant to the City of Brisbane or is associated with significant trends in local or regional development. For these reasons, 595 Tunnel Avenue is recommended ineligible for individual listing under Criteria A/1.

#### *Criteria B/2 – People*

Preliminary research failed to uncover significant associations between 595 Tunnel Avenue and significant persons or businesses. The subject property was constructed between 1956 and 1963 as a warehouse for the Van Arsdale-Harris Lumber Company, a prominent lumber company that was established in San Francisco in 1888. The Company had purchased and developed the subject property by 1953 and appears to have continuously owned and/or occupied the property since that period. The Company's founders, William W. Van Arsdale and Matthew Harris were prominent industrialists who were successful entrepreneurs in the lumber industry in California and the San Francisco Bay Area during the late 19<sup>th</sup> and early 20<sup>th</sup> centuries. However, Van Arsdale and Harris died in 1909 and 1911, respectively, over 40 years before the Company purchased the property at 595 Tunnel Avenue. Furthermore, there is no evidence to suggest that any subsequent owner or employee are significantly associated with the subject property. As research does not indicate that 595 Tunnel Avenue is significantly associated with the productive life of any significant person or business, it is recommended ineligible for individual listing under Criteria B/2.

#### *Criteria C/3 – Design/Engineering*

The subject property at 595 Tunnel Avenue is not significant for its design or engineering. The property was developed between 1956 and 1963 and is a common example of a light industrial property type constructed during this period. It does not appear to be the work of a master architect. Furthermore, the property appears to have undergone substantial modification since its original construction, including additions to the warehouse's west and north façades. For these reasons, 595 Tunnel Avenue is recommended ineligible for individual listing under Criteria C/3.

#### *Criteria D/4 – Information Potential*

While most often applied to archaeological districts and sites, Criteria D/4 can also apply to buildings, structures, and objects that contain important information. For these types of properties to be eligible under Criteria D/4, they themselves must be, or must have been, the principal source of the important information, and the information must be considered important. 595 Tunnel Avenue is representative of an industrial property type constructed during the mid-twentieth century. While it reflects an important period of industrial development in early Brisbane, the subject building was constructed of standard materials (wood, metal, and concrete) and with standard methodologies. Furthermore, the construction and manufacturing methodologies of vernacular commercial buildings are well documented. There is no evidence to suggest that 595 Tunnel Avenue would yield significant information that would expand our current knowledge or theories of design, methods of construction, or other information that is not already known. As such, it is recommended ineligible for listing under Criteria D/4.

#### *Integrity Analysis*

In addition to being eligible for listing under at least one of the National Register or California Register criteria, a property must also retain sufficient integrity to convey its historical significance. There are seven aspects to consider when evaluating the integrity of a property: location, design, setting, materials, workmanship, feeling, and association. As discussed above, the commercial building at 595 Tunnel Avenue is not recommended as eligible for listing under any criteria; therefore, a further assessment of integrity is not presented.

### Summary

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

In summary, ESA recommends 595 Tunnel Avenue ineligible for listing in the National Register or California Register under any criteria.

**\*B12. References:** (Continued from page 2)

Ancestry.com. *1900 United States Federal Census* [database on-line]. Year: 1900; Census Place: *Squaw Valley, Siskiyou, California*; Roll: 113; Page: 16; Enumeration District: 0130. Lehi, UT, USA: Ancestry.com Operations Inc, 2004.

Ancestry.com. *1870 United States Federal Census* [database on-line]. Year: 1870; Census Place: *Mendocino, Sonoma, California*; Roll: M593\_91; Page: 294A. Lehi, UT, USA: Ancestry.com Operations, Inc., 2009.

"All Work Stock is Very Scarce." *Lodi News Sentinel*. June 12, 1906.

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County of San Mateo. *About San Mateo County*. Accessed February 15, 2023. <https://www.smcgov.org/about-san-mateo-county>.

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Environmental Science Associates. *Brisbane Baylands Draft EIR*. Prepared for the City of Brisbane. June 2013.

"Mat Harris Passes Away." *Petaluma Argus Courier*. August 15, 1911.

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Oral History Associates, Inc. *Brisbane City of Stars: The First Twenty-Five Years 1961-1986*. 1989

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Public Works Manual. United States, Public Works Journal Corporation. 1993.

Sanborn Map Company. *Insurance Maps of San Francisco, California*. Volume 2. 1899. Sheet 181.

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"To All Boat Builders!" *San Francisco Examiner*. August 23, 1953.

"Weatherwood Tile." *The Times*. June 23, 1939.

"William W. Van Arsdale." *San Bernadino County Sun*. April 2, 1909.

"Untitled." *The California Lumber Merchant*. September 01, 1932. [https://issuu.com/building-products/docs/tclm\\_09011932/s/28767337](https://issuu.com/building-products/docs/tclm_09011932/s/28767337).



State of California — The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary [Abstract]  
HRI #  
Trinomial  
**NRHP Status Code**

Other Listings  
Review Code

Reviewer

Date

Page 1 of 9

**\*Resource Name or #:** 601 Tunnel Avenue

**P1. Other Identifier:** Golden State Lumber and Plywood

**\*P2. Location:** ☐ Not for Publication ☒ Unrestricted

**\*a. County** San Mateo

**\*b. USGS 7.5' Quad** San Francisco South **Date** 1981 Within unsectioned portion of Cañada de Guadalupe land grant

**c. Address** 601 Tunnel Avenue **City** Brisbane **Zip** 94005

**d. UTM: Zone** 10S, 552856.95 mE/ 4173046.25 mN; NAD 83

**e. Other Locational Data:** APN 005-250-020

**\*P3a. Description:** 601 Tunnel Avenue is a one-story light industrial building located on a 5.28-acre lot in an industrial area of the City of Brisbane. The subject property is an active lumber supply site and features multiple modern metal and wood storage structures and clusters of raw cut lumber. The original volume constructed ca. 1956-62 currently functions as a commercial space. It is of wood-frame construction and features an irregular footprint measuring approximately 145 feet long and 120 feet wide. The building is primarily clad with wood board and corrugated metal siding and capped by a double gabled roof covered with asphalt shingles. The primary (south) façade features two partially glazed, metal doors that are recessed beneath an extended eave supported by wood posts. A wood-frame storage structure has been added to the building's southeast corner. It is clad with horizontal wood board siding and capped with a gabled roof covered with composite shingles and a row of skylights. The south façade faces a concrete parking lot partially enclosed by wood and metal storage structures to the east and south (**Figure 1**). The building's north and east façades were not accessible to ESA staff during a June 2024 pedestrian survey. Continued on page 3.

**\*P3b. Resource Attributes:** HP6. 1-3 story commercial building

**\*P4. Resources Present:** ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



**P5b. Description of Photo:** South façade/primary entrance of 601 Tunnel Avenue, view facing northeast. Source: ESA, 2024.

**\*P6. Date Constructed/Age and Source:**

☒ Historic ☐ Prehistoric  
☐ Both

Ca. 1956-1962. Source: Historicaerials.com; R.L. Polk & Co., 1962.

**\*P7. Owner and Address:**

Brisbane Properties  
601 Tunnel Avenue  
Brisbane, CA 94005-1106

**P8. Recorded by:**

Amy Langford, ESA  
550 Kearny Street  
San Francisco, CA 94108

**\*P9. Date Recorded:** June 13, 2024

**\*P10. Survey Type:**

Intensive

**\*P11. Report Citation:** Sims, Cleveland, and Grady, *Baylands Specific Plan, Cultural Resources Technical Report*, prepared by Environmental Science Associates, prepared for Metis Environmental Group, 2023.

**\*Attachments:** ☐ NONE ☐ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record  
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record  
☐ Artifact Record ☐ Photograph Record ☐ Other (List):



## BUILDING, STRUCTURE, AND OBJECT RECORD

\*Resource Name or # 601 Tunnel Avenue \*NRHP Status Code 6Z

Page 2 of 8

B1. Historic Name: unknown

B2. Common Name: Golden State Lumber & Plywood

B3. Original Use: Auto Sales Lot

B4. Present Use: Wholesale lumber warehouse and lot

\*B5. Architectural Style: Utilitarian

\*B6. Construction History: (Construction date, alterations, and date of alterations)

Constructed between 1956 and 1963. For more information, see Table 1 on page 5.

\*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date: N/A Original Location: N/A

\*B8. Related Features: none

B9a. Architect: Unknown

b. Builder: Unknown

\*B10. Significance: Theme Mid-Century Industrial Development Area Brisbane

Period of Significance N/A Property Type Industrial Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

### Brisbane

As the subject property is in the City of Brisbane, which was not incorporated until the 1960s, the early history of the area is best understood from the history of San Mateo County. "San Mateo County was formed in April 1856 out of the southern portion of then-San Francisco County with a population of fewer than 5,000 people and little in the way of commerce." (County of San Mateo, 2023) It is 455 square miles and is bordered by the Pacific Ocean to the west, San Francisco Bay and Santa Clara County to the east, the City and County of San Francisco to the north, and Santa Cruz County to the south (County of San Mateo, 2023). A large portion of the County is occupied by open space including state parks while population centers are primarily located along the two coast lines. Brisbane is located at the northern end of the county between South San Francisco and Millbrae. (Continued on page 3)

B11. Additional Resource Attributes: none

\*B12. References:

See page 8.

B13. Remarks: none

\*B14. Evaluator: Amy Langford, ESA

\*Date of Evaluation: June 2024



\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

\*P3a. Description: (Continued from page 1)

The west façade is dominated by a large addition measuring approximately 50 feet wide and 150 feet long that currently functions as a window and door showroom (**Figure 2**). It is of wood-frame construction, clad with corrugated metal siding and capped with a flat roof. The addition's south elevation features two metal sash, one-over-one windows. A series of metal steps enclosed with a metal railing provides access to a partially glazed metal door. The addition's south elevation features four metal sash, one-over-one windows. Three corrugated metal planter boxes are located beneath the windows. The addition's west elevation features seven metal sash, one-over-one windows and two corrugated metal planter boxes. A partially glazed metal door with a metal accessibility ramp is located at the addition's northwest corner.

An addition along the building's north façade was only partially accessible to ESA staff during a June 2024 pedestrian survey. It is of wood-frame construction, clad with corrugated metal siding, and capped with a flat roof. Typical fenestration consists of metal sash windows. The north addition appears to feature at least three large entryways for the storage of industrial machinery (**Figure 3**).



**Figure 1.** Southeast storage addition (left, northeast view) and wood and metal storage structures (right, south view).



**Figure 2.** West façade addition of 601 Tunnel Avenue, view facing northeast (left) and northwest (right).

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update



Figure 3. North façade addition, view facing southeast.

**\*B10. Significance:** (Continued from page 2)

*Southern Pacific Railroad*

The subject property is located on land that was previously owned and occupied by the Southern Pacific Railroad. The following context is an excerpt from Carey & Company's technical report that was prepared for the Visitacion Valley Redevelopment EIR (Carey & Company, 2008):

The Southern Pacific Railroad Company was one of the most powerful and prominent of American railroad companies in the nineteenth and twentieth centuries. Founded in 1865 by San Francisco businessmen, with the aim to construct a line between San Francisco and San Diego, California, it eventually merged with Central Pacific Railroad Company in 1870, expanding the reach of the western portion of the First Transcontinental Railroad in the United States. In 1904, the Southern Pacific Railroad Company had begun filling in land along the Bay and building their tunnel, still extant, at the corner of Blanken and Tunnel avenues. Southern Pacific provided commuter service to Visitacion Valley on the line called the "Bayshore cutoff," which was known as the most important access route for the San Francisco Peninsula.

The creation of the Cutoff was the product of massive efforts to fill, level and dig through the land in order to connect the burgeoning industrial factory district of the South Bay to the heart of the city and spur development. In 1907, the cutoff was complete, and gave Southern Pacific trains a quick and more direct route into San Francisco. Shop facilities for repairing cars and locomotives were constructed along the Cutoff. A freight yard was established adjacent to the tracks and was noted as the major classification yard for the San Francisco terminal. Freight traffic in the Bay Area experienced a gradual decline in the 1970s, and by 1979, the Bayshore freight yard began moving yard activity to South San Francisco. In 1996, Southern Pacific Railroad Company was succeeded by Union Pacific Railroad. The former lots belonging to Southern Pacific are currently under the ownership of Universal Paragon, and the buildings are now occupied by various commercial organizations.

*Bayshore Boulevard*

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. "According to the 1913-1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse" (Carey & Company, 2008). "The Schlage Lock Factory was well established by this time, with numerous industrial buildings lining the eastern side of Bayshore Boulevard" (Carey & Company, 2008).

*Incorporation*

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. San Francisco to the north Daly City to the northwest and South San Francisco to the south each could have annexed the area. Development pressure was also felt from the Southern Pacific railroad, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a



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subsidiary of the Crocker Estate Company (Oral History Associates, Inc., 1989). Prior to incorporation, the area was under the control of San Mateo County (Oral History Associates, Inc., 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased the threat of annexation as the development would need to be annexed into only one of the surrounding cities in order to provide services such as police, fire, water, and sewer (Oral History Associates, Inc., 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc., 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted to incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc., 1989). The subject property is north of the originally incorporated city.

#### *Crocker Land Company*

The Crocker Land Company, a subsidiary of Crocker Estate Company, owned properties throughout California (Oral History Associates, Inc., 1989). The owner of the Crocker Estate Company was Charles Crocker, founder of Crocker Bank (Oral History Associates, Inc., 1989). "Its San Mateo County holdings included all of the undeveloped property on San Bruno Mountain and a large tract of tidelands on the east side of the Bayshore Freeway" (Oral History Associates, Inc., 1989). "The town of Brisbane was almost completely surrounded by Crocker-owned property" (Oral History Associates, Inc., 1989).

#### *Industrial Development*

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both Southern Pacific Railroad and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History Associates, Inc., 1989). Some of Southern Pacific Railroad's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc., 1989). The subject property is located on what was, in the early 1960s, lands owned by Southern Pacific Railroad (Oral History Associates, Inc., 1989). Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the Southern Pacific ceased operations.

#### **Ownership and Occupancy**

The subject property is developed with a large, light industrial warehouse that appears to have been constructed between 1956 and 1962 (Figure 4). A review of San Francisco city directories indicates that the subject property functioned as a commercial space for an auto sales company owned by Richard Hoegler from 1962 to 1968 (Table 2). City directories do not list any subsequent occupant of the subject property until 1977, upon which the building appears to have been used as a flea market until 1980. Since 1981, the property appears to have been continuously owned and/or occupied by a series of lumber companies. In 1981, a San Francisco city directory lists the occupant of 601 Tunnel Avenue as Sierra Point Lumber & Plywood Company. The Sierra Point Lumber & Plywood Company occupied the subject property until at least 2013 (Figure 5). According to San Mateo County assessor records, 601 Tunnel Avenue was owned by Douglas Galten, the CEO of the Tamalpais Lumber and Milling Company, Inc., between ca. 1986 and 2006 (Bizapedia, 2024).

The subject property is currently occupied by the Golden State Lumber Company. The company, originally named San Marin Lumber Company, was founded by Glen Nobmann in San Rafael in 1959 (goldenstatelumber.com, n.d.; marinlumber.com, n.d.). Nobmann's son, Lee, eventually renamed the business and eventually opened nine locations in the San Francisco Bay Area and Northern California. While preliminary research failed to confirm the beginning of Golden State Lumber's occupancy of 601 Tunnel Avenue, a review of available records noted below (Table 2) indicates that the company became tenants after 2013.

A record of known alterations to the subject property is listed below in Table 1. Known owners and occupants are listed in Table 2.

**Table 1: Property & Building History for 601 Tunnel Avenue**

Date	Alteration / Change	Source
1956	The subject property appears undeveloped in a 1956 aerial photograph.	Historicaerials.com
1963	The subject building appears in a 1963 historic aerial on the eastern portion of the lot.	Cas-sm_1-81 (1963)



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\*Resource Name or # 601 Tunnel Avenue

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

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1982-87	Additions have been constructed along the primary (west) façade and north façade.	Historicaerials.com
1993-2002	Three additional buildings appear at the northwest, south, and southeast portions of the lot.	Historicaerials.com
2014-16	A small structure appears at the northern perimeter of the lot.	Historicaerials.com

**Table 2: Ownership & Use History for 601 Tunnel Avenue**

Year(s) of Occupancy	Owner/Occupant(s)/Business	Notes
1962-1967	Richard Hoegler (unspecified auto sales warehouse) (occupant)	R.L. Polk & Co. (1962-67)
1968	Richard Hoegler, Car Palace Auto (occupant)	R.L. Polk & Co. (1968)
1977-80	Flea Market; Fiesta Fair (occupant)	R.L. Polk & Co. (1977-80)
1981-ca. 2013	Sierra Point Lumber & Plywood Company (occupant)	R.L. Polk & Co. (1981-82); ESA (2013).
Unknown-1986	Sierra Point Ltd. (owner)	San Mateo County Assessor, Document #1986-090498
Ca. 1986-2006	Douglas and Diane Galten (owners)	San Mateo County Assessor, Document #2006-094601
2006-2024	Brisbane Properties (owner)	San Mateo County Assessor, Document #2006-094601
Unknown-2024	Golden State Lumber (occupant)	ESA, 2024.



Source: UC Santa Barbara Library, Framefinder Aerial Imagery, Cas-sm\_1-81, 1:12,000, January 1, 1963, accessed May 24, 2024.

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\*Resource Name or # 601 Tunnel Avenue

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

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Figure 4. 1963 Aerial Photograph of 601 Tunnel Avenue. Subject property outlined in red.



Source: ESA, Brisbane Baylands Draft EIR, prepared for City of Brisbane, June 2013.

Figure 5. Primary façade of 601 Tunnel Avenue, ca. 2007.

## Regulatory Framework

### *National Register of Historic Places*

A property is eligible for listing in the National Register of Historic Places (National Register) if it meets the National Register listing criteria at 36 CFR 60.4, as stated below:

The quality of significance in American history, architecture, archaeology, engineering, and culture is present in districts, sites, buildings, structures, and objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association and that:

- A) Are associated with events that have made a significant contribution to the broad patterns of our history, or
- B) Are associated with the lives of persons significant in our past, or
- C) Embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction, or
- D) Have yielded, or may be likely to yield, information important in prehistory or history

### *California Register of Historical Resources*

To be eligible for the California Register of Historical Resources (California Register) a historical resource must be significant under one or more of the following criteria:

- 1. Is associated with events that have made a significant contribution to the broad patterns of California's history and cultural heritage;
- 2. Is associated with the lives of persons important in our past;
- 3. Embodies the distinctive characteristics of a type, period, region, or method of construction, represents the work of an important creative individual, or possesses high artistic values; or
- 4. Has yielded, or may be likely to yield, information important to prehistory or history.

## Significance Evaluation

The subject property at 601 Tunnel Avenue is evaluated below for potential historic significance according to National Register of Historic Places (National Register) Criteria A through D and California Register of Historical Resources (California Register) Criteria 1 through 4.

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\*Date: June 2024

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*Criteria A/1 – Events*

Research does not indicate that there are any significant associations between the building at 601 Tunnel Avenue and important events or patterns in history. Research indicates that the extant warehouse was constructed between 1956 and 1962 and has continuously functioned as a commercial storage and sales facility since its construction date. As a wholesale auto warehouse and, later, as a lumber sales warehouse, the subject property reflects an important period of economic and commercial growth in Brisbane after the city's incorporation in 1961, but not in an individually significant way. Archival research failed to indicate that the building is significant to the City of Brisbane or is associated with significant trends in local or regional development. For these reasons, 601 Tunnel Avenue is recommended ineligible for individual listing under Criteria A/1.

*Criteria B/2 – People*

Preliminary research failed to uncover significant associations between 601 Tunnel Avenue and significant persons or businesses. The subject property has been occupied by multiple businesses since its construction. However, the building's association with these various occupants does not appear to rise to the level of significance. As research does not indicate that 601 Tunnel Avenue is significantly associated with the productive life of any significant person or business, it is recommended ineligible for individual listing under Criteria B/2.

*Criteria C/3 – Design/Engineering*

The subject property at 601 Tunnel Avenue is not significant for its design or engineering. The property was developed between 1956 and 1962 and is a common example of a light industrial property type constructed during this period. It does not appear to be the work of a master architect. Furthermore, the property appears to have undergone substantial modification since its original construction, including additions to the warehouse's west and north façades. For these reasons, 601 Tunnel Avenue is recommended ineligible for individual listing under Criteria C/3.

*Criteria D/4 – Information Potential*

While most often applied to archaeological districts and sites, Criteria D/4 can also apply to buildings, structures, and objects that contain important information. For these types of properties to be eligible under Criteria D/4, they themselves must be, or must have been, the principal source of the important information, and the information must be considered important. 601 Tunnel Avenue is representative of an industrial property type constructed during the mid-twentieth century. While it reflects an important period of industrial and commercial development in early Brisbane, the subject building was constructed of standard materials (wood, metal, and concrete) and with standard methodologies. Furthermore, the construction and manufacturing methodologies of light industrial buildings are well documented. There is no evidence to suggest that 601 Tunnel Avenue would yield significant information that would expand our current knowledge or theories of design, methods of construction, or other information that is not already known. As such, it is recommended ineligible for listing under Criteria D/4.

*Integrity Analysis*

In addition to being eligible for listing under at least one of the National Register or California Register criteria, a property must also retain sufficient integrity to convey its historical significance. There are seven aspects to consider when evaluating the integrity of a property: location, design, setting, materials, workmanship, feeling, and association. As discussed above, the commercial building at 601 Tunnel Avenue is not recommended as eligible for listing under any criteria; therefore, a further assessment of integrity is not presented.

**Summary**

In summary, ESA recommends 601 Tunnel Avenue ineligible for listing in the National Register or California Register under any criteria.

**\*B12. References:** (Continued from page 2)

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\*Resource Name or # 601 Tunnel Avenue

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

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State of California — The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary [Abstract]  
HRI #  
Trinomial  
**NRHP Status Code**

Other Listings  
Review Code

Reviewer

Date

Page 1 of 11

**\*Resource Name or #:** 950 Tunnel Avenue

**P1. Other Identifier:** Kinder Morgan Brisbane Terminal

**\*P2. Location:** ☐ Not for Publication ☒ Unrestricted

**\*a. County** San Mateo

**\*b. USGS 7.5' Quad** San Francisco South **Date** 1981 Within unsectioned portion of Cañada de Guadalupe land grant

**c. Address** 950 Tunnel Avenue **City** Brisbane **Zip** 94005

**d. UTM:** Zone 10S, 552929.09 mE/ 4171818.44 mN; NAD 83

**e. Other Locational Data:** APN 005-162-370

**\*P3a. Description:** The Kinder Morgan Brisbane Terminal (Terminal) is located at 950 Tunnel Avenue on a 23.48-acre lot located 0.20 miles northwest of the Brisbane Lagoon. The Terminal was inaccessible to ESA staff during a June 2024 intensive survey. The following descriptions are based on observations made from the public right-of-way. The Terminal is situated in a relatively undeveloped portion of the City of Brisbane and bounded by Tunnel Avenue to the east and a segment of the former Southern Pacific railroad to the west. It is accessed via a paved, gated entrance at the west side of Tunnel Avenue and surrounded by metal security fencing. The western portion of the terminal contains 20 refined petroleum products tanks. Designed to store CARB gas, CARB ULSD, turbine, and ethanol, the tanks individually hold a storage capacity ranging from 10,000 to 75,000 barrels. Collectively, the tanks possess a total storage capacity of 618,000 barrels (Kinder Morgan, 2019). (Continued on page 3)

**\*P3b. Resource Attributes:** HP8. Industrial building

**\*P4. Resources Present:** ☐ Building ☐ Structure ☐ Object ☐ Site ☒ District ☐ Element of District ☐ Other (Isolates, etc.)



**P5b. Description of Photo:** Kinder Morgan Terminal gated entrance, view facing west. ESA, 2024.

**\*P6. Date Constructed/Age and Source:**

☒ Historic ☐ Prehistoric

☐ Both

Ca. 1969. Source: Southern Pacific Railroad History Center, n.d.

**\*P7. Owner and Address:**

SFPP

1100 West Town and Country Road  
Orange, CA 92868-4600

**P8. Recorded by:**

Amy Langford, ESA

550 Kearny Street

San Francisco, CA 94108

**\*P9. Date Recorded:** June 13, 2024

**\*P10. Survey Type:**

Intensive

**\*P11. Report Citation:** Sims, Cleveland, and Grady, *Baylands Specific Plan, Cultural Resources Technical Report*, prepared by Environmental Science Associates, prepared for Metis Environmental Group, 2023.

**\*Attachments:** ☐ NONE ☐ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record  
☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record  
☐ Artifact Record ☐ Photograph Record ☐ Other (List):

## BUILDING, STRUCTURE, AND OBJECT RECORD

\*Resource Name or # 950 Tunnel Avenue \*NRHP Status Code 6Z

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- B1. Historic Name: Southern Pacific Tank Farm/Brisbane Tank Farm  
B2. Common Name: Kinder Morgan Brisbane Terminal  
B3. Original Use: petroleum pipeline terminal B4. Present Use: petroleum pipeline terminal  
\*B5. Architectural Style: Utilitarian  
\*B6. Construction History: (Construction date, alterations, and date of alterations)  
Constructed ca. 1969. For more information, see Table 1 on page 6.

\*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date: N/A Original Location: N/A

\*B8. Related Features: none

B9a. Architect: Unknown b. Builder: Unknown  
\*B10. Significance: Theme Mid-Century Industrial Development Area Brisbane  
Period of Significance N/A Property Type Industrial Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

### Brisbane

As the subject property is in the City of Brisbane, which was not incorporated until the 1960s, the early history of the area is best understood from the history of San Mateo County. "San Mateo County was formed in April 1856 out of the southern portion of then-San Francisco County with a population of fewer than 5,000 people and little in the way of commerce." (County of San Mateo, 2023) It is 455 square miles and is bordered by the Pacific Ocean to the west, San Francisco Bay and Santa Clara County to the east, the City and County of San Francisco to the north, and Santa Cruz County to the south (County of San Mateo, 2023). A large portion of the County is occupied by open space including state parks while population centers are primarily located along the two coast lines. Brisbane is located at the northern end of the county between South San Francisco and Millbrae. (Continued on page 4)

B11. Additional Resource Attributes: none

\*B12. References:

See page 11.

B13. Remarks: none

\*B14. Evaluator: Amy Langford, ESA

\*Date of Evaluation: June 2024





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\*Date: June 2024

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\*P3a. Description: (Continued from page 1)

The eastern portion of the Terminal is comprised of a cluster of light industrial buildings, office buildings, sheds, and truck loading stations. The northeast end of the terminal features three components: a steel and concrete tower structure, a metal truck loading station capped with a flat roof with a corrugated metal overhang (**Figure 1**), and a one-story ancillary building (**Figure 2**). The ancillary modular building is of wood-frame construction and features a roughly rectangular plan. It is clad with corrugated metal siding and capped by a low-pitched gable roof covered with corrugated metal. From south to north, the building's primary (east) façade features a boarded-up window; a partially glazed, wood pedestrian door that has been partially boarded up; a second boarded-up window; and a roll-up, machinery door. The building's north and south façades are devoid of fenestration. The building's west façade was not visible from the public right-of-way.



**Figure 1.** Northeast loading station and tower, view facing northwest.



**Figure 2.** Northeast ancillary building, view facing southwest.

Two ancillary buildings are located directly west of the Terminal's entrance gate on Tunnel Avenue. Situated slightly to the northeast is a one-story modular building. The building features a rectangular footprint, is clad in corrugated metal siding, and capped with a flat corrugated metal roof. The primary (east) façade features two flush, raised pedestrian doors that are accessed by metal steps supported by metal railing. Typical fenestration consists of metal sash, sliding windows (**Figure 3**). To the south of the modular building is what appears to be an office or sales building. It is of wood frame construction and features a rectangular footprint. The building is clad with stucco siding and capped with a shed roof. The building's primary (east) façade is set beneath a roof overhang. It features a roll-up, metal, vehicular entrance and a partially glazed, pedestrian entrance. Typical fenestration consists of metal-frame, one-over-one windows (**Figure 3**).



**Figure 3.** Office building and modular ancillary building, view facing west.

A third cluster of ancillary buildings and structures is located at the southeast end of the Terminal. It includes a metal truck loading station capped with a flat roof with a corrugated metal overhang. To the east of the truck loading station is a one-story

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light industrial building. It is of wood-frame construction, features an irregular footprint, clad with corrugated metal siding, and capped with an offset, corrugated metal shed roof. The north façade features two roll-up, metal, vehicular entrances. The building's east façade features three pairs of metal-frame windows set beneath a steel and corrugated metal overhang (**Figure 4**). The building's south façade features a third roll-up, metal, vehicular entrance. An addition clad with corrugated metal siding is attached to the building's west façade. A row of metal-frame windows wraps around the addition's north and west elevations. Two small windows are visible on the addition's south façade (**Figure 5**).



**Figure 4.** Southeast portion of the Terminal, view facing southwest.



**Figure 5.** Southeast portion of the Terminal, view facing northwest.

**\*B10. Significance:** (Continued from page 2)

#### *Southern Pacific Railroad*

The subject property is located on land that was previously owned and occupied by the Southern Pacific Railroad. The following context is an excerpt from Carey & Company's technical report that was prepared for the Visitacion Valley Redevelopment EIR (Carey & Company, 2008):

The Southern Pacific Railroad Company was one of the most powerful and prominent of American railroad companies in the nineteenth and twentieth centuries. Founded in 1865 by San Francisco businessmen, with the aim to construct a line between San Francisco and San Diego, California, it eventually merged with Central Pacific Railroad Company in 1870, expanding the reach of the western portion of the First Transcontinental Railroad in the United States. In 1904, the Southern Pacific Railroad Company had begun filling in land along the Bay and building their tunnel, still extant, at the corner of Blanken and Tunnel avenues. Southern Pacific provided commuter service to Visitacion Valley on the line called the "Bayshore cutoff," which was known as the most important access route for the San Francisco Peninsula.

The creation of the Cutoff was the product of massive efforts to fill, level and dig through the land in order to connect the burgeoning industrial factory district of the South Bay to the heart of the city and spur development. In 1907, the cutoff was complete, and gave Southern Pacific trains a quick and more direct route into San Francisco. Shop facilities for repairing cars and locomotives were constructed along the Cutoff. A freight yard was established adjacent to the tracks and was noted as the major classification yard for the San Francisco terminal. Freight traffic in the Bay Area experienced a gradual decline in the 1970s, and by 1979, the Bayshore freight yard began moving yard activity to South San Francisco. In 1996, Southern Pacific Railroad Company was succeeded by Union Pacific Railroad. The former lots belonging to Southern Pacific are currently under the ownership of Universal Paragon, and the buildings are now occupied by various commercial organizations.

#### *Bayshore Boulevard*

Originally known as the San Bruno Toll Road, Bayshore Boulevard was developed by 1860. "According to the 1913-1915 Sanborn map, development along Bayshore Boulevard (then called San Bruno Avenue) between Arleta Avenue [, just north of the subject property,] and the [San Francisco – San Mateo] county line to the south was sparse" (Carey



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& Company, 2008). "The Schlage Lock Factory was well established by this time, with numerous industrial buildings lining the eastern side of Bayshore Boulevard" (Carey & Company, 2008).

#### *Incorporation*

As development and annexation pressures from the surrounding communities increased over the 1950s, the citizens of the community considered incorporation. San Francisco to the north Daly City to the northwest and South San Francisco to the south each could have annexed the area. Development pressure was also felt from the Southern Pacific railroad, which occupied 600 acres of land to the east of the town, and the Crocker Land Company, a subsidiary of the Crocker Estate Company (Oral History Associates, Inc., 1989). Prior to incorporation, the area was under the control of San Mateo County (Oral History Associates, Inc., 1989). Crocker Land Company announced the future development of an industrial park in the Guadalupe Valley, north of Brisbane, in the early 1950s that increased the threat of annexation as the development would need to be annexed into only one of the surrounding cities in order to provide services such as police, fire, water, and sewer (Oral History Associates, Inc., 1989). This announcement led to the formation of the Brisbane Citizens' Committee in the summer of 1953; this group of residents advocated for the incorporation of Brisbane (Oral History Associates, Inc., 1989). The first vote for incorporation failed, however, in 1961 the citizens of Brisbane voted to incorporate a 2.5-square-mile area that would have a city manager form of government (Oral History Associates, Inc., 1989). The subject property is north of the originally incorporated city.

#### *Crocker Land Company*

The Crocker Land Company, a subsidiary of Crocker Estate Company, owned properties throughout California (Oral History Associates, Inc., 1989). The owner of the Crocker Estate Company was Charles Crocker, founder of Crocker Bank (Oral History Associates, Inc., 1989). "Its San Mateo County holdings included all of the undeveloped property on San Bruno Mountain and a large tract of tidelands on the east side of the Bayshore Freeway" (Oral History Associates, Inc., 1989). "The town of Brisbane was almost completely surrounded by Crocker-owned property" (Oral History Associates, Inc., 1989).

#### *Industrial Development*

In the 1960s, due to its small population, the city focused on developing an industrial tax base to fund the newly incorporated city. This led the city manager to look east of Bayshore Boulevard. In December of 1952, both Southern Pacific Railroad and PG&E, whose lands totaled 700 acres, requested annexation into Brisbane (Oral History Associates, Inc., 1989). Some of Southern Pacific Railroad's lands extended into the Bay including 8,350 acres of tidelands and Bay waters (Oral History Associates, Inc., 1989). The subject property is located on what was, in the early 1960s, lands owned by Southern Pacific Railroad (Oral History Associates, Inc., 1989). Industrial Way is dominated by light industrial uses such as automobile service shops, hauling service companies, and suppliers of various goods. The area appears to have been redeveloped, primarily in the 1960s and 1970s, after the Southern Pacific ceased operations.

#### **Ownership and Occupancy**

The subject property is in a commercial and industrial neighborhood northeast of downtown Brisbane. It is developed with a large, multi-component petroleum tank farm constructed ca. 1969, which appears to have functioned as a petroleum pipeline terminal since its date of construction. A review of historic aerials and topographic maps indicates that the subject property was initially comprised of a large cluster of approximately 17 industrial tanks and 6 ancillary buildings and structures. Several tanks, ancillary structures, sheds, and outbuildings had been added to the property between 1973 and 1980 (**Figures 8 and 9**). The subject property appears to have been largely unchanged from its ca. 1970s spatial configuration, aside from the removal of several small ancillary buildings and the addition of a large, rectangular structure to the southern portion of the subject property (**Table 1**).

#### *Southern Pacific Tank Farm*

The subject property at 950 Tunnel Avenue originally functioned as a pipeline terminal facility for the Southern Pacific Pipe Line, Inc.'s San Francisco Bay Area pipeline system. In 1955, the Southern Pacific Railroad became the first American railroad company to own and operate a major refined products pipeline system (Southern Pacific Railroad History Center, n.d.). By the 1950s, the dramatic decline of passenger rail transportation had coincided with a rapidly growing demand for the transportation of crude and petroleum products across the United States. To address the expanding need for domestic petroleum transportation, the Southern Pacific Railroad began transporting petroleum-laden tank cars on along its transcontinental rail lines between Arizona's Tucson-Phoenix region and California's Imperial Valley. With its existing intercontinental rail system located along the route of what the Stanford Research Institute projected would serve as a cost-effective and expedient future pipeline system, Southern Pacific constructed an initial pipeline system by the end of 1955.

The original system operated by the newly devised Southern Pacific Pipe Line company comprised two pipelines. The first connected Los Angeles oil refineries with their counterparts in Phoenix, Arizona, and included intermediate delivery terminals

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at Niland and Colton, California. The second linked El Paso refineries with the Phoenix and Tucson areas (Southern Pacific Railroad History Center, n.d.). The original Los Angeles to Phoenix system began at the originating pump station at Watson, California. From Watson, the pipeline travels through the Norwalk, California, segment (constructed 1972), La Habra (near Ontario, California), Colton, and Niland, California, before passing through Yuma, Arizona, and reaching its terminus near Phoenix, Arizona (Southern Pacific Railroad History Center, n.d.). The original system expanded over the subsequent decades, including the construction of a 779-mile pipeline between Richmond/Concord, California, and Reno, Nevada. In 1969, the company completed the construction of a new system in the San Francisco Bay Area to serve Oakland International Airport, San Francisco International Airport, Brisbane, and Bayshore Yard. By the end of 1975, Southern Pacific Pipe Line, Inc., operated nearly 2,500 miles of product pipeline, 21 pump or booster stations, 20 terminal facilities, and 9 additional delivery facilities (Southern Pacific Railroad History Center, n.d.).

Brisbane's Southern Pacific Tank Farm was one of the 20 Southern Pacific Pipe Line terminal facilities constructed during the late 1960s and early 1970s. In February 1968, Southern Pacific proposed plans for a new tank farm to the San Mateo County Planning Commission. At the time, an assistant manager within the Southern Pacific real estate department stated, "Southern Pacific's wholly owned subsidiary, Southern Pacific Company Pipe Lines, Inc., has now given the word to proceed without delay with the development of a pipe line (sic) terminal at Brisbane" (*Brisbane Bee Democrat*, February 15, 1968). With a projected development cost in excess of \$3,000,000, the proposed tank farm was accompanied with the promise that several oil companies would relocate their offices to Brisbane and further accelerate the economic development of the newly incorporated city (*Brisbane Bee Democrat*, February 15, 1968). The new Brisbane tank farm (**Figure 7**) was designed to "duplicate" a preexisting Southern Pacific Pipeline Tank Farm located in nearby San Jose and would augment the Bay Area's petroleum storage and transportation capacity (*Brisbane Bee Democrat*, March 14, 1968). Construction of the Brisbane Southern Pacific Pipeline Tank Farm was completed in 1969.

Preliminary research indicates that the subject property has continuously functioned as a pipeline terminal since its date of construction. Local newspapers continuously referred to the subject property as the "Southern Pacific tank farm" (*Brisbane Bee Democrat*, September 23, 1976) and Southern Pacific retained ownership of the property until ca. 1997 (**Table 2**). In 1983, the Southern Pacific Company (owner of the Southern Pacific Railroad) merged with Santa Fe industries (owner of the Atchison, Topeka and Santa Fe Railroads), forming the Santa Fe Pacific Corporation (*New York Times*, September 28, 1983). Following the merger, Southern Pacific Pipe Line, Inc., was acquired by Santa Fe Pacific Pipelines, Inc (SFPP) (ESA, 1998). In 1997, SFPP was purchased by Kinder Morgan Energy Partners L.P. (*Wall Street Journal*, October 21, 1997). Established in 1997, Kinder Morgan has since become one of the largest energy infrastructure companies in North America (Kinder Morgan, n.d.). Assessor Records indicate that the property was briefly owned by the property management company, Sunquest Properties, Inc., before being purchased by Kinder Morgan subsidiary SFPP in 2003. The subject property, now commonly known as the Kinder Morgan Brisbane Terminal, is one component of the Kinder Morgan Pacific Operations Northern Region's Bay Area Line. The Bay Area Line comprises two origin sites (Richmond and Amoroco Stations) and three destination terminals (Brisbane, Oakland Airport, and San Francisco Airport) (Kinder Morgan, 2019).

A record of known alterations to the subject property is listed below in **Table 1**. Known owners and occupants are listed in **Table 2**.

**Table 1: Property & Building History for 950 Tunnel Avenue**

Date	Alteration / Change	Source
1956	A historic aerial photograph shows evidence of grading on the subject property. It is bounded by the Southern Pacific Railroad to the west and Tunnel Avenue to the north, east, and south.	Historicaerials.com
1965	A historic aerial photograph shows no further development on the subject property. The portion of the San Francisco Bay previously bounded by Tunnel Avenue and the Bayshore Freeway has been dredged and graded for development.	Cas-65-130 1-82 (May 1, 1965) ( <b>Figure 6</b> )
1969	Southern Pacific Pipe Lines, Inc., completes construction of its San Francisco Bay Area pipeline system, including the Southern Pacific Tank Farm/Brisbane Tank Farm.	Southern Pacific Railroad History Center (n.d.)
1973	The Southern Pacific Tank Farm first appears on a 1973 USGS topographic map, which depicts a cluster of approximately 17 petroleum tanks and 6 ancillary buildings or structures.	Sage, 2021 ( <b>Figure 8</b> ).
1982	A historic aerial photograph shows several smaller structures at the north end of the subject property are no longer present.	Historicaerials.com



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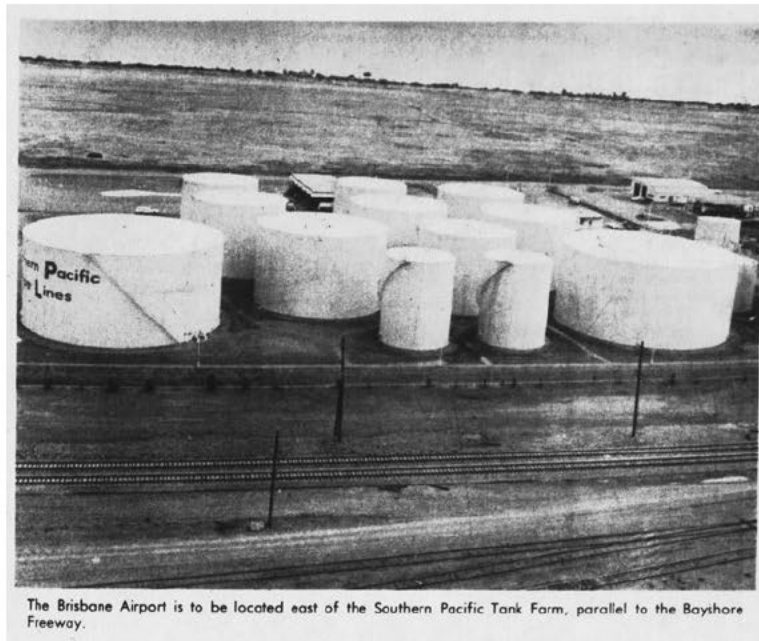
1984	A historic aerial photograph indicates that the Southern Pacific Tank Farm has expanded, constructing several additional industrial tanks and clusters of small ancillary structures, sheds, and outbuildings on the southern and easter portions of the subject property.	WAC_84C (March 17, 1980) (Figure 9).
1987-2016	Historic aerial photographs from 1987, 1993, 2002, 2005, 2009, 2010, 2012, 2014, and 2016 depict a spatial organization of the property consistent with the 1982 aerial photograph.	Historicaerials.com
2018	A historic aerial photograph indicates that a long, rectangular structure has been constructed on the southern portion of the subject property.	Historicaerials.com

**Table 2: Ownership & Use History for 950 Tunnel Avenue**

Year(s) of Occupancy	Owner/Occupant(s)/Business	Source
Ca. 1969-1997	Southern Pacific Transportation Company/Southern Pacific Pipeline, Inc. (owner)	San Mateo County Assessor, Document # 1997-042118; Southern Pacific Railroad History Center (n.d.)
1997-2003	Sunquest Properties, Inc. (owner)	San Mateo County Assessor, Document # 1997-042118
2003-Present	SFPP Limited Partnership (owner)	San Mateo County Assessor, Document #2003-183643

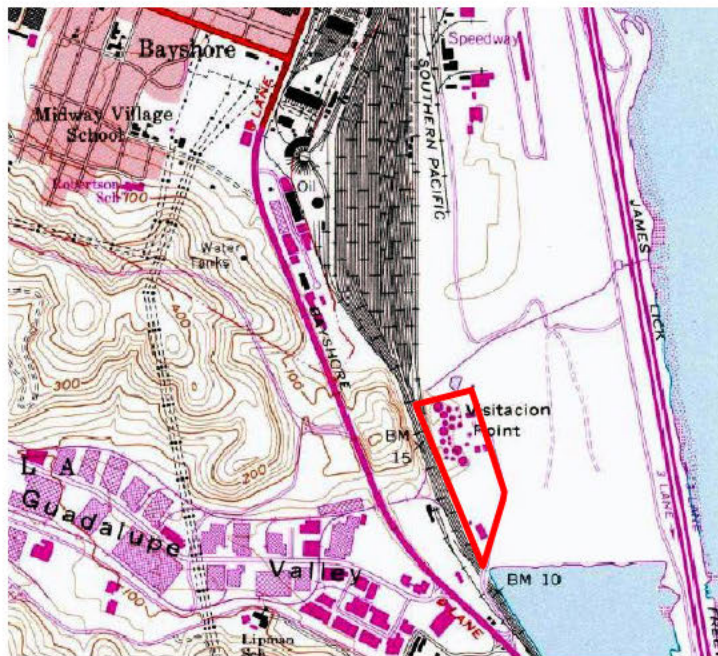


Source: UC Santa Barbara Library, Framefinder Aerial Imagery, Cas-65-130\_1-82, 1:12,000, May 1, 1965, accessed May 24, 2024.  
**Figure 6.** 1965 Aerial Photograph of 950 Tunnel Avenue. Subject property outlined in red.



Source: *Brisbane Bee*, September 28, 1972.

Figure 7. Aerial view of the Southern Pacific Tank Farm, 1972.



Source: Bob Sage, *Brisbane, CA: A Cartographic Journey Through Time*, 2021.

Figure 8. The subject property (outlined in red) is first depicted on a USGS topographic map in 1973.



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Source: UC Santa Barbara Library, Framefinder Aerial Imagery, WAC\_84C, Frame 6-195, 1:31,680, March 17, 1984, accessed May 24, 2024.

**Figure 9.** 1984 Aerial Photograph of 950 Tunnel Avenue depicts similar spatial configuration to current conditions. Subject property outlined in red.

## Regulatory Framework

### *National Register of Historic Places*

A property is eligible for listing in the National Register of Historic Places (National Register) if it meets the National Register listing criteria at 36 CFR 60.4, as stated below:

The quality of significance in American history, architecture, archaeology, engineering, and culture is present in districts, sites, buildings, structures, and objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association and that:

- A) Are associated with events that have made a significant contribution to the broad patterns of our history, or
- B) Are associated with the lives of persons significant in our past, or
- C) Embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction, or
- D) Have yielded, or may be likely to yield, information important in prehistory or history

### *California Register of Historical Resources*

To be eligible for the California Register of Historical Resources (California Register) a historical resource must be significant under one or more of the following criteria:

- 1. Is associated with events that have made a significant contribution to the broad patterns of California's history and cultural heritage;
- 2. Is associated with the lives of persons important in our past;
- 3. Embodies the distinctive characteristics of a type, period, region, or method of construction, represents the work of an important creative individual, or possesses high artistic values; or
- 4. Has yielded, or may be likely to yield, information important to prehistory or history.

## Significance Evaluation

The Kinder Morgan Brisbane Terminal at 950 Tunnel Avenue is evaluated below for potential historic significance according to National Register of Historic Places (National Register) Criteria A through D and California Register of Historical Resources (California Register) Criteria 1 through 4. Because access to the property was limited to the public right-of-way along Tunnel

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\*Date: June 2024

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Avenue, an assessment of remaining material integrity was not possible. Therefore, a survey of the extent of any remaining elements and their conditions has not been completed. The following is based on the historical record and comparative analysis of extant regional terminals owned and operated by Kinder Morgan. It should be considered preliminary until such time that access to the site can be arranged to confirm these preliminary recommendations.

#### *Criteria A/1 – Events*

Preliminary research does not indicate that there are any significant associations between the Kinder Morgan Brisbane Terminal and important events or patterns in history. When it was constructed ca. 1969, the subject property served as one of 20 terminal facilities owned and operated by Southern Pacific Pipe Line, Inc. At the time, the “Southern Pacific Tank Farm” was one component of the Southern Pacific Railroad’s first foray into the domestic transportation of refined products between Arizona and California. By the 1970s, the Southern Pacific pipeline system was comprised of approximately 2,500 miles of product pipeline and 50 associated stations and facilities. As such, the Brisbane Terminal has historically been a key component of a larger domestic petroleum product transportation infrastructure managed by Southern Pacific Railroad. While the Terminal is associated with patterns of petroleum pipeline development and regional industrial development, it is not significantly associated with events or thematic activities that have made a significant contribution to the broader patterns of local or regional history, nor the cultural heritage of California or the United States. For these reasons, 950 Tunnel Avenue is recommended ineligible for individual listing under Criteria A/1.

#### *Criteria B/2 – People*

The Kinder Morgan Brisbane Terminal does not appear to possess significant associations with significant persons in the past. The Terminal functioned as a petroleum pipeline terminal for Southern Pacific Pipe Line, Inc., then Santa Fe Pacific Pipelines, Inc., which became a subsidiary of Kinder Morgan in 1997. Since its construction, numerous people have been employed at the Terminal over the years. Preliminary archival research does not indicate that the Terminal is significantly associated with any people who were/are important to local, California, or national history. As research does not indicate that 950 Tunnel Avenue is significantly associated with the productive life of any significant person, it is recommended ineligible for individual listing under Criteria B/2.

#### *Criteria C/3 – Design/Engineering*

The Kinder Morgan Brisbane Terminal does not appear to be significant for its design or engineering. The Terminal was originally designed as a petroleum pipeline terminal for the Southern Pacific Pipe Line, Inc. Constructed ca. 1969, the facility was designed in a vernacular utilitarian style. As a unit, the Terminal does not exemplify a distinctive architectural style or embody the distinctive characteristics of a type, period, region, or method of construction. As a pipeline terminal constructed during the late 1960s, the property appears to be a common example of a utilitarian property type constructed during this period and possesses a similar spatial configuration and assemblage of structures and buildings present at other sites within Kinder Morgan’s Pacific Operations (Kinder Morgan, 2019). Furthermore, the Terminal does not appear to be the work of a master architect. Preliminary research did not identify an architect or architectural firm responsible for the design of the original tank farm, and it is likely that staff architects or engineers for Southern Pacific were involved in its construction. While the Terminal reflected the contemporary engineering and technological capabilities present during the late 1960s, research does not indicate that the Terminal possesses or represents any engineering innovation that rises to the level of significance. Finally, the design of the Terminal does not articulate a particular concept of design so fully that it expresses an aesthetic ideal, and it therefore does not possess high artistic value. For these reasons, 950 Tunnel Avenue is recommended ineligible for individual listing under Criteria C/3.

#### *Criteria D/4 – Information Potential*

While most often applied to archaeological districts and sites, Criteria D/4 can also apply to buildings, structures, and objects that contain important information. For these types of properties to be eligible under Criteria D/4, they themselves must be, or must have been, the principal source of the important information, and the information must be considered important. 950 Tunnel Avenue is representative of an industrial property type constructed during the mid-twentieth century. While it reflects an important period of industrial development in early Brisbane, the subject building was constructed of standard materials (wood, metal, and concrete) and with standard methodologies. Furthermore, the construction and manufacturing methodologies of vernacular commercial buildings are well documented. There is no evidence to suggest that 950 Tunnel Avenue would yield significant information that would expand our current knowledge or theories of design, methods of construction, or other information that is not already known. As such, it is recommended ineligible for listing under Criteria D/4.

#### *Integrity Analysis*

In addition to being eligible for listing under at least one of the National Register or California Register criteria, a property must also retain sufficient integrity to convey its historical significance. There are seven aspects to consider when evaluating the integrity of a property: location, design, setting, materials, workmanship, feeling, and association. As discussed above, the

\*Recorded by: Amy Langford, ESA

\*Date: June 2024

☒ Continuation

☐ Update

commercial building at 950 Tunnel Avenue is not recommended as eligible for listing under any criteria; therefore, a further assessment of integrity is not presented.

### Summary

In summary, ESA recommends 950 Tunnel Avenue ineligible for listing in the National Register or California Register under any criteria.

### \*B12. References: (Continued from page 2)

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State of California – The Resources Agency  
DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary # P-41-002720  
HRI # \_\_\_\_\_  
Trinomial \_\_\_\_\_  
NRHP Status Code   Z    
Other Listings \_\_\_\_\_  
Review Code \_\_\_\_\_ Reviewer \_\_\_\_\_ Date \_\_\_\_\_

Page 1 of 1

\*Resource Name or # (Assigned by recorder) Map Reference #

**P1. Other Identifier:** Pacific Fruit Express Ice Mechanization Plant

\*P2. Location: ☐ Not for Publication ☒ Unrestricted

\*a. County San Mateo

and (P2b and P2c or P2d. Attach a Location Map as necessary.)

\*b. USGS 7.5' Quad San Francisco Date 9 6 h 0 revised 0 T    ; R    ;    ¼ of Sec    ;    B.M.

c. Address Bayshore Blvd City Brisbane Zip 94004

d. UTM: (give more than one for large and/or linear resources) Zone    mE/    mN

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, etc., as appropriate)

APN: 005-000

\*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

The 3-acre parcel at 3401 Bayshore Boulevard is located west of the Caltrain / Union Pacific Railroad line, south of Bayshore / Visitacion Yards, and east of the San Bruno Main in the Bayland area of Brisbane. There are three buildings and one trailer located on the parcel and the property currently serves as the Machinery and Equipment Company. The building is described in detail on the Chain of Title Sheet.

\*P3b. Resource Attributes: (List attributes and codes) (HP8) Industrial Building

\*P4. Resources Present: ☒ Building ☐ Structure ☐ Object ☐ Site ☐ District ☐ Element of District ☐ Other (Isolates, etc.)



P5b. Description of Photo: (View, date, accession #) July 06 camera facim rthwest, P h og ahl

\*P6. Date Constructed/Age and Sources:

☒ Historic ☐ Prehistoric ☐ Both

9 Th p n et. al, Pacific Fruit Express Company

\*P7. Owner and Address:

Brisbane Mach & Equipm et  
Bayshore Blvd  
Brisbane, CA 94004

\*P8. Recorded by: (Name, affiliation, address)

C. McMorris, S. Klima, J. Cheney  
JRP Historical Consulting,  
1000rew Ave, Suite 100  
Dayton, CA 95404

\*P9. Date Recorded: July 06

\*P10. Survey Type: (Describe)

Inventory

\*P11. Report Citation: (Cite survey report and other sources, or enter "none.") JRP Historical Consulting "Historic Properties Inventory and Evaluation Report, Peninsula Corridor Joint Powers Board South San Francisco Station and Track Work Project, Brisbane and South San Francisco, San Mateo County, California, Caltrain Mile Posts: 06.10 to 10.6," May 2006.

\*Attachments: ☐ None ☐ Location Map ☐ Sketch Map ☒ Continuation Sheet ☒ Building, Structure, and Object Record ☐ Archaeological Record ☐ District Record ☐ Linear Feature Record ☐ Milling Station Record ☐ Rock Art Record ☐ Artifact Record ☐ Photograph Record

☐ Other (list)   

DPR 523A (1/95)

\*Required Information



Page 2 of 2

\*NRHP Status Code 6Z

\*Resource Name or # (Assigned by recorder) Map Reference # 1

B1. Historic Name: Pacific Fruit Express Ice Mechanization Plant

B2. Common Name: Machinery & Equipment Company

B3. Original Use: Ice Manufacturer B4. Present Use: Warehouse

\*B5. Architectural Style: Industrial/Commercial

\*B6. Construction History: (Construction date, alteration, and date of alterations) Built 1901; removed 1960; relocated 1961; removed 1962; blacksmith building; removed 1963; garage, 1964; construction of office building approximately 1965; building of office building 1966

\*B7. Moved? ☒ No ☐ Yes ☐ Unknown Date: Original Location:

\*B8. Related Features:

B9. Architect: Unknown b. Builder: Southern Pacific Railroad / Pacific Fruit Express

\*B10. Significance: Theme n/a Area n/a

Period of Significance n/a Property Type n/a Applicable Criteria n/a

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The former Pacific Fruit Express Ice Mechanization Plant property at 3401 Bayshore Boulevard does not appear to meet the criteria for listing in the National Register of Historic Places (NRHP) and the California Register of Historic Resources (CRHR). (See Criteria Sheet)

B11. Additional Resource Attributes: (List attributes and codes) \_\_\_\_\_

\*B12. References: Antiquities in Perspective J. Chubb and Robert J. Chubb, *Pacific Fruit Express Company*, (Berkeley: Signet Press, 1961); *Southern Pacific Company Station Plans Bayshore Visitation, San Francisco and Marin Counties*, Coast Division February 1961, revised August 1961; December 1961; *San Francisco Mercury*, *The Southern Pacific, 1901-1985*; and see footnotes in B10 Significance.

B13. Remarks:

\*B14. Evaluator:

Julia Chaney Amand Blaser/Christy McMorris

\*Date of Evaluation: October 1988

(This space reserved for official comments.)



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\*Resource Name or # (Assigned by recorder) Map Reference # 1

\*Recorded by C. McMorris / J. Cheney / S. Klima \*Date July 9 8 ☒ Continuation ☐ Update

### P3a. Description (continued):

The main building on this property was constructed in 1924. It is located at the north corner of the parcel. It is known as the “ice house.” It is a one to two-story warehouse that rests on a concrete slab foundation and appears to have a structural system composed of a combination of concrete piers and steel I-beams with hollow clay tile and brick infilled walls. The building has a tower element at the north end, a shorter middle section, and a long extension at the south end, as shown in **Photograph 1** and **Photograph 2**. The building has a tall, unadorned concrete base above which the main stylistic elements of the building are the concrete piers which extend the height of the building and are arranged approximately every 15 feet and which project 18 inches from the façade. Each pier is faced with brick and ornamented with colored, geometric vertical brick patterns at the top. A masonry and concrete stringcourse divide each story of the building and a buff-colored brick course functions as a cornice on the building. Two metal rolling doors are located on the east side of the tower element. The south end of the ice house is the one-story flat roof wing on which the building’s architectural detail, such as the brick ornament, brick-clad piers, and stringcourses provide are continued. An open steel frame shed, with a corrugated metal roof, extends the length of the east side of the south end of the building, as shown in **Photograph 3**. A large wood sign is attached by a metal frame above the corrugated metal roof. Fenestration at the south end of the building, shown in **Photograph 4**, consists of industrial steel sash windows, each a combination awning sashes flanked by fixed pane sashes. Entrances to this building include top-hinged sliding metal doors. A portion of this wing has been enclosed with plywood to form a small storage room.

This building was originally used as an ice house owned by Southern Pacific Railroad subsidiary Pacific Fruit Express to manufacture and store ice that was then loaded onto freight cars carrying produce or meat. The large two-story tower was the “winter storage room,” with its metal truss / timber roof and cork walls, shown in **Photograph 5**, and the adjacent one-story wing was used for “day storage.” The long wing on the south end of the building (**Photograph 4**) was originally the tank and compressor room. The parcel also at one time contained a separate condenser building and a blacksmith shop, but those buildings have been removed. There was also a small garage at the rear of the two-story ice house, which has also been removed.<sup>1</sup> A 10-freight car length concrete deck a spur line on each side of it that was used to load and unload ice from trains originally fronted the building. The concrete deck has been removed as have some of the spur lines, although the abandoned spur is still visible in portions of the parcel, as shown in **Photograph 6**.

To the south of this building is a two-story office building that was constructed in the early 1960s, shown in **Photograph 7**. It has a one-story addition that was constructed in 1966 at its north side. The building is sided with stucco and topped with a mansard-style roof. The roof is sheathed with wood shakes and has a dental molding cornice along the roofline. Fenestration throughout this modern building consists of aluminum-sash sliding windows on the second story and multi-light, wood frame awning windows on the bottom floor. The main entrance of the office building is on the east façade of the one-story addition and contains double leaf glazed doors flanked by sidelights. The main entry is sheltered by an overhang and is accessed by a set of three steps.

A shed is also located on south of the warehouse and office buildings. The small rectangular shaped building, shown in **Photograph 8**, rest on a concrete foundation and is topped with a front gable roof sheathed with wood shakes. The walls are clad with horizontal wood siding. A metal door serves as its only entrance. The parcel also contains a temporary trailer/office, shown in **Photograph 9**, that is surrounded by machinery and equipment.

<sup>1</sup> Southern Pacific Company, “Station Plan of Bayshore Visitacion, San Francisco and San Mateo Counties, Coast Division,” February 1923 revised through December 1930 [available at the California State Railroad Museum, Sacramento]; and *Station Map, Visitacion San Francisco and San Mateo Counties California* (San Francisco: Valuation Department Southern Pacific Company, no date).

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\*Resource Name or # (Assigned by recorder) Map Reference # 1

\*Recorded by C. McMorris / J. Cheney / S. Klima \*Date July 9 8 ☒ Continuation ☐ Update

## **B10. Significance (continued):**

### Historic Context

The former Pacific Fruit Express Ice Mechanization Plant at 3401 Bayshore Boulevard is located in Brisbane which was originally part of the 9,500 acre Rancho Canada de Guadalupe la Visitacion y Rodeo Viejo that Jacob Primer Leese acquired in the 1830s. Charles Crocker, one of the “Big Four” Southern Pacific Railroad tycoons of California, purchased 3,000 acres of the former Rancho Canada de Guadalupe la Visitacion y Rodeo Viejo in 1884 and called it Visitacion Ranch. The area that became the City of Brisbane was originally developed as dairies, poultry farms, and ranch lands in the late nineteenth century.

The economic development of what was to become the cities of Brisbane and South San Francisco gradually shifted from agriculture and ranching to a more industrialized-based economy during the turn of the twentieth century. In the wake of the 1906 earthquake, land speculators looked to the Guadalupe Valley as a possible residential development site for San Franciscans. The construction of the Southern Pacific Railroad’s Bayshore Cutoff in 1907 made the route from San Francisco to the part of the peninsula easier and attracted settlers to Brisbane. Only a small amount of residential construction actually took place, though, between 1908 and the late 1920s, in what was then called the “City of Visitacion.”<sup>2</sup> This change in the area’s development was due in large part to the presence of Southern Pacific’s railroad that connected San Francisco with the peninsula via the Bayshore Cutoff. This cutoff eliminated extra travel time, steep grades, and track curvature from the original route and provided easy access through the mountainous region that had isolated Guadalupe Valley from San Francisco.

The railroad’s development along the bayshore was largely the result of Edward H. Harriman’s modernization efforts for the Southern Pacific Railroad during the early twentieth century. Harriman, and his company Union Pacific, gained control of Southern Pacific in 1901.<sup>3</sup> He continued many projects that Southern Pacific had already scheduled, but also pressed for additional modernizations. He revived the plans for a coastline route in San Mateo County that became the Bayshore Cutoff. Versions of the Bayshore Cutoff had been suggested since the 1870s, but the railroad could not justify the high cost of construction along rugged shoreline at the time. Harriman instituted numerous improvements along the Southern Pacific line between San Francisco and San Jose.<sup>4</sup> Work on the Bayshore Cutoff began in 1904 and continued for three years, opening for traffic in December 1907. The shoreline route between San Bruno and San Francisco shortened the distance and reduced the maximum grade. The project required boring five tunnels, the fifth of which passed through the hill at Sierra Point, in present-day Brisbane. Additional earthwork was necessary in the large cut at Visitacion Point where about 750,000 yards of soil and rock were removed. This excavated material augmented dredged fill material from the bottom of bay, both of which were used to create a wide, open area just north of the point for construction of the Bayshore freight yard.<sup>5</sup> The new rail route impacted the development of industry in the area

<sup>2</sup> In 1929, the name Visitacion City was changed to Brisbane by Arthur Annis (also spelled “Ennis” in some sources), a local realtor and land developer. Annis thought that the “City of Visitacion” was often confused for “Visitation Valley” a district not far from the Guadalupe Valley. Oral History Associates, *A Spirit of Independence*, 4-5 and 11.

<sup>3</sup> Donovan Hofsommer, “For Territorial Dominion in California and the Pacific Northwest: Edward H. Harriman and James J. Hill,” *California History* (Spring 1991), 31.

<sup>4</sup> Donovan Hofsommer, *The Southern Pacific, 1901-1985*, (College Station : Texas A&M University Press, 1986) 9-11. Collis Potter Huntington, the last of the “Big Four,” died on August 13, 1900. Hopkins died in 1878, followed by Crocker in 1888 and Stanford in 1893.

<sup>5</sup> Southern Pacific Bureau of News, “Historical Outline,” 77; Board of Supervisors, *San Francisco Municipal Reports for the Fiscal Year 1903-1904, Ending June 30, 1904* (San Francisco: J. B. McIntyre, Printer and Bookbinder, 1905), 679-709; Loren Nicholson, *Rails Across the Ranchos*, Centennial Edition (San Luis Obispo, CA: California Heritage Publishing Associates, 1993), 133-138; Rufus Steele, “The Spread of San Francisco: The New City...” *Sunset Magazine* 19 (June 1907): 116-117; “Construction on the Bay Shore Line of the Southern Pacific Co.,” *The Railway and Engineering Review* (October 20, 1906): 807-809.

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\*Resource Name or # (Assigned by recorder) Map Reference # 1

\*Recorded by C. McMorris / J. Cheney / S. Klima \*Date July 9 8 ☒ Continuation ☐ Update

over the next several decades. The few industries that were already located in the area and elsewhere along the bay shore, were soon joined by dozens more as manufacturers of all types, including the Pacific Fruit Express Company (PFE), in 1924, at what is now 3401 Bayshore Boulevard. This property was located adjacent to the railroad tracks and not far from Bayshore Yard, providing it easy access to trains.<sup>6</sup>

The Pacific Fruit and Express Company (PFE) was an example of the Southern Pacific Railroad Harriman's strong business sense. Harriman established several subsidiary companies to Southern Pacific railroad, including PFE, which he started in 1906 and began operating in 1907.<sup>7</sup> PFE was a joint venture between Southern Pacific Railroad and the Union Pacific Railroad and provided ice houses and ice manufacturing plants located in several cities in California. California fruit, for which the state was famous, was shipped across the country by rail. Ice was crucial to the transportation of perishable goods, including fruit and meat, and ice houses such as PFE's were used to store, and later manufacture their ice that cooled freight cars carrying produce. Farmers could send produce to the east without having to worry that the produce would rot before it reached eastern markets. PFE became the largest iced freight car company and revolutionized the shipping of perishable foods.<sup>8</sup> The building located at 3401 Bayshore Boulevard was originally constructed as the PFE ice manufacturing plant in Brisbane and it currently is the Machinery and Equipment Company, a retail food, chemical, and mineral equipment company.

In the early 1900s, PFE relied on ice obtained from natural sources in the Sierras to chill the produce freight cars. Ice was brought to ice storage houses where it was stock piled and then loaded onto produce freight cars. In the 1920s, PFE began to construct new and enlarged ice plants and facilities to manufacture ice on site rather than bringing natural ice in from the Sierra Nevada. The company streamlined their ice manufacturing plants by creating a standard layout for the plants. The layout of the PFE plant at Visitacion was in keeping with this general design in its reinforced concrete frame, with brick infill forming the walls. The interior walls of the cold storage room contained two to three inches of Lith (stone) faced with several inches of cork.<sup>9</sup> As shown, the cork layers are still visible on the interior walls of the cold storage room of the PFE Visitacion plant. The plant at Visitacion also had a ten car-length platform that was accessible on either side by parallel railroad tracks (one for loading, the other for unloading).<sup>10</sup> This was also a common feature of PFE ice plants. The railroad tracks and concrete icing deck have been removed from the site; however, the winter and summer brick storage building and the one-story wing attached to it remain.<sup>11</sup>

### Historic Evaluation

The former PFE Ice Mechanization Plant in Brisbane, at 3401 Bayshore Boulevard, has been evaluated under NRHP Criteria A and C (CRHR Criteria 1 and 3). Research did not indicate any possible historic person associated with this property that would make it eligible under NRHP Criterion B (CRHR Criterion 2) and, in rare instances, buildings themselves can serve as sources of important information about historic construction materials or technologies, this structure is otherwise documented and does not appear to be a principal source of important information in this regard and thus is not significant under NRHP Criterion D (CRHR Criterion 4).

<sup>6</sup> Blum, "South San Francisco: The Making of an Industrial City," 114-134; Sanborn Map Company, *Sanborn Fire Insurance Maps, South San Francisco, California*, (San Pablo, CA: Vlad Shkurkin, 1910. The San Mateo County Appraisal Report (June 7, 1962) for this property provides an estimated date of construction for this property as 1910. There is no evidence to support this early date of construction. The building is not represented on the USGS, *South San Francisco Quadrangle*, 15 minute series, 1915. Furthermore, the design of the building indicates that the southern end wing was designed to house large equipment necessary to manufacture ice.

<sup>7</sup> Anthony Thompson, Robert J. Church, and Bruce Jones, *Pacific Fruit Express Company*, (Berkeley, CA: Signature Press, 2000), 9-10.

<sup>8</sup> Thompson, et al., *Pacific Fruit Express Company*, 307-308; and Southern Pacific Company, "Station Plan of Bayshore Visitacion, San Francisco and San Mateo Counties, Coast Division," February 1923 revised through December 1930 [available at the California State Railroad Museum, Sacramento]

<sup>9</sup> Thompson, et al., *Pacific Fruit Express Company*, 292.

<sup>10</sup> Thompson, et al., *Pacific Fruit Express Company*, 291.

<sup>11</sup> Photographs from the California State Railroad Museum Library, (Sacramento, no date).



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\*Resource Name or # (Assigned by recorder) Map Reference # 1

\*Recorded by C. McMorris / J. Cheney / S. Klima \*Date July 9 8 ☒ Continuation ☐ Update

Under Criterion A and 1, this building appears to be significant within the context of ice manufacturing and produce shipment. PFE was the most successful of the ice manufacturing and shipment companies and dominated the market for many years. During their peak, the company constructed many ice plants along the Southern Pacific Railroad, and in particular constructed at least five in the San Francisco Bay Area. Of these plants, this property reflects the importance ice manufacturing in the produce industry in California as it enabled local California produce to be transported to the eastern part of the country, and therefore contributed to the economic growth of the state. This particular ice house is significant to these industries in that its location made it an important station within the Bay Area as it linked the local produce industry to the rest of the country. This ice house is representative of when PFE began to manufacture ice, rather than harvest and store natural ice. A concrete ice deck was constructed on the property to allow for quick and convenient loading of the ice. The ice house has significance in the development of industry in Brisbane as well, as the ice house was the reason that several car loads of produce per day ran along the spur lines to Brisbane.

Under Criterion C, this building does not appear to be a significant example industrial construction from the early 1900s. The building has a interesting design when considering its industrial use, however, the use of the brick and simplified architectural ornament was common among industrial construction during the early part of the twentieth century. This building does not appear to be a significant example of industrial construction. Examples in the Bay Area of this type of construction are common, although not many survive in Brisbane, and there are significant examples throughout San Francisco and Oakland. Thus, this building does not appear to be a significant example of industrial construction in the late nineteenth and early twentieth century and it does appear to be the work of a master, as PFE is not known to have had designers of such stature on staff for the construction of its facilities and it is not clear who was the original architect for the building.

Although this building appears to be significant within the context of ice packing and produce shipment, under Criterion A and 1, it does not retain sufficient integrity to convey its significance and therefore does not appear to meet the criteria for listing in the NRHP or the CRHR. The plant was unusual in that the ice deck was constructed of concrete instead of wood which was typically used because the latter material allowed for easy repairs and modifications to the deck. Shown below in **Photograph 10**, the ice deck on this plant was a two-story feature on the east side of the building and was approximately ten cars long and allowed for three cars to be unloaded/loaded at once. It was essential in loading ice into the produce cars. Also, because there were two sets of spur tracks cars could be unloaded and loaded simultaneously. Now, without the spurs and the ice deck, the building no longer retains the character-defining features of an ice manufacturing and shipment plant. Additionally, the garage and blacksmith shop have been demolished and spurs removed so that only the ice storage facilities remain on the property. **Photograph 11** shows the building in its historic setting. This setting has been greatly altered over the years. These changes include a new office building built to the south of the former ice plant, the Bayshore Highway (now Bayshore Boulevard) which created a tall berm to the west of the building, and the filling in of the bay to the east and south of the building.

This property has been evaluated in accordance with section 15064.5(a)(2)-(3) of the CEQA Guidelines, using the criteria outlined in Section 5024.1 of the California Public Resources Code, and does not appear to meet the criteria for listing in the California Register of Historic Resources.

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\*Resource Name or # (Assigned by recorder) Map Reference # 1

\*Recorded by C. McMorris / J. Cheney / S. Klima \*Date July 9 0 ☒ Continuation ☐ Update

**Photographs (continued):**



**Photograph 2.** North end of Ice House, camera facing south, July 19, 2005.



**Photograph 3.** East side of Ice House, camera facing northwest, July 19, 2005.

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\*Resource Name or # (Assigned by recorder) Map Reference # 1

\*Recorded by C. McMorris / J. Cheney / S. Klima \*Date July 9 0 ☒ Continuation ☐ Update



**Photograph 4.** South end of Ice House, interior, camera facing south, July 19, 2005.



**Photograph 5.** Ice House tower, interior, July 19, 2005



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\*Resource Name or # (Assigned by recorder) Map Reference # 1

\*Recorded by C. McMorris / J. Cheney / S. Klima \*Date July 9 0 ☒ Continuation ☐ Update



**Photograph 6.** Spur east of Ice House, camera facing north, July 19, 2005.



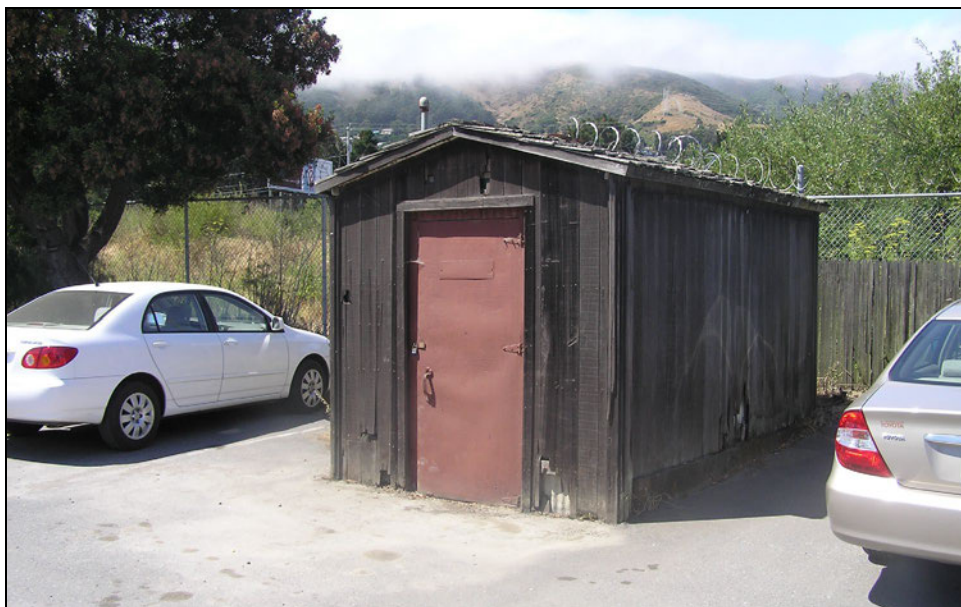
**Photograph 7.** Office Building, camera facing northwest, July 19, 2005.



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\*Resource Name or # (Assigned by recorder) Map Reference # 1

\*Recorded by C. McMorris / J. Cheney / S. Klima \*Date July 9 0 ☒ Continuation ☐ Update



**Photograph 8.** Wood shed in parking lot, camera facing west, July 19, 2005.



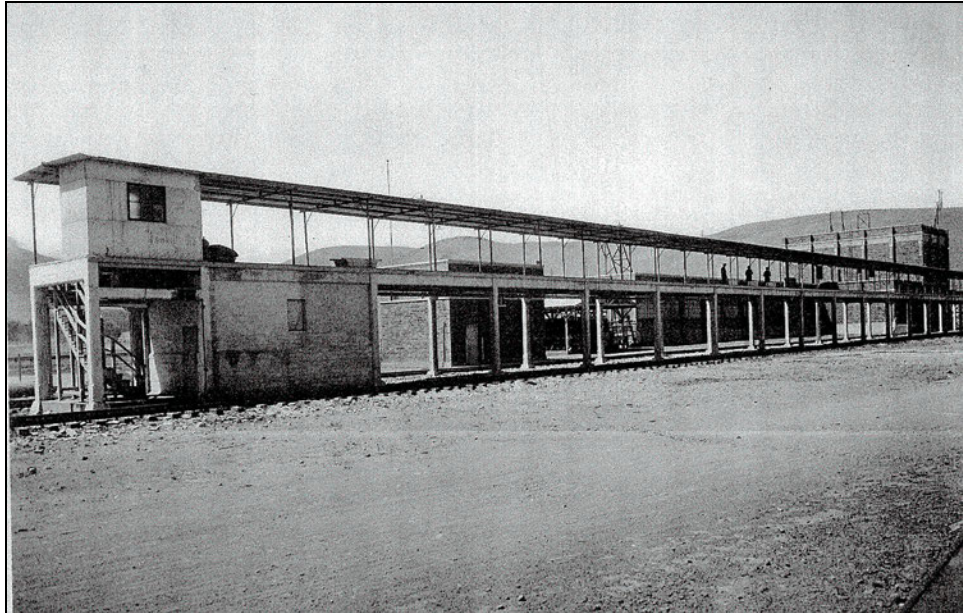
**Photograph 9.** Trailer on property east of Ice House, camera facing north, July 19, 2005.

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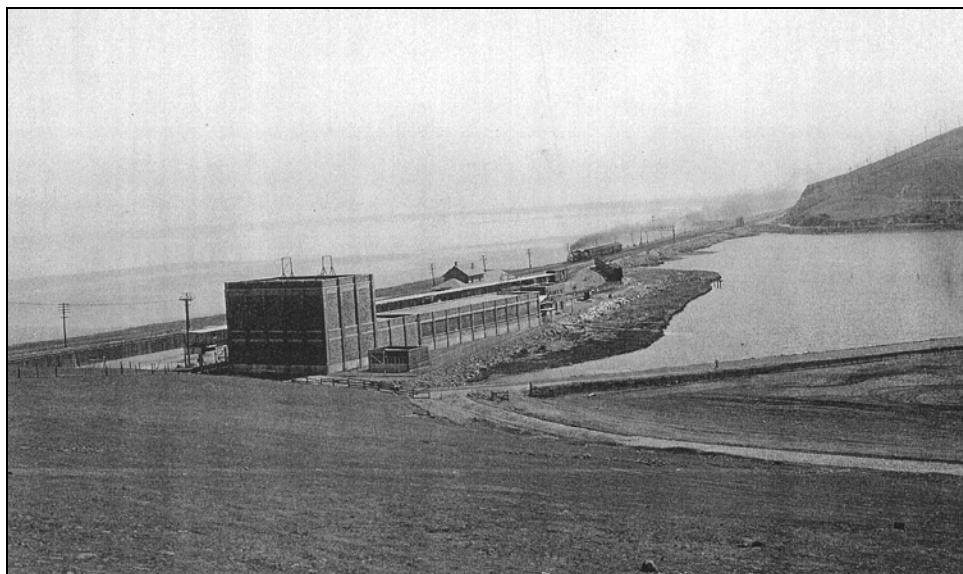
\*Resource Name or # (Assigned by recorder) Map Reference # 1

\*Recorded by C. McMorris / J. Cheney / S. Klima \*Date July 9 0 ☒ Continuation ☐ Update

## Historic Photographs



**Photograph 10.** Pacific Fruit and Express Company Visitation plant. Note the two buildings on the far right side of the photograph are still present. Concrete icing deck, railroad tracks, and brick building in the center of the photograph have been removed. California State Railroad Museum Library, n.d.



**Photograph 11.** Pacific Fruit and Express Company Visitation plant. View facing southeast. California State Railroad Museum Library, n.d.

United States Department of the Interior  
National Park Service

National Register of Historic Places  
Registration Form

This form is for use in nominating or requesting determinations for individual properties and districts. See instructions in *How to Complete the National Register of Historic Places Registration Form* (National Register Bulletin 16A). Complete each item by marking "X" in the appropriate box or by entering the information requested. If any item does not apply to the property being documented, enter "N/A" for "not applicable." For functions, architectural classification, materials, and areas of significance, enter only categories and subcategories from the instructions. Place additional entries and narrative items on continuation sheets (NPS Form 10-900a). Use a typewriter, word processor, or computer, to complete all items.

1. Name of Property

historic name Southern Pacific Railroad Bayshore Roundhouse

other names/site number N/A

2. Location

street & number Junction of Industrial Way and Bayshore Avenue

☐ not for publication N/A

city or town Brisbane

☐ vicinity N/A

state California

code CA

county San Mateo

code 081

zip code 94005

3. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act of 1986, as amended, I hereby certify that this ☒ nomination ☐ request for determination of eligibility meets the documentation standards for registering properties in the National Register of Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60. In my opinion, the property ☒ meets ☐ does not meet the National Register Criteria. I recommend that this property be considered significant ☐ nationally ☐ statewide ☒ locally. (☐ See continuation sheet for additional comments.)

Michael Wayne Anderson  
Signature of certifying official/Title

18 FEB 2010  
Date

California Office of Historic Preservation  
State or Federal agency and bureau

In my opinion, the property ☐ meets ☐ does not meet the National Register criteria. (☐ See continuation sheet for additional comments.)

\_\_\_\_\_  
Signature of commenting or other official

\_\_\_\_\_  
Date

\_\_\_\_\_  
State or Federal agency and bureau

4. National Park Service Certification

I hereby certify that this property is:

☒ entered in the National Register

☐ See continuation sheet.

☐ determined eligible for the  
National Register

☐ See continuation sheet.

☐ determined not eligible for the  
National Register

☐ removed from the National  
Register

☐ other (explain): \_\_\_\_\_

\_\_\_\_\_  
Signature of the Keeper

\_\_\_\_\_  
Date of Action

3/26/2010



Southern Pacific Railroad Bayshore Roundhouse  
Name of Property

San Mateo County, CA  
County and State

## 5. Classification

### Ownership of Property

(Check as many boxes as apply)

- ☒ private  
☐ public-local  
☐ public-State  
☐ public-Federal

### Category of Property

(Check only one box)

- ☒ building(s)  
☐ district  
☐ site  
☐ structure  
☐ object

### Number of Resources within Property

(Do not include previously listed resources in the count.)

Contributing	Noncontributing	
1		buildings
		sites
1		structures
		objects
2		Total

### Name of related multiple property listing

(Enter "N/A" if property is not part of a multiple property listing.)

N/A

### Number of contributing resources previously listed in the National Register

0

## 6. Function or Use

### Historic Functions

(Enter categories from instructions)

Transportation/rail-related

### Current Functions

(Enter categories from instructions)

Vacant/Not in Use

## 7. Description

### Architectural Classification

(Enter categories from instructions)

Early 20<sup>th</sup> century/Other: Industrial

### Materials

(Enter categories from instructions)

foundation Concrete  
roof Composition rolled roofing  
walls Brick  
other

### Narrative Description

(Describe the historic and current condition of the property on one or more continuation sheets.)



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## Southern Pacific Railroad Bayshore Roundhouse, San Mateo County

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### Narrative Description

#### Summary

The Southern Pacific Railroad Bayshore Roundhouse consists of a semi-circular shaped, one-story brick building and turntable pit. It is located in Brisbane, San Mateo County, California, in the southwest corner of the former 200-acre Southern Pacific rail yard. The building encompasses 108 degrees of curvature and is 87 feet deep, from an inside radius of 125 feet to an outside radius of 212 feet. The roundhouse contains 17 enclosed stalls, numbered clockwise from 24 to 40. Significant features are a turntable pit and whisker tracks marked by a series of extant lighting poles. The roundhouse is in damaged condition but retains its integrity of location, design, setting, materials, workmanship, and feeling.

#### Resource

The Southern Pacific Railroad Bayshore Roundhouse consists of a semi-circular shaped, one-story brick building and turntable pit. The building encompasses 108 degrees of curvature and is 87 feet deep, from an inside radius of 125 feet to an outside radius of 212 feet. Subtracting the thickness of the exterior walls, the interior depth is 85 feet, the dimension that appears in the title of original construction drawings. The roundhouse contains 17 enclosed stalls, numbered clockwise from 24 to 40. The remaining stalls (#1-23) are open to the air (so-called "whisker tracks") and are located to the southwest of (counterclockwise from) the enclosed stalls. A series of extant lighting poles mark the location of each outdoor stall. Original construction plans and photographs suggest that pile foundations were driven for all 40 stalls although superstructure was completed for only 17 of them.

The predominant building material is brick, used for the wall along the outer circumference, the end walls, and an internal fire separation wall. The roof is radial gabled, clad with pitch and gravel, and has a light and ventilation monitor along the gable's peak at the east edge. The interior construction is timber post and beam with knee braces. There are limited applications of trusses in the roof structure, including shallow kingpost trusses at longer purlin spans as well as a deeper truss taking the place of a column that would have otherwise interfered with the drop pit between stalls 37 and 38. These trusses consist of wood compression chords and struts with wrought iron (or possibly steel) looped eyebars.<sup>1</sup>

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Cast iron posts are located between the stalls along the inner circumference. These once supported equipment doors and still support cast iron lintels carrying the roof above each opening.

Arched windows are located on the outer circumference of the roundhouse and in the east facing wall at stall 40. The east facing (side) wall has four sets of triple, double hung windows, nine over nine divided lights with wood sashes. The west facing (side) wall is unpierced because it was intended to be an interior fire wall separating stall 24 from the planned but never built stall 23.<sup>2</sup> The roundhouse's curve is actually a series of faceted segments corresponding to each stall. Each segment on the north facade contains two sets of double hung windows, with nine over nine divided lights and wood sashes. All of the glass panes have been broken and many of the wood sashes are damaged to various degrees.

Inside the roundhouse, drop pits are visible in stalls #39 and #40. These are partially filled with debris. The floor is concrete and no tracks are visible in the roundhouse. At some point during the roundhouse's history, a wood partition was erected between stalls 39 and 40 so that the latter could be used as a paint booth. This partition is remarkable in that it has a beaded wainscot, an unusually high degree of finish for a maintenance facility that indicates the possible involvement of the railroad's cabinetry shop. The only other extant partition is an interior brick wall providing a fire separation between stalls 32 and 33.

The Bayshore roundhouse was completed with a steel turntable that sat within a 90-foot diameter concrete pit. The turntable was expanded to 110-foot diameter in 1941 to handle longer locomotives.<sup>3</sup> The turntable was removed after the building was no longer used as a roundhouse. The concrete turntable pit is extant and filled with water and overgrown with vegetation. The circumference of the top of the pit walls are visible in several locations.

The open air stalls, located adjacent to the brick roundhouse at the southwest corner of the building where the "whisker tracks" once were laid, are traced by a series of extant, wood lighting poles dating from the period of significance that marks the location of each outdoor stall (#1-23). The whisker tracks are not visible and may have been removed or they may be covered by topsoil.

The roundhouse site is bounded on the south by Industrial Boulevard and on the west by Bayshore Boulevard. To the north and east of the roundhouse lie a flat expanse of land that once contained numerous tracks, buildings, and structures of the rail yard. All that remain are the roundhouse, an adjacent building to the north (the former tank and boiler shop not subject to this nomination), and three wood sheds located on the northern edge of the yard. Also, the main line

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### Southern Pacific Railroad Bayshore Roundhouse, San Mateo County

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of the Southern Pacific's former Peninsula route to San Francisco runs along the eastern edge of the yard and is currently used for Caltrain commuter service and infrequent freight operations. A passenger train station of recent vintage is located along the tracks several hundred yards north of the roundhouse.

#### **Alterations or changes to the property.**

The roundhouse experienced minor alterations during its period of significance and retains its integrity.<sup>4</sup> Historic photographs and original construction drawings show light and ventilation monitors running along the greater portion of the roof, as well as smoke jacks to vent the smokestacks of steam locomotives running inside the building.<sup>5</sup> It is not known when these monitors and smoke jacks were removed. The iron posts along the inside circumference of the roundhouse originally were covered with swing out wood doors but these doors are not extant. The opening to stall #39 is presently covered with corrugated metal. Several door openings in the outer circumference allowed locomotives to run completely through the building. The door opening at stall 26 is shown on the original construction drawings, and a second door at stall 37 that was either closed up or never built. The present door openings at stalls 36 and 38 were clearly added at a later date. These two openings preserve evidence of changing maintenance standards, as the jambs are toothed in with different colored brick and a lower quality of workmanship than the original masonry.

#### **Deterioration**

The building shows signs of damage due to vandalism, neglect, weather, and a fire that occurred in 2001, burning through the roof structure over stalls 24 through 32 but stopped by the brick fire separation wall so that the roof over stalls 33 through 40 remains relatively intact. There is graffiti on many of the wall surfaces. All of the window panes are broken, as are many of the sashes, but the arched window openings and sills are intact. All of these signs of deterioration can be repaired and the roundhouse can be restored without conjecture using existing extant materials and forms, historical records, and other information.

#### **Historic Integrity**

As discussed in Section 8, the former Southern Pacific Railroad roundhouse at Bayshore and its functionally related turntable are significant under National Register of Historic Places Criterion C (Design/Construction) for embodying "the distinctive characteristics of a type, period, or method of construction," of a Southern Pacific Railroad roundhouse of the late 19<sup>th</sup> and early 20<sup>th</sup> centuries. To be listed in the National Register of Historic Places, a property must not only be shown to be significant under the National Register criteria, but it also must have integrity.

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There are seven aspects of historic integrity: location, design, setting, materials, workmanship, feeling and association. According to the National Register, "retention of design, workmanship, and materials will usually be more important than location, setting, feeling, and association" for properties significant under Criterion C.<sup>6</sup> The roundhouse retains these essential aspects of integrity:

The design, i.e., its "form, plan, space, structure, and style" is unchanged.

The building's workmanship, "... the evidence of artisans' labor and skill in constructing or altering a building, structure, object, or site," i.e., remain. All the brick work remains and the interior roof framing and trusses are still extant in more than one half of the building.

Furthermore, subsequent alterations, such as the paint booth partition and the toothed-in jambs of the run-through doors, have attained their own significance as a physical record of the building's evolution in response to changing locomotive maintenance needs.

The roundhouse retains its "key exterior materials dating from the period of its historic significance," i.e., the brick walls. While some of the brick walls have been painted with graffiti, they do not appear to have been seriously compromised. The cast iron posts and lintels along the interior circumference also remain. The latter bear a sequence of stall numbers, 24 through 40, conveying the railroad's original intent to build a larger building. The lighting posts along "whisker tracks" 1 through 23 visually complete the circumference of the roundhouse, marking the extent of original foundation piling that lies beneath.

Although of lesser importance, the roundhouse also retains the aspects of location, setting, and feeling. The building has not been moved, so the location is the same. The setting has changed to the extent that most of the rail yard buildings and tracks have been removed. However, the main line of the Southern Pacific's former Peninsula route is still extant and is in constant use. Trains are seen and heard from the roundhouse, as they were historically. The roundhouse also retains its feeling, i.e., the historic sense of a particular period of time. Its association as the place of roundhouse as been somewhat compromised. Thus, the building retains six of the seven aspects of integrity: location, design, setting, materials, workmanship, and feeling.

Despite the fire in 2001 and its somewhat degraded physical condition, the roundhouse retains its historic integrity. Although the condition of a portion of the building's roof structure has suffered, the effects have not compromised the property's overall historic integrity. All exterior walls remain intact, preserving the original plan extent of the building. The unique shape of the



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## Southern Pacific Railroad Bayshore Roundhouse, San Mateo County

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building, its relatively simple construction, and the retention of its distinctive materials allow it to convey its historic significance. There is no mistaking this building for anything other than a railroad roundhouse. According to the National Register:

(under Criterion C) a property important for illustrating a particular architectural style or construction technique must retain most of the physical features that constitute that style or technique. A property that has lost some historic materials or details can be eligible *if* it retains the majority of the features that illustrate its style in terms of the massing, spatial relationships, proportion, pattern of windows and doors, texture of materials, and ornamentation. The property is not eligible, however, if it retains some basic features conveying massing but has lost the majority of the features that once characterized its style.<sup>7</sup>

The roundhouse retains the majority of its features. The roundhouse retains all the physical features "important for illustrating a particular architectural style or construction technique" such as its massing, spatial relationships, proportion, pattern of windows and doors, texture of

materials, and ornamentation. Loss of some of the roof material, which is not a key feature, does not preclude the roundhouse from eligibility for the National Register.

<sup>1</sup> Southern Pacific Railroad, Maintenance of Way Division, "85 Ft. Brick Roundhouse, Bay Shore, Roof Truss over Drop-Pits," Drawing 8172, Sheet 12, California State Railroad Museum, Sacramento, Calif.

<sup>2</sup> Southern Pacific Railroad, Maintenance of Way Division, "Part Piling Plan," Drawing 8172, Sheet 3, California State Railroad Museum, Sacramento, Calif.

<sup>3</sup> Signor, *Southern Pacific's Coast Line*, 146.

<sup>4</sup> Historic Sanborn maps do not exist for unincorporated San Mateo County where the train yard was located. The County of San Mateo did not issue building permits until 1933, after the roundhouse was built. Although building permit records were supposedly transferred to the City of Brisbane after the area was incorporated, no permits could be located for buildings in the Bayshore Yard except for ones issued in the recent past.

<sup>5</sup> The California State Railroad Museum in Sacramento maintains extensive files on the Southern Pacific Railroad, including original construction drawings for the Bayshore Roundhouse.

<sup>6</sup> Section VIII, <http://www.nps.gov/history/nr/publications/bulletins/nrb15/>. Accessed March 16, 2009.

<sup>7</sup> "How to Apply the National Register Criteria for Evaluation bulletin," National Register Web site, [www.nps.gov/nr/publications/bulletins/nrb15/](http://www.nps.gov/nr/publications/bulletins/nrb15/), accessed February 10, 2009.

## 8. Statement of Significance

### Applicable National Register Criteria

(Mark "x" in one or more boxes for the criteria qualifying the property for National Register listing)

- ☐ A Property is associated with events that have made a significant contribution to the broad patterns of our history.
- ☐ B Property is associated with the lives of persons significant in our past.
- ☒ C Property embodies the distinctive characteristics of a type, period, or method of construction or represents the work of a master, or possesses high artistic values, or represents a significant and distinguishable entity whose components lack individual distinction.
- ☐ D Property has yielded, or is likely to yield information important in prehistory or history.

### Criteria Considerations

(Mark "X" in all the boxes that apply.)

Property is:

- ☐ A owned by a religious institution or used for religious purposes.
- ☐ B removed from its original location.
- ☐ C a birthplace or a grave.
- ☐ D a cemetery.
- ☐ E a reconstructed building, object, or structure.
- ☐ F a commemorative property.
- ☐ G less than 50 years of age or achieved significance within the past 50 years.

### Narrative Statement of Significance

(Explain the significance of the property on one or more continuation sheets.)

## 9. Major Bibliographical References

(Cite the books, articles, and other sources used in preparing this form on one or more continuation sheets.)

### Previous documentation on file (NPS):

- ☐ preliminary determination of individual listing (36 CFR 67) has been requested.
- ☐ previously listed in the National Register
- ☐ previously determined eligible by the National Register
- ☐ designated a National Historic Landmark
- ☐ recorded by Historic American Buildings Survey # \_\_\_\_\_
- ☐ recorded by Historic American Engineering Record # \_\_\_\_\_

### Areas of Significance

(Enter categories from instructions)

Transportation

Architecture

### Period of Significance

1910-1957

### Significant Dates

N/A

### Significant Person

(Complete if Criterion B is marked above)

### Cultural Affiliation

### Architect/Builder

Unknown/Southern Pacific Railroad

### Primary Location of Additional Data

- ☐ State Historic Preservation Office
- ☒ Other State agency
- ☐ Federal agency
- ☐ Local government
- ☐ University
- ☐ Other

Name of repository:

California State Railroad Museum Library

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## Southern Pacific Railroad Bayshore Roundhouse, San Mateo, CA

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### Narrative Statement of Significance:

The former Southern Pacific Railroad roundhouse at Bayshore and its functionally related turntable are significant under National Register of Historic Places Criterion C (Design/Construction) for embodying “the distinctive characteristics of a type, period, or method of construction,” of a Southern Pacific Railroad brick roundhouse of the late 19<sup>th</sup> and early 20<sup>th</sup> centuries. As an early 20th century transportation building, it is significant on the local/regional level in the areas of Transportation and Architecture. Developed in response to the need to service and repair locomotives during the steam era, roundhouses typically had circular footprints with tracks that were arranged radially leading to separate stalls. Roundhouses often had turntables to position locomotives into individual stalls. Although there were non-roundhouse designs, the roundhouse form was a compact solution to service steam locomotives that were generally designed to run in the forward direction with only limited ability to back up. It is the only extant railroad roundhouse of the many that once existed in California during the era of steam locomotives. It is eligible for listing as an example of a distinctive railroad support facility. The period of significance is 1910-1957. This period coincides with the date the roundhouse was completed (1910) to the year when steam locomotives were last serviced in the roundhouse (1957).

### Historic Context:

#### Native American Period

The site of the Bayshore roundhouse and rail yard was once part of San Francisco Bay. The Bayshore roundhouse and yard were constructed on man-made land on what one had been a shallow cove at the end of Visitacion Valley, located on the border between San Francisco and San Mateo counties. Native Americans lived along San Francisco bay from the mid-peninsula to San Francisco.<sup>1</sup> The Yelamu tribe lived in the area of San Francisco and the villages of Amuctac and Tubsinte were located in Visitacion Valley.<sup>2</sup>

#### Spanish/Mexican Period

Spanish colonization of the San Francisco peninsula began when over 200 Spanish colonists arrived in June 1776 to build a fort (the Presidio of San Francisco) and mission, (San Francisco de Asis, commonly known as Mission Dolores) using Native Americans as a workforce. A nearby pueblo or town, Yerba Buena (the future San Francisco), was also established. Mexico achieved independence from Spain in 1821 and the Mexican Congress secularized the mission

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lands in 1833, making large tracts available to petitioners requesting ranch land from the government.<sup>3</sup> Jacob Lesse took possession of a 9,500 acres land grant in 1841 called *Rancho Canada de Guadalupe la Visitacion y Rodeo Viejo* which included three valleys: *La Visitacion Rodeo Viejo* and *Canada de Guadalupe*. The grant included portions of the future the cities of San Francisco, Daly City, and Brisbane. He transferred the Visitacion Valley rancho to Robert Ridley for lands in Sonoma in 1845.

American Period

When the United States took control of California after Mexican-American War (1846-1848), many titles to Mexican land grants were contested. In time, major land owners in Visitacion Valley included Francois Pioche and Henry Schwerin who both established plant nurseries in Visitacion Valley. Visitacion Valley generally runs east to west along the boundary of San Francisco and San Mateo counties.

The area also witnessed vegetable farms in the late 1800s.<sup>4</sup> Most of the farmers in Visitacion Valley were Italian immigrants from Genoa, who also worked the outlying areas in Mission Valley, Noe Valley, Hayes Valley, Ocean View, Bayview, and Lake Merced. Entire families and hired hands cultivated ten-acre plots with water pumped by windmills. The produce was loaded into horse-drawn wagon in the early morning hours and driven to the Colombo Market,<sup>5</sup> a vegetable exchange established in 1876 at Davis and Front streets, the "greatest vegetable market in the world."<sup>6</sup> After the turn of the 20<sup>th</sup> century, farming became less attractive and the land was more valuable as home sites.<sup>7</sup> While some farms survived in Visitacion Valley until the 1940s,<sup>8</sup> and some nurseries until the 1960s, the transformation of agricultural land into residential tracts was almost complete by World War II.

Most of the Bayshore yard including the roundhouse lies in the City of Brisbane, immediately south of San Francisco. Until Brisbane incorporated in 1961, the rail yard was located in an incorporated part of San Mateo County. The town of Brisbane lies on San Bruno Mountain, south of the yard and physically separated by hills. Brisbane was subdivided for the first time in 1908 but no streets or utilities were installed and the area remained limited to a few roadhouses near the old county road (today's Bayshore Boulevard) and pastures. Growth was slow until the 1930s when more than 400 houses were built on the hill sides on 25 foot wide lots between 1929 and 1933. By 1942, the population was about 3,000 and it increased to 4,000 by 1960.<sup>9</sup>



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## Southern Pacific Railroad Bayshore Roundhouse, San Mateo, CA

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### Southern Pacific History

The Southern Pacific Railroad had significant impact on the West, the state of California, and the San Francisco Bay Area. Many believe that the Southern Pacific had a baneful effect on the state's economic development because of its monopolistic practices, dominance of shipping rates, land holdings, and political clout. A more recent view sees the Southern Pacific as more aligned with the public welfare and as "a major force in shaping agricultural, industrial, commercial, and urban growth and modernization."<sup>10</sup> In any case, the Southern Pacific impacts were far reaching in the settlement and development of California in the 19<sup>th</sup> and early 20<sup>th</sup> century.

The antecedents of the Southern Pacific began in 1861 when railroad engineer Theodore Judah convinced Collis P. Huntington, Leland Stanford, Charles Crockett and Mark Hopkins, to build the western leg of the transcontinental railroad over the Sierra Nevada Mountains with the creation of the Central Pacific Railroad. After Judah's death in 1863, the Big Four, as they later became known, would subsequently run an ever expanding railroad enterprise for several decades though a complex arrangement of many companies and subsidiaries.<sup>11</sup>

At about the time of Judah's death, another railroad was completed running between San Francisco and San Jose. In 1864, the San Francisco and San Jose Railroad (SF&SJ) began running trains from San Jose to San Bruno along the present day Caltrain route. At San Bruno, the line curved west to avoid the San Bruno Mountain and entered San Francisco near Lake Merced. The steam trains ran through San Francisco neighborhoods to a terminal at 25<sup>th</sup> and Valencia Streets.<sup>12</sup> The backers of the SF&SJ railroad also attempted to build a transcontinental line but they hoped to go several hundred miles south and then turn east around the Sierra Nevada Mountains. They renamed their railroad the Southern Pacific. The Big Four acquired this railroad in 1870, kept the name, and rapidly pushed it south to Los Angeles (1876), Tucson (1880), El Paso (1881), and finally New Orleans (1883).<sup>13</sup>

By the 1890s, C.P. Huntington was the last survivor of the Big Four and the Southern Pacific (SP) was one of the largest transportation companies in the world with oceanic steamships, ferries, interurban trains, and 8,000 miles of track.<sup>14</sup> In 1901, Edward H. Harriman, who had acquired the Union Pacific Railroad in 1897, assumed financial control of the Southern Pacific after Huntington's death. Harriman unified the Union Pacific and Southern Pacific railroads under a single management. Acknowledged at the time as "the most competent railroad man in the world," Harriman rehabilitated the Union Pacific railroad and was determined to rehabilitate the Southern Pacific by reducing curvature, lowering grades, and improving the rolling stock.<sup>15</sup>

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Creation of the Bayshore Yard and Roundhouse 1904-1908

During the next eight years (1901-1909), Harriman spent \$247 million on the Southern Pacific Railroad, twenty times more than was spent in the previous eight years.<sup>16</sup> As part of a campaign to rehabilitate and modernize the Southern Pacific and other railroads under his control, Harriman built new sidings, expanded block signals, laid millions of tons of ballast, installed heavier rail, built stronger bridges, and bought real estate for new terminals. He also completed three, multimillion dollar projects: the Lucin Cutoff across the Great Salt Lake in Utah (opened 1904), the Montavlo cut-off south of Santa Barbara (opened 1904), and the Bayshore cut-off (opened 1907).<sup>17</sup> The Bayshore yard contains the roundhouse subject to this nomination.

The Bayshore Cut-off involved rerouting the line east from San Bruno and then north along the bay. This new route avoided the narrow right of way, sharp curves, steep grades and San Franciscans' bitter opposition to trains running through their backyards of the original route.<sup>18</sup> The Bayshore route required filling the bay at several places, boring five tunnels, and building a long trestle, but it increased capacity and reduced operating costs.<sup>19</sup>

As part of the project, the cove at Visitation Valley was filled in with tailings from the tunnels. More than 200 acres were reclaimed from the bay to make room for the railroad right of way and for the construction of a major marshalling yard with extensive maintenance facilities. This yard would supplant the cramped San Francisco machine shops at 16<sup>th</sup> Street and the car repair and roundhouse at Mariposa Street (Mission Bay). The new yard, called Bayshore, was 8,400 feet long and included a roundhouse, machine shop, car shops, and a hump<sup>20</sup> for making up train consists, the second to be built on the Pacific coast.<sup>21</sup>

Bayshore was one of several yards and roundhouses that were located along the route from San Francisco to Santa Barbara. This route was administered by the Southern Pacific's Coast Division. Coast Division roundhouses were located at Mission Bay (San Francisco), Bayshore, San Jose, Watsonville Junction, San Luis Obispo, and Santa Barbara. The Mission Bay, San Jose, and Santa Barbara facilities primarily provided light service while Bayshore and San Luis Obispo conducted heavier repairs. During the 1920s, the Manager of Development for the Southern Pacific Railroad claimed that the Bayshore yard held the distinction for maintaining nearly all the 179 locomotives operated by the Coast Division.<sup>22</sup> This implies a heavy utilization of the Bayshore roundhouse. The Southern Pacific used the Bayshore yard for major railcar rehabilitation and locomotive maintenance from 1914 to 1983.<sup>23</sup>

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Bayshore Yard

The roundhouse was one of many buildings and structures constructed at the Bayshore yard. The bulk of the construction took place from 1907 through 1920, although additions were made in later years. The roundhouse was under construction from about 1908 and was completed by the end of 1910, making it the first, or one of first, buildings built in the yard and the only one still extant.<sup>24</sup> The freight car repair shop was operating by November 1917 and the passenger shops were expected to open in December.<sup>25</sup>

By 1920, the yard contained a freight car repair shop, passenger coach repair shop, machine shop, store, and two 150-ton track scales. Under construction was a tank and boiler shop.<sup>26</sup> In 1921, Bayshore yard's repair and heavy locomotive repair shops were handling 42,000 cars a month on 64 outbound and inbound tracks and 21 repair tracks. By the mid twenties, the yard employed 1,196 employees who earned an average of \$164.31 month.<sup>27</sup> Some of these workers were apprentices, men aged 16-21, who took instruction on becoming railroad mechanics in one of eleven trades. Enrollment was open and,

Preference is given to the sons of employees but any willing ambitious young man between 16 and 21 who can read and write and understands ordinary arithmetic will receive consideration...<sup>28</sup>

Although apprenticeships were limited to males, during the first World War 100,000 women worked in the nation's railroads, which were operating under the temporary control of the federal government through the United States Railroad Administration. Apparently, no women were employed in the Bayshore yard during WWI, although women were employed elsewhere during the war by the Southern Pacific.<sup>29</sup>

Many skilled workers were employed at a roundhouse including boilermakers, machinists, sheet metal fabricators, painters, carpenters, blacksmiths, and pipefitters. Unskilled or semi-skilled workers were wipers (who cleaned the locomotives), oilers, ash-pit men, and hostlers (who drove the engines into the roundhouse). Boilermakers repaired or cleaned boiler tubes, a dirty and dangerous occupation, and they received some of the highest wages, \$2.94 for a 10-hour day in 1874 on the Sante Fe Railroad, for example. Machinists were paid slightly more \$3.02/day for their skills fabricating metal parts of the locomotives. Railroad engineers were highest paid at \$3.55/day.<sup>30</sup>

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The challenging life of a roundhouse foreman on the Southern Pacific was chronicled in 1920:

The roundhouse foreman, to a certain extent, must be familiar with all of the trades in connection with the upkeep of the locomotive. He must be conversant with all of the rules as required by the Federal and State laws and regulations governing the upkeep and care of locomotive, boilers and tender and stationary plants. He must know the engineers' and firemen's agreements and rules that govern shop crafts. He must also be temperamentally fitted to the job, for his patience is sometimes tested to the limit. In addition to this he must be something of a diplomat...He must have a good stiff backbone, and if he has a hide like an armadillo so much the better, for he is the shock-absorber and the target of most of the slings and arrows of criticism directed at his department...it is the roundhouse foreman's good judgment to a great extent that keeps the trains moving."<sup>31</sup>

The Bayshore yard served one of the business segments on the entire Southern Pacific (SP) system. During the 1920s, Bayshore handled 22 million gross tons per mile, making it SP's heaviest traveled stretch of track.<sup>32</sup> While the 1930s saw a fall off in traffic, the line again became the heaviest on the SP system during World War II with 46.5 million gross tons per mile in 1943. Little has been recorded about activities at the yard during the war. It is known that special blackout hoods to shield headlights, signal lights, and markers from enemy planes were fabricated in Bayshore Yard for use during WWII.<sup>33</sup> Women were employed during the war at Bayshore as were workers from Mexico.<sup>34</sup>

After the war, diesel powered locomotives supplanted and then replaced steam powered locomotives across the country including the Southern Pacific. Diesel freight engines were introduced by SP in 1948 and diesel switchers entered service on Coast Division in 1950. Steam locomotives were phased out on the Coast line during mid 1950s and the last use of steam engines was on the San Francisco Peninsula commuter service on January 22, 1957.<sup>35</sup>

Diesel engines require considerably less maintenance than steam engines and roundhouses were largely obsolete by the late 1950s with the end of steam.<sup>36</sup> Southern Pacific's steam facilities were quickly closed: the Santa Barbara roundhouse closed in 1956; steam facilities at San Luis Obispo and Watsonville Junction closed in 1957; and the Mission Bay roundhouse was closed in 1960. None of these roundhouses are extant. In 1957, Bayshore became a graveyard for steam engines waiting to be scrapped.<sup>37</sup>

The Bayshore yard declined as San Francisco became less important as a freight terminal after WWII. The Western and Coast divisions were combined in 1964 with Oakland becoming the



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Southern Pacific Railroad Bayshore Roundhouse, San Mateo, CA

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hub of operations in the Bay Area. Further consolidations occurred along the line during the 1960s with closing of Santa Barbara and San Luis Obispo yards.<sup>38</sup> In the spring of 1979, Bayshore yard was phased out and facilities were dismantled over next several years.<sup>39</sup> One of the last demolitions occurred in 2000 when a permit was issued by the City of Brisbane for the demolition of seven structures containing about 3,000 square feet and identified as "the old firehouse and abandoned buildings and fragment walls."<sup>40</sup> The only buildings that currently remain are the roundhouse, the adjacent former tank and boiler shop (rented by the Lazzari Fuel Company), and two or three wood, one-story buildings at the northern end of the yard. The roundhouse suffered a fire on September 1, 2001 which burned about one half of the roof.<sup>41</sup>

The Significance of the Railroad Roundhouse Property Type

The Bayshore roundhouse is the only surviving roundhouse built by Southern Pacific in California. It embodies the distinctive characteristics of the type, period, and method of construction used in roundhouses of the steam era in the early 20<sup>th</sup> century. It is also the only surviving roundhouse of any railroad in California, not counting a few small or specialized railroads.<sup>42</sup> The City of Brisbane lists the roundhouse as a historic resource.<sup>43</sup>

During the steam era, locomotives required constant maintenance and most of this work took place in roundhouses where engines were serviced and repaired between runs.<sup>44</sup> The basic roundhouse structure was a building with a circular footprint. Railroad tracks were arranged radially. Each track led to a separate bay or "stall." Stalls could be used for storage, light service, or heavy service, depending on the setup of the roundhouse. The roundhouse was a compact solution to the need to house steam locomotives that were generally designed to run in the forward direction with only a limited ability to back up. Although commonly called a roundhouse, completely circular roundhouses appear to have been rare in the west. Instead, roundhouses were built in semi-circular designs in varying degrees. The circle was often completed with a number of outdoor tracks, as is the case with the Bayshore roundhouse.<sup>45</sup> Even though it was never built out to its intended 40 stalls, the Bayshore roundhouse was one of the larger roundhouses in California.

An integral feature of most roundhouses was its turntable, a device for positioning locomotives so they could enter a stall for servicing.<sup>46</sup> Turntables were the most compact solution to the problem of re-orienting locomotives. A rotating bridge structure, or "turntable," pivoted about the center of a circular pit with masonry walls, in this case concrete. This rotating structure allowed for access to all of the tracks within the roundhouse. The bridge structure would be positioned to allow a locomotive to access the turntable deck from the entrance track. The

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Southern Pacific Railroad Bayshore Roundhouse, San Mateo, CA

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locomotive would proceed onto the turntable and stop. An operator would then rotate the turntable to access the desired track or stall.

Many nineteenth century roundhouses in the west were constructed of wood but by the 20th century, more permanent and less flammable materials such as stone, concrete, or brick were being used. The roof and interior columns were often of heavy timber. The design of the Bayshore roundhouse reflects these trends. While the architects or engineers who designed it are not known, the roundhouse was a product of the Southern Pacific's construction department. The design is similar to the SP brick roundhouse at San Jose constructed in 1899, with pilastered brick walls along the ends and outer circumference with arched windows; radial gabled roof with light and ventilation monitors along the gable's peak.<sup>47</sup> Inside, drop pits were installed under the bays to conduct maintenance.<sup>48</sup>

The most significant features of the Bayshore roundhouse are its brick construction, curved shape, openings for stalls on one side, and windows on the other side. The design is relatively simple, essentially a curved shaped garage for locomotives. The floor is concrete and the roof is a simple wood framing system. Because it was built on reclaimed land with high potential for post-construction settlement, both the building and tracks within it were built on pile-supported concrete foundations. The building remains unaltered from its configuration in 1957, which had survived in nearly original condition from 1910. The turntable was removed and the circular concrete pit is now filled with water and obscured by vegetation. While the rest of the yard is now nearly vacant, the roundhouse is a visible reminder of the railroad's past. The scene looks much as it did in 1910 when the roundhouse was the prominent feature on the vast expanse of land.

#### Summary

The former Southern Pacific Railroad roundhouse at Bayshore and its functionally related turntable are significant under National Register of Historic Places Criterion C (Design/Construction) for embodying "the distinctive characteristics of a type, period, or method of construction," of a Southern Pacific Railroad brick roundhouse of the late 19<sup>th</sup> and early 20<sup>th</sup> centuries. As an early 20th century transportation building, it is significant on the local/regional level in the areas of Transportation and Architecture. It is the only extant railroad roundhouse of the many that once existed in California during the era of steam locomotives. It is eligible for listing as an example of a distinctive railroad support facility. The period of significance is 1910-1957. This period coincides with the date the roundhouse was completed (1910) to the year when steam locomotives were last serviced in the roundhouse (1957).

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Southern Pacific Railroad Bayshore Roundhouse, San Mateo, CA

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<sup>1</sup> Alan Hynding, *From Frontier to Suburb, The Story of the San Mateo Peninsula* (Belmont, CA: Star Publishing Company, 1982), 8.

<sup>2</sup> Cynthia Cox, et.al, *San Francisco's Visitacion Valley* (Charleston, SC: Arcadia Publishing, 2005), 9.

<sup>3</sup> Mel Scott, *The San Francisco Bay Area: A Metropolis in Perspective* (Berkeley: University of California Press, 1959), 9-11.

<sup>4</sup> Deanna Paoli Gumina, *The Italians of San Francisco 1850-1930* (The Center for Migration Studies of New York, 1978), 99.

<sup>5</sup> "Where the Land Gives Most to the Acre, Italian Vegetable Gardens of San Francisco," *San Francisco Chronicle*, January 18, 1903.

<sup>6</sup> Gumina, *The Italians of San Francisco 1850-1930*, 101

<sup>7</sup> Richard Brandi, "Farms, Fire and Forest: Adolph Sutro and Development West of Twin Peaks," *Argonaut*, San Francisco Museum and Historical Society, Vol. 14, No. 1, Summer 2003, 42.

<sup>8</sup> Raymond Stevenson Dondero, "The Italian Settlement of San Francisco," (Master thesis, U.C. Berkeley, 1953), 57.

<sup>9</sup> Hynding, *From Frontier to Suburb, The Story of the San Mateo Peninsula*, 248.

<sup>10</sup> Richard J. Orsi, *Sunset Limited, The Southern Pacific Railroad and the Development of the American West 1850-1930*, Berkeley: University of California Press 2005, xiv.

<sup>11</sup> Brian Solomon, *Southern Pacific Railroad* (St. Paul, MN: Voyager Press, 2007), 15.

<sup>12</sup> John R. Signor, *Southern Pacific's Coast Line* (Wilton, CA: Signature Press, 1994), 31-21.

<sup>13</sup> Solomon, *Southern Pacific Railroad*, 24.

<sup>14</sup> Ibid, 31.

<sup>15</sup> Don L. Hofsommer, *The Southern Pacific, 1901-1985* (College Station: Texas A&M University Press, 1986), 9.

<sup>16</sup> Harry Klein, *Life and Legend of E. H. Harriman* (Chapel Hill and London: University of North Carolina Press, 2000), 256.

<sup>17</sup> Hofsommer, *The Southern Pacific, 1901-1985*, 14-19.

<sup>18</sup> Signor, *Southern Pacific's Coast Line*, 31-21.

<sup>19</sup> Ibid, 27-37.

<sup>20</sup> A hump is a raised section of track designed to move cars to selected tracks using gravity.

<sup>21</sup> Ibid, 35.

<sup>22</sup> Letter to Jack Hanlon from R.E. Kelly, Southern Pacific Manager of Development, dated April 19, 1926.

<sup>23</sup> "Community Relations Plan: Southern Pacific Railroad Transportation Company Rail Yard Brisbane, California," April 7, 1989, Mara Feeney & Associates, 3.

<sup>24</sup> Signor *Southern Pacific's Coast Line*, 34 shows a photo of the roundhouse and a one story and two story building in 1910.

<sup>25</sup> "Railroad Shops Busily at Work on Equipment to Meet Wartime Needs," *The Bulletin*, Southern Pacific Company employee publication, Vol. VI, No. 22, November 15, 1917, 3.

<sup>26</sup> T. Ahern, "Following in the Footsteps of the Padres, The Coast Division, rich in romance, history, industrial growth and operating achievements, described by it Superintendent," *The Bulletin*, Southern Pacific Company employee publication, Vol. IX, No. 1, November 1920, 3.

<sup>27</sup> Cynthia Cox, et al., *San Francisco's Visitacion Valley*, 42.

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<sup>28</sup> "S.P. Instructors Teach Appendices Eleven Trades," *The Bulletin*, Southern Pacific Company employee publication, Vol. IX, No. 3, March 1920, 12.

<sup>29</sup> "Women in the Field of Railroading," *The Bulletin*, Southern Pacific Company employee publication, Vol. VIII, No. 8, August 1919, 11. Women were employed in the SP's General Office, and the West Oakland, Sacramento, and Los Angeles shops.

<sup>30</sup> Hans and April Halberstat, *The American Train Depot & Roundhouse* (Osceola, WI: Motorbooks International, 1995), 155.

<sup>31</sup> E. R. Burroughs, "Fuel, The Engineer and The Roundhouse Foreman," *The Bulletin*, Southern Pacific Company employee publication, Vol. IX, No. 5, May 1920, 18.

<sup>32</sup> Signor, *Southern Pacific's Coast Line*, 49.

<sup>33</sup> *Ibid.*, 143, 146.

<sup>34</sup> There were wash and locker facilities for women and temporary living quarters for Mexican nationals at Bayshore during WWII, see the California State Railroad Museum archives CE 9878 / Sketch A June 25, 1943 / Aug. 4, 1943. Wash and locker building, for women employees only. 17 x 24 in. Paper negative. Filing location: Big Four Map Case 16 ID 2919. Also listed is Southern Pacific. CE 9954 / P 7 Feb. 1944 Temporary living quarters for Mexican nationals. 9 x 14 in. White line on blue paper. Filing location: Box 36 ID 4227.

<sup>35</sup> Signor, *Southern Pacific's Coast Line*, 158.

<sup>36</sup> <http://www.sanfranciscotrains.org/bayshore-roundhouse.html> accesses February 23, 2009

<sup>37</sup> Signor, *Southern Pacific's Coast Line*, 154-159.

<sup>38</sup> Signor, *Southern Pacific's Coast Line*, 171.

<sup>39</sup> *Ibid.*, 267

<sup>40</sup> City of Brisbane Application for Building Permit 00-0413-08, approved April 13, 2000.

<sup>41</sup> "Fire Destroys Old Railroad House," *San Mateo County Times*, September 1, 2001.

<sup>42</sup> Researching railroad roundhouse architecture is complicated by the lack of scholarly and popular attention to the subject. While much has been written about train terminals and stations, there is a dearth of material about roundhouses.

<sup>43</sup> City of Brisbane, General Plan Background Report, "Existing and Potential Parks, Recreation, Historic and Cultural Resources," dated March 1994.

<sup>44</sup> [www.sanfranciscotrains.org/bayshore-roundhouse.html](http://www.sanfranciscotrains.org/bayshore-roundhouse.html) accessed February 23, 2009.

<sup>45</sup> E-mailed from Randolph R. Ruiz, AIA, November 14, 2008.

<sup>46</sup> The turntable was omitted at some shortline roundhouses (e.g., the San Francisco State Belt Line) where a small number of stalls could be served by a branching network of engine-stall leads.

<sup>47</sup> In 2000, the San Jose roundhouse was disassembled, palletized and moved to the county fairgrounds. The California Trolley and Railroad Corporation Website, [www.ctrc.org/history/lenzen-roundhouse/lenzen-roundhouse](http://www.ctrc.org/history/lenzen-roundhouse/lenzen-roundhouse), accessed January 20, 2009.

<sup>48</sup> Hans and April Halberstadt, *The American Train Depot and Roundhouse*, 147.



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Sources:

Books

Cox, Cynthia, et.al. *San Francisco's Visitation Valley*. Charleston, SC: Arcadia Publishing, 2005.

Dondero, Raymond Stevenson. "The Italian Settlement of San Francisco," Master thesis, U.C. Berkeley, 1953.

Gumina, Deanna Paoli. *The Italians of San Francisco 1850-1930*. The Center for Migration Studies of New York, 1978.

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Hofsommer, Don L. *The Southern Pacific, 1901-1985*. College Station: Texas A&M University Press, 1986.

Hynding, Alan. *From Frontier to Suburb, The Story of the San Mateo Peninsula*. Belmont, CA: Star Publishing Company, 1982.

Klein, Harry. *Life and Legend of E. H. Harriman*. Chapel Hill and London: University of North Carolina Press, 2000.

Orsi, Richard J. *Sunset Limited, The Southern Pacific Railroad and the Development of the American West 1850-193*. Berkeley: University of California Press 2005.

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Signor, John R. *Southern Pacific's Coast Line*. Wilton, CA: Signature Press, 1994.

Solomon, Brian. *Southern Pacific Railroad*. St. Paul, MN: Voyager Press, 2007.

Thompson, Anthony J. and John R Signor. *Southern Pacific's Coast Line Pictorial*. Wilton, CA: Signature Press, 2000.

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## Reports

City of Brisbane, General Plan Background Report, "Existing and Potential Parks, Recreation, Historic and Cultural Resources," dated March 1994.

Mara Feeney & Associates. "Community Relations Plan: Southern Pacific Railroad Transportation Company Rail Yard Brisbane, California," April 7, 1989.

## Periodicals

*The Bulletin*, Southern Pacific Company employee publication:

"Railroad Shops Busily at Work on Equipment to Meet Wartime Needs," Vol. VI, No. 22, November 15, 1917.

"Women in the Field of Railroading," Vol. VIII, No. 8, August 1919.

"S.P. Instructors Teach Appendices Eleven Trades," Vol. IX, No. 3, March 1920.

E. R. Burroughs, "Fuel, The Engineer and The Roundhouse Foreman," Vol. IX, No. 5, May 1920.

T. Ahern, "Following in the Footsteps of the Padres, The Coast Division, rich in romance, history, industrial growth and operating achievements, described by it Superintendent," Vol. IX, No. 1, November 1920.

Southern Pacific Railroad Bayshore Roundhouse  
Name of Property

San Mateo County, CA  
County and State

## 10. Geographical Data

### Acreage of Property

### UTM References

(Place additional UTM references on a continuation sheet)

	Zone	Easting	Northing		Zone	Easting	Northing
1	10	552390	4172700	3	—	—	—
2	—	—	—	4	—	—	—

☐ See continuation sheet.

### Verbal Boundary Description

(Describe the boundaries of the property on a continuation sheet.)

The property is located on Assessor's Parcel Number APN 005-340-060 (see map)

### Boundary Justification

(Explain why the boundaries were selected on a continuation sheet.)

## 11. Form Prepared By

name/title Richard Brandi, Architectural Historian

organization \_\_\_\_\_ date November 9, 2009

street & number 125 Dorchester Way telephone 415 753-5130

city or town San Francisco state CA zip code 94127

### Additional Documentation

Submit the following items with the completed form:

### Continuation Sheets

### Maps

A **USGS map** (7.5 or 15 minute series) indicating the property's location.

A **Sketch map** for historic districts and properties having large acreage or numerous resources.

### Photographs

Representative **black and white photographs** of the property.

### Additional items

(Check with the SHPO or FPO for any additional items)

### Property Owner

(Complete this item at the request of the SHPO or FPO.)

Name Steve Hanson, General Manager, Universal Paragon Corp.

street & number 150 Executive Park Blvd # 4200 telephone (415) 468-6676

city or town San Francisco state CA zip code 94134

**Paperwork Reduction Act Statement:** This information is being collected for applications to the National Register of Historic Places to nominate properties for listing or determine eligibility for listing, to list properties, and to amend existing listings. Response to this request is required to obtain a benefit in accordance with the National Historic Preservation Act, as amended (16 U.S.C. 470 *et seq.*).

**Estimated Burden Statement:** Public reporting burden for this form is estimated to average 18.1 hours per response including the time for reviewing instructions, gathering and maintaining data, and completing and reviewing the form. Direct comments regarding this burden estimate or any aspect of this form to the Chief, Administrative Services Division, National Park Service, P.O. Box 37127, Washington, DC 20013-7127; and the Office of Management and Budget, Paperwork Reductions Project (1024-0018), Washington, DC 20503.

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### Verbal Boundary Description

The boundary of the roundhouse is pie-shaped and includes the buildings and structures of the property subject to the nomination: the roundhouse, turntable pit, and whisker tracks. The roundhouse is semi-circular shaped and encompasses 108 degrees of curvature. It contains 17 enclosed stalls, numbered clockwise from 24 to 40, and 23 open air stalls, the so-called "whisker tracks" located to the southwest of (counterclockwise from) the enclosed stalls. A series of extant lighting poles mark the location of each outdoor stall. Additionally, a steel turntable once sat within a 90-foot diameter concrete pit (later expanded to 110-foot diameter). The turntable was removed and the concrete turntable pit is extant and filled with water and vegetation.

The boundary of the nominated property extends in a curve approximately 10 feet north of the roundhouse to an existing fence and continues west and south in a circumference that includes the poles marking the whisker tracks. The resultant pie shaped boundary encompasses the whisker tracks and the turntable pit.

### Verbal Boundary Justification

The nominated property includes the property historically occupied by the roundhouse, its associated whisker tracks, and the turntable pit described in the verbal boundary description.



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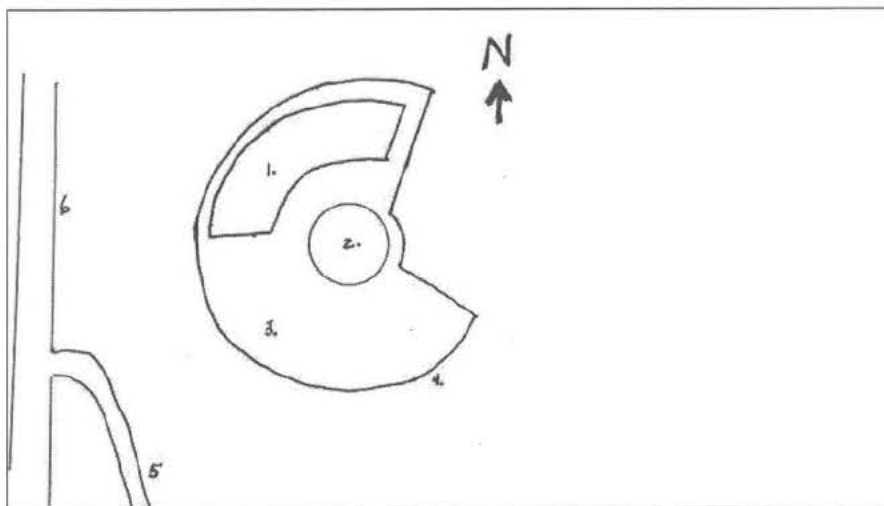
Southern Pacific Railroad Bayshore Roundhouse, San Mateo, CA

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### Geographical Data:

**Southern Pacific Railroad Bayshore Roundhouse: Contributing Buildings, Structures and Site.**



### Legend

- 1. Roundhouse
- 2. Turntable pit
- 3. Whisker tracks area
- 4. Boundary line of area to be included in nomination
- 5. Industrial Way
- 6. Bayshore Boulevard (not to scale)

Scale 1" = 135 feet

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Southern Pacific Railroad Bayshore Roundhouse, San Mateo, CA

Section number Add'l Documentation Photographs Page 1 of 1

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**Photographs**

Name of Property: Southern Pacific Railroad Bayshore Roundhouse,  
Address: Bayshore Boulevard and Industrial Way, City of Brisbane,  
County and State: San Mateo county, California.  
Location of Digital Files: Richard Brandi, 125 Dorchester Way, San Francisco, CA  
  
Photographer: All photos taken by Richard Brandi.  
Photos taken February 2009.

CA\_SanMateo\_SPRoundhouse\_0001

The former Southern Pacific rail yard, camera looking northwest, with roundhouse in the center of photo.

CA\_SanMateo\_SPRoundhouse\_0002

The north façade of Southern Pacific roundhouse, camera looking southeast.

CA\_SanMateo\_SPRoundhouse\_0003

The north façade of Southern Pacific Roundhouse, camera looking west.

CA\_SanMateo\_SPRoundhouse\_0004

The south façade of Southern Pacific Roundhouse, camera looking east.

CA\_SanMateo\_SPRoundhouse\_0005

The south façade of Southern Pacific Roundhouse, camera looking west.

CA\_SanMateo\_SPRoundhouse\_0006

The east façade Southern Pacific Roundhouse, camera looking west.

CA\_SanMateo\_SPRoundhouse\_0007

The west façade of Southern Pacific Roundhouse, camera looking northeast.

CA\_SanMateo\_SPRoundhouse\_0008

Close up of windows on north façade of Southern Pacific Roundhouse, camera looking south.

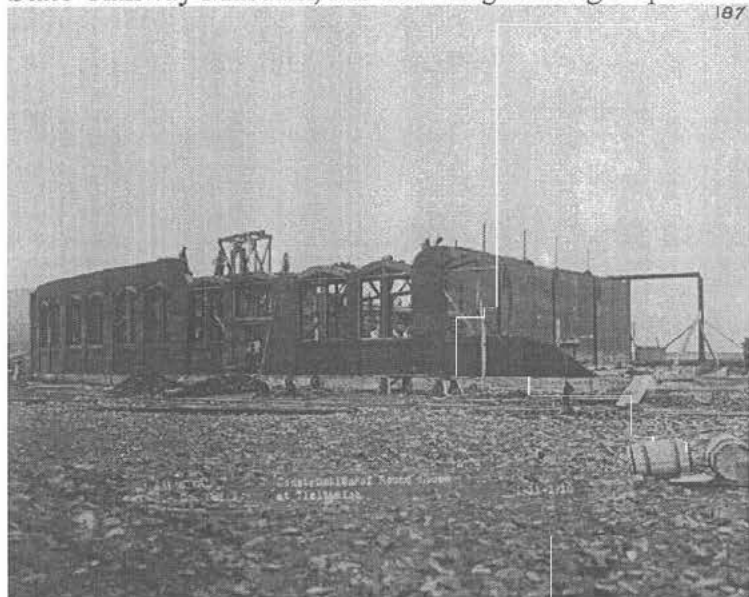
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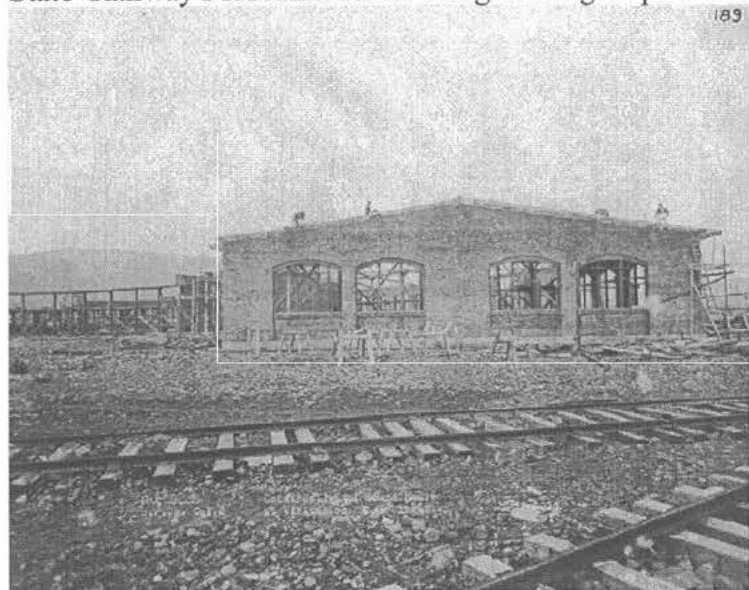
Southern Pacific Railroad Bayshore Roundhouse, San Mateo, CA

Section number **Add'l Documentation Historic Illustrations** Page **1 of 3**

Historic Illustration 1. Roundhouse under construction looking east in 1910. Source: California State Railway Museum, MS 10/4 Engineering Dept. Records SPED b-3.



Historic Illustration 2. Roundhouse under construction looking west in 1910. Source: California State Railway Museum, MS 10/4 Engineering Dept. Records SPED b-3.



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## Southern Pacific Railroad Bayshore Roundhouse, San Mateo, CA

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Historic Illustration 3. Bayshore Yard looking north in 1911. Roundhouse is in the upper left hand side of photo. Source: John R Signor, *Southern Pacific's Coast Line* (Wilton, CA: Signature Press, 1994) 39.



Historic Illustration 4. Bayshore Yard looking southwest in 1921. Source: John R Signor, *Southern Pacific's Coast Line* (Wilton, CA: Signature Press, 1994), 49.





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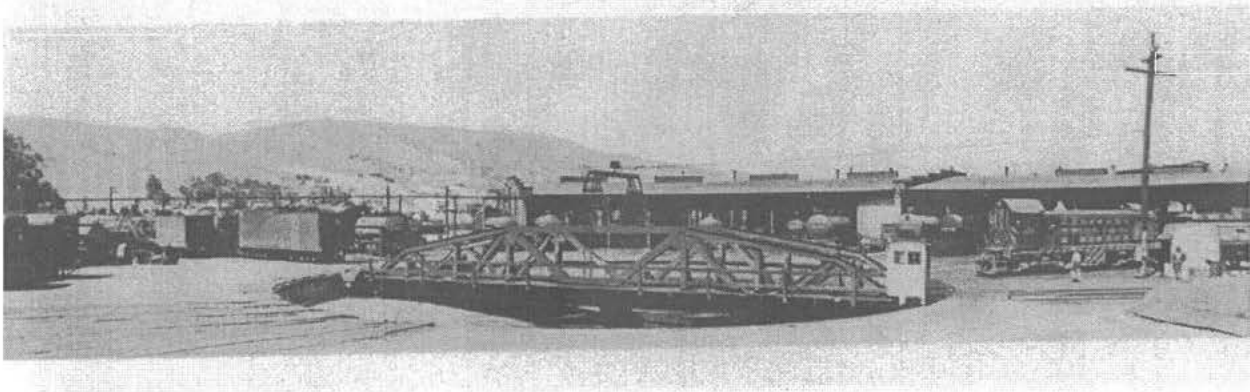
## National Register of Historic Places Continuation Sheet

Southern Pacific Railroad Bayshore Roundhouse, San Mateo, CA

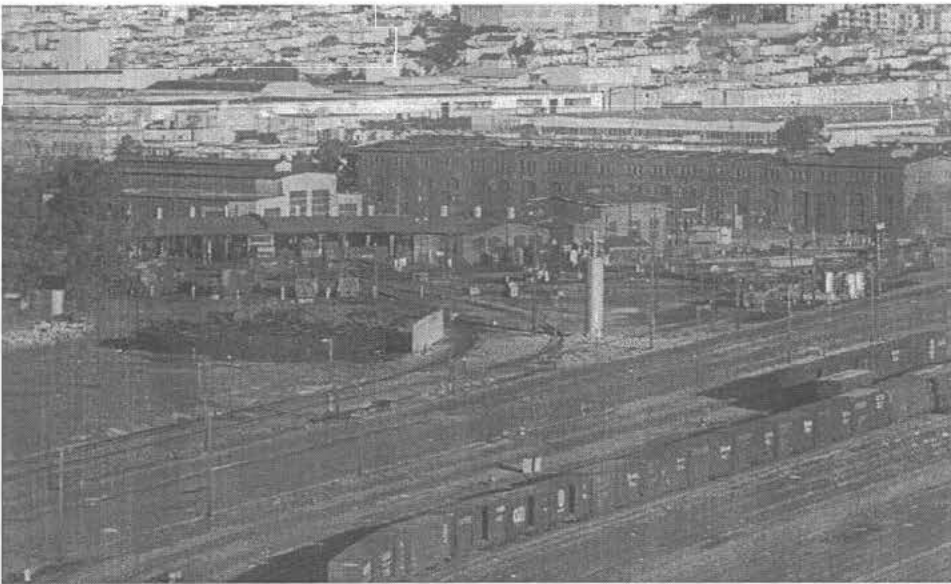
Section number **Add'l Documentation-Historic Illustration** Page **3 of 3**

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Historic Illustration 5. Bayshore Roundhouse and turntable looking northwest in 1950. Source: John R. Signor, *Southern Pacific's Coast Line* (Wilton, CA: Signature Press, 1994), 187.



Historic Illustration 6. Roundhouse and turntable with other yard buildings (no longer extant) in 1975. Source: Anthony J. Thompson and John R. Signor (*Southern Pacific's Coast Line Pictorial*, Wilton, CA: Signature Press, 2000), 39.



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NATIONAL REGISTER OF HISTORIC PLACES  
CONTINUATION SHEET

Section \_\_\_\_\_ Page \_\_\_\_\_

SUPPLEMENTARY LISTING RECORD

NRIS Reference Number: 10000113 Date Listed: 03/26/2010

Southern Pacific Railroad

Bayshore Roundhouse

Property Name

San Mateo

County

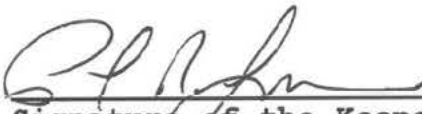
CA

State

N/A

Multiple Name

-----  
This property is listed in the National Register of Historic Places in accordance with the attached nomination documentation subject to the following exceptions, exclusions, or amendments, notwithstanding the National Park Service certification included in the nomination documentation.



Signature of the Keeper

3/26/2010

Date of Action

-----  
Amended Items in Nomination:

Acreage:

The appropriate acreage is: *less than one acre*.  
[The nomination omitted a citation.]

Significance:

*Transportation* is deleted as an Area of Significance.  
[Generally, properties significant under Criterion C select Architecture or Engineering as the most appropriate area of significance. Transportation, here, reflects more the function of the property than its significance, although a case under NR Criterion A (Transportation) might have been possible for this rare extant infrastructure resource.]

These clarifications were confirmed with the CA SHPO office.

DISTRIBUTION:

National Register property file  
Nominating Authority (without nomination attachment)

UNITED STATES DEPARTMENT OF THE INTERIOR  
NATIONAL PARK SERVICE

NATIONAL REGISTER OF HISTORIC PLACES  
EVALUATION/RETURN SHEET

REQUESTED ACTION: NOMINATION

PROPERTY Southern Pacific Railroad Bayshore Roundhouse  
NAME:

MULTIPLE  
NAME:

STATE & COUNTY: CALIFORNIA, San Mateo

DATE RECEIVED: 2/19/10 DATE OF PENDING LIST: 3/04/10  
DATE OF 16TH DAY: 3/19/10 DATE OF 45TH DAY: 4/05/10  
DATE OF WEEKLY LIST:

REFERENCE NUMBER: 10000113

REASONS FOR REVIEW:

APPEAL: N DATA PROBLEM: N LANDSCAPE: N LESS THAN 50 YEARS: N  
OTHER: N PDIL: N PERIOD: N PROGRAM UNAPPROVED: N  
REQUEST: Y SAMPLE: N SLR DRAFT: Y NATIONAL: N

COMMENT WAIVER: N

\_\_\_ACCEPT \_\_\_RETURN \_\_\_REJECT \_\_\_\_\_DATE

ABSTRACT/SUMMARY COMMENTS:

**The Southern Pacific Railroad Bayshore Roundhouse is significant under National Register Criterion C (Architecture) at the local level of significance. The building is an important illustration of early twentieth century railroad facility construction. Built in 1910, the roundhouse is the only surviving structure of its type built by the Southern Pacific Railroad in California, and served as an important component of the system's operating infrastructure in an era of significant growth and modernization. Despite the low physical integrity of the resource (fire damage, vandalism, neglect) the property is readily able to convey its distinctive historic function and construction design.**

RECOM./CRITERIA Accept Criterion C

REVIEWER Paul Lusignea DISCIPLINE HISTORIAN

TELEPHONE \_\_\_\_\_ DATE 3/26/2010

DOCUMENTATION see attached comments Y/N see attached SLR Y/N

If a nomination is returned to the nominating authority, the nomination is no longer under consideration by the NPS.























*Southern Pacific  
Railroad Bayshore  
Roundhouse  
Nimintion*



SOUTHERN PACIFIC RAILROAD  
7A BAYSHORE ROUNDHOUSE  
BAYSHORE, SAN MATEO COUNTY, CALIFORNIA  
ZONE 10  
EASTING 552390  
NORTHING 4172700

Produced by the United States Geological Survey  
Topography compiled 1956. Planimetry derived from imagery taken 1993 and other sources. Photomaps using imagery taken 1995; no major culture or drainage changes observed. PLS and survey control current as of 1959. Boundaries revised 1999.  
North American Datum of 1927 (NAD 27). Projection and 1000-meter grid: Universal Transverse Mercator, zone 10 10 000-foot ticks: California Coordinate System of 1927 (zone 3).  
North American Datum of 1983 (NAD 83) is shown by dashed corner ticks. The values of the shift between NAD 27 and NAD 83 for 7.5-minute intersections are obtainable from National Geodetic Survey NADCON software.  
There may be private inholdings within the boundaries of the National or State reservations shown on this map.  
Where omitted, land lines have not been established.

UTM GRID AND 1999 MAGNETIC NORTH DECLINATION AT CENTER OF SHEET  
0°20' 276 MILS  
15 1/4°

SCALE 1:24 000  
1 000 0 1 KILOMETERS  
1 000 0 1 MILES  
CONTOUR INTERVAL 25 FEET  
NATIONAL GEODETIC VERTICAL DATUM OF 1929  
TO CONVERT FROM FEET TO METERS, MULTIPLY BY 0.3048



QUADRANGLE LOCATION

1	2	3
4	5	6
7	8	

1 Point Bonita  
2 San Francisco North  
3 Oakland West  
4  
5 Hunters Point  
6  
7 Montara Mountain  
8 San Mateo

ROAD CLASSIFICATION  
Primary highway  
hard surface .....  
Secondary highway  
hard surface .....  
Light-duty road, hard or  
improved surface .....  
Unimproved road .....  
Interstate Route  
U.S. Route  
State Route

SAN FRANCISCO SOUTH, CA  
1995  
NIMA 1559 III NW-SERIES V895



THIS MAP COMPLIES WITH NATIONAL MAP ACCURACY STANDARDS  
FOR SALE BY U.S. GEOLOGICAL SURVEY, P.O. BOX 25286, DENVER, COLORADO 80225  
A FOLDER DESCRIBING TOPOGRAPHIC MAPS AND SYMBOLS IS AVAILABLE ON REQUEST





**Planning & Building Department  
Historic Resources Advisory Board**

Mitch Postel  
John Edmonds  
Deke Sonnichsen  
Robert Schoeppner

Elizabeth Bogel  
Nancy Oliver  
William Howland  
Mike Bursak

Robert Crow  
Greg Timm  
Gael Erickson

County Office Building  
455 County Center  
Redwood City, California 94063  
(650) 363-1837

May 27, 2009

Cris Hart, President  
San Francisco Trains, Inc.  
223 Mariposa Street  
Brisbane, CA 94005

JUN 02 2009

Dear Mr. Hart:


SUBJECT: County Historic Resource Advisory Board's support  
of placing the Southern Pacific Railroad Roundhouse Building  
in Brisbane on the National Register of Historic Places.

At the Historic Resource Advisory Board's (HRAB) April 15, 2009 meeting, you appeared and asked the HRAB for support in your ongoing application for the nomination of the old (circa. 1908) Southern Pacific Railroad roundhouse building (located within the city limits of Brisbane) for individual listing on the National Register of Historic Places. If successful, the roundhouse would also be automatically listed on the California Register of Historical Resources.

The HRAB has heard your testimony as to your efforts to research and save the roundhouse, reviewed Sections 7 and 8 of the subject nomination application, and read the confirmation (via e-mail) from the City of Brisbane that the roundhouse is recognized as a locally significant cultural resource in the City's General Plan. As a result, the HRAB, at their May 20, 2009 meeting, voted unanimously to prepare and send this letter as evidence of their enthusiastic support for such preservation and official listing efforts. The HRAB agrees that such nomination and subsequent listing will help to ensure the preservation of the only surviving roundhouse built by Southern Pacific in California.

If we can be of any further support or assistance in your efforts, please contact me at 650/593-1793.

Sincerely,

  
John Edmonds  
Chairman, HRAB

JE:DJH:pac - DJHT0490\_WPN.DOC

cc: Mitch Postel, Director, County Historical Association  
Lisa Grote, Community Development Director  
John Swiecki, Principal Planner, City of Brisbane  
Steve Hanson, General Manager, Universal Paragon, Inc.  
Jay Correia, California Office of Historic Preservation, Registration Unit

RESOLUTION NO. 2010-06

A RESOLUTION OF THE CITY COUNCIL OF THE  
CITY OF BRISBANE SUPPORTING THE EFFORTS OF SAN FRANCISCO TRAINS, A NON  
PROFIT ASSOCIATION, TO PLACE THE SOUTHERN PACIFIC RAILROAD  
ROUNDHOUSE AT BAYSHORE YARDS IN BRISBANE CALIFORNIA ON THE NATIONAL  
REGISTER OF HISTORIC PLACES

WHEREAS, the Conservation Element of the City of Brisbane General Plan (General Plan Figure IX-J) identifies the former Southern Pacific Railroad Roundhouse in the Future Phases project area as an important cultural resource to the City; and

WHEREAS, a cultural resources background report prepared for the City of Brisbane General Plan (Report OS-1) also identified the Roundhouse as an existing historical resource; and

WHEREAS, historic designation will help protect the historic integrity of the roundhouse structure

WHEREAS, historic designation could also promote rehabilitation and restoration of the roundhouse through financial incentives and funding opportunities available to designated historic resources.

NOW, THEREFORE BE IT RESOLVED, that the City Council of the City of Brisbane hereby supports the efforts of San Francisco Trains, a non profit association, to place the Southern Pacific Railroad Roundhouse at Bayshore Yards in Brisbane on the National Register of Historic Places.

  
W. Clarke Conway  
Mayor

I certify that the foregoing Resolution No. 2010-06 was duly and regularly adopted at a regular meeting of the Brisbane City Council on the 19<sup>th</sup> day of January, 2010, by the following vote:

AYES: Councilmembers Bologoff, Lentz, Richardson, Waldo, and Mayor Conway

NOES: None

ABSENT: None

  
Sheri Marie Spedacci  
City Clerk

**Southern Pacific Bayshore Roundhouse  
Brisbane  
San Mateo County  
Staff Report**

The Southern Pacific Bayshore Roundhouse is a semi-circular one-story brick building with associated whisker tracks arranged radially around a central turntable pit. The building is located in the city of Brisbane, San Mateo County, at the southwest corner of the former 200 acre Southern Pacific railyard, and was constructed in 1910. The building encompasses 108 degrees of curvature and is 87 feet deep, from an inside radius of 125 feet to an outside radius of 212 feet. The roundhouse is constructed of brick with a ceiling of wooden beams, and contains 17 enclosed stalls, numbered clockwise from 24 to 40. The roof is radially gabled, clad with pitch and gravel. The building has arched wooden windows. A firewall is located between stalls 32 and 33, and the exterior wall adjacent to stall 24 is also a firewall. The whisker tracks are indicated by light poles at the end of each track. In the center of the roundhouse's radius is a circular concrete turntable pit, 110 feet wide.

The Bayshore Roundhouse is significant under Criterion C as an example of vernacular railroad architecture. Roundhouses were used for the storage and minor maintenance of steam locomotives and typically used in conjunction with a turntable, which was used to turn locomotives and allow access to the roundhouse. The facility was modified several times over the period of its use, including expansion of the turntable pit from 80 to 110 feet and minor modifications to the building's interior. The building's period of significance is 1910 until 1957, from the date of the roundhouse's construction until the year that the facility was last used to store steam locomotives.

The building suffered a fire in 2001 that destroyed the roof and wooden windows between stalls 24 and 32 but was stopped by the brick firewall. The brick, concrete and metal portions of the building remain intact. There is no longer a turntable in the pit, but the pit itself is intact.

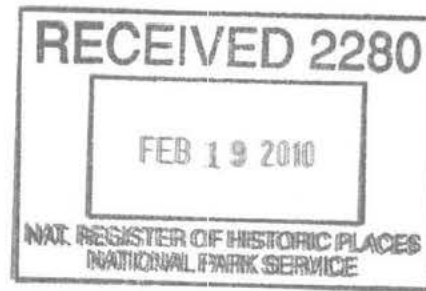
The property is being nominated on behalf of the property owner. Staff supports the nomination as written.

Staff recommends the State Historical Resources Commission determine that the Southern Pacific Bayshore Roundhouse meets National Register Criterion C at the local level of significance and recommends the State Historic Preservation officer approve the nomination for forwarding to the National Park Service for listing in the National Register

William Burg  
Historian I  
January 5, 2010

**OFFICE OF HISTORIC PRESERVATION  
DEPARTMENT OF PARKS AND RECREATION**

P.O. BOX 942896  
SACRAMENTO, CA 94296-0001  
(916) 653-6624 Fax: (916) 653-9824  
calshpo@ohp.parks.ca.gov



February 18, 2010

Ms. Carol Shull, Keeper  
National Register of Historic Places  
National Park Service 2280  
1201 I (Eye) Street, NW  
Washington, DC 20005

Subject: **Southern Pacific Railroad Bayshore Roundhouse  
Brisbane, San Mateo County, California  
National Register of Historic Places**

Dear Ms. Shull: *Carol*

Enclosed please find the **Southern Pacific Railroad Bayshore Roundhouse** nomination to the National Register of Historic Places. This property is located in Brisbane, San Mateo County, California. On January 29, 2010, the State Historical Resources Commission unanimously found the property eligible for the National Register under Criterion C at the local level of significance.

The property is nominated on behalf of the property owners, the Universal Paragon Corporation.

If you have any questions regarding this nomination, please contact William Burg of my staff at 916-653-8936.

Sincerely,

Milford Wayne Donaldson, FAIA  
State Historic Preservation Officer

Enclosures



# Appendix C

## **NAHC and Tribal Correspondence**



## Ashleigh Sims

---

**From:** Ashleigh Sims  
**Sent:** Thursday, February 9, 2023 2:15 PM  
**To:** nahc@nahc.ca.gov  
**Subject:** LGTCL and SLF Search Request - Baylands Specific Plan, San Mateo County  
**Attachments:** 20230209\_Brisbane\_Baylands\_Specific\_Plan.pdf; Fig1\_Brisbane\_Baylands\_Project\_Location.pdf

Hi NAHC,

Attached is a Local Government Tribal Consultation List and Sacred Lands File Search Request for the Baylands Specific Plan in San Mateo County.

Please let me know if you have any questions.

Thanks,  
Ashleigh

**Ashleigh Sims M.A., RPA** (*she/her*)  
Archaeologist

**ESA | Environmental Science Associates**  
1425 N. McDowell Boulevard, Suite 200  
Petaluma, CA 94954

**direct/mobile:** 916.595.5539  
[asims@esassoc.com](mailto:asims@esassoc.com) | [esassoc.com](http://esassoc.com)

Get the latest news: [Stay In Touch](#)

## Local Government Tribal Consultation List Request

### Native American Heritage Commission

1550 Harbor Blvd, Suite 100  
West Sacramento, CA 95691  
916-373-3710  
916-373-5471 – Fax  
[nahe@nahe.ca.gov](mailto:nahe@nahe.ca.gov)

#### Type of List Requested

☐ CEQA Tribal Consultation List (AB 52) – *Per Public Resources Code § 21080.3.1, subs. (b), (d), (e) and 21080.3.2*

☒ General Plan (SB 18) – *Per Government Code § 65352.3.*

#### Local Action Type:

☐

General Plan

☐

General Plan Element

☐

General Plan Amendment

☒

Specific Plan

☐

Specific Plan Amendment

☐

Pre-planning Outreach Activity

#### Required Information

Project Title: Baylands Specific Plan

Local Government/Lead Agency: City of Brisbane

Contact Person: Ashleigh Sims, ESA

Street Address: 1425 N. McDowell Blvd., Suite 200

City: Petaluma Zip: 94954

Phone: 916-595-5539

Fax: \_\_\_\_\_

Email: asims@esassoc.com

#### Specific Area Subject to Proposed Action

County: San Mateo

City/Community: Brisbane

#### Project Description:

The Baylands Specific plan area is between Highway 101 and Bayshore Boulevard in Brisbane, California. The purpose of the specific plan is to provide for the productive reuse of a brownfield site in a manner that eliminates ongoing ecological damage and ensures the safety of all who will use the site. The project is being reviewed under CEQA and is subject to SB 18.

#### Additional Request



Sacred Lands File Search – *Required Information:*

USGS Quadrangle Name(s): San Francisco South

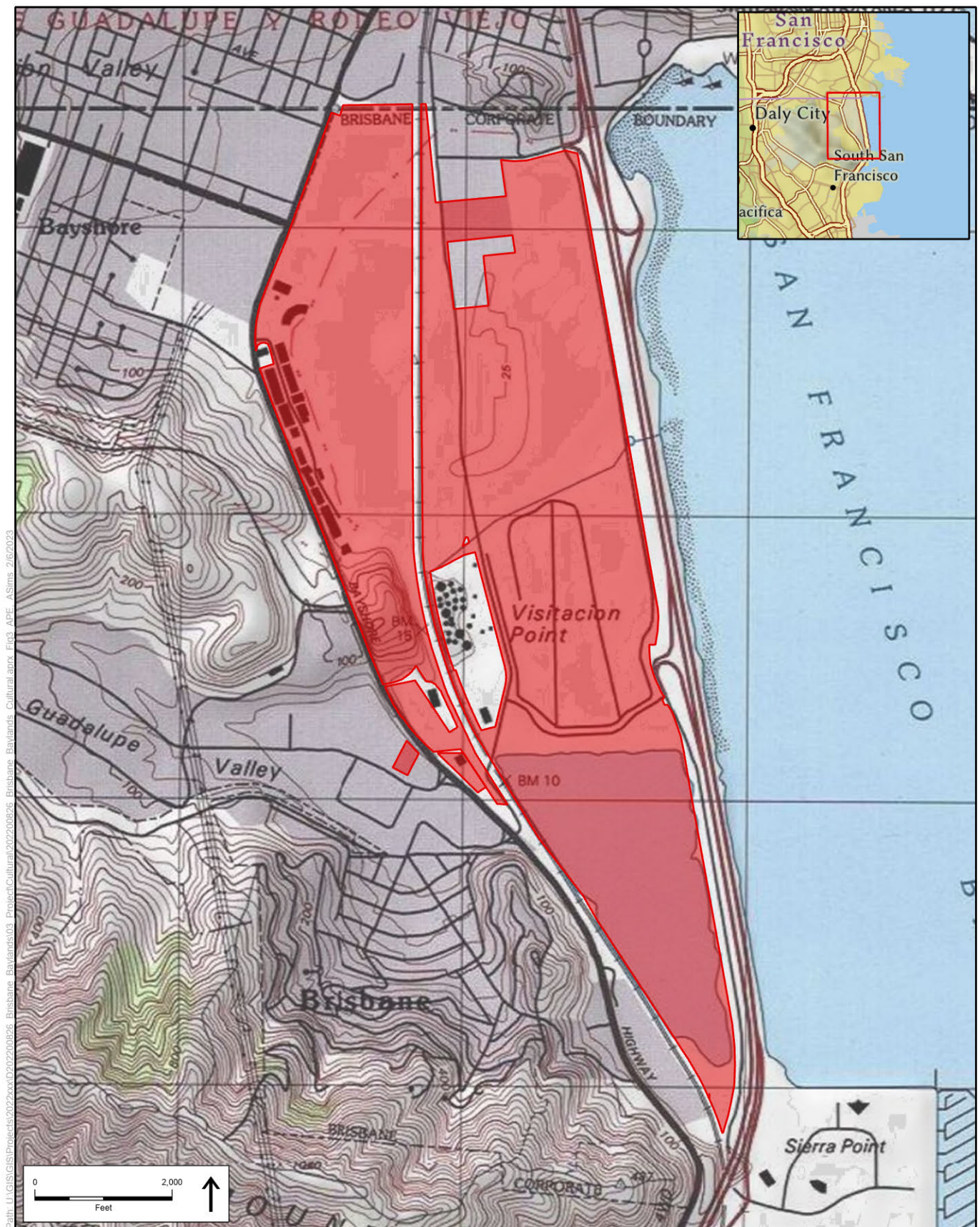
Canada de Guadalupe, Canada de Guadalupe Visitation, and unsectioned fill

Township: n/a

Range: n/a

Section(s): n/a





SOURCE: ESA, 2023; ESRI, 2023

D202200826. Brisbane Baylands

**Figure 1**  
Project Location

## Ashleigh Sims

---

**From:** Campagne, Cody@NAHC <Cody.Campagne@nahc.ca.gov>  
**Sent:** Monday, February 20, 2023 10:33 AM  
**To:** Ashleigh Sims  
**Subject:** Baylands Specific Plan Project  
**Attachments:** SB 18 AB 52 Combo No 3-28-2022.pdf; Baylands Specific Plan 2.20.2023.pdf

Good Morning,

Attached is the response to the project referenced above. If you have any additional questions, please feel free to contact our office email at [nahc@nahc.ca.gov](mailto:nahc@nahc.ca.gov).

Regards,

### **Cody Campagne**

Native American Heritage Commission  
1550 Harbor Blvd., Suite 100  
West Sacramento, CA 95691  
[Cody.Campagne@nahc.ca.gov](mailto:Cody.Campagne@nahc.ca.gov)  
Direct Line: (916) 573-1033  
Office: (916) 373-3710





## NATIVE AMERICAN HERITAGE COMMISSION

February 20, 2023

Ashleigh Sims  
ESA

Via Email to: [asims@esassoc.com](mailto:asims@esassoc.com)

CHAIRPERSON  
**Laura Miranda**  
Luiseño

VICE CHAIRPERSON  
**Reginald Pagaling**  
Chumash

SECRETARY  
**Sara Dutschke**  
Miwok

COMMISSIONER  
**Isaac Bojorquez**  
Ohlone-Costanoan

COMMISSIONER  
**Buffy McQuillen**  
Yokayo Pomo, Yuki,  
Nomlaki

COMMISSIONER  
**Wayne Nelson**  
Luiseño

COMMISSIONER  
**Stanley Rodriguez**  
Kumeyaay

COMMISSIONER  
**[Vacant]**

COMMISSIONER  
**[Vacant]**

EXECUTIVE SECRETARY  
**Raymond C.  
Hitchcock**  
Miwok/Nisenan

**NAHC HEADQUARTERS**  
1550 Harbor Boulevard  
Suite 100  
West Sacramento,  
California 95691  
(916) 373-3710  
[nahc@nahc.ca.gov](mailto:nahc@nahc.ca.gov)  
[NAHC.ca.gov](http://NAHC.ca.gov)

**Re: Native American Consultation, Pursuant to Senate Bill 18 (SB18), Government Codes §65352.3 and §65352.4, as well as Assembly Bill 52 (AB52), Public Resources Codes §21080.1, §21080.3.1 and §21080.3.2, Baylands Specific Plan Project, San Mateo County**

Dear Ms. Sims:

Attached is a consultation list of tribes with traditional lands or cultural places located within the boundaries of the above referenced counties or projects.

Government Codes §65352.3 and §65352.4 require local governments to consult with California Native American tribes identified by the Native American Heritage Commission (NAHC) for the purpose of avoiding, protecting, and/or mitigating impacts to cultural places when creating or amending General Plans, Specific Plans and Community Plans.

Public Resources Codes §21080.3.1 and §21080.3.2 requires public agencies to consult with California Native American tribes identified by the Native American Heritage Commission (NAHC) for the purpose of avoiding, protecting, and/or mitigating impacts to tribal cultural resources as defined, for California Environmental Quality Act (CEQA) projects.

The law does not preclude local governments and agencies from initiating consultation with the tribes that are culturally and traditionally affiliated within your jurisdiction. The NAHC believes that this is the best practice to ensure that tribes are consulted commensurate with the intent of the law.

Best practice for the AB52 process and in accordance with Public Resources Code §21080.3.1(d), is to do the following:

*Within 14 days of determining that an application for a project is complete or a decision by a public agency to undertake a project, the lead agency shall provide formal notification to the designated contact of, or a tribal representative of, traditionally and culturally affiliated California Native American tribes that have requested notice, which shall be accomplished by means of at least one written notification that includes a brief description of the proposed project and its location, the lead agency contact information, and a notification that the California Native American tribe has 30 days to request consultation pursuant to this section.*

The NAHC also recommends, but does not require that lead agencies include in their notification letters, information regarding any cultural resources assessment that has been completed on the area of potential affect (APE), such as:



1. The results of any record search that may have been conducted at an Information Center of the California Historical Resources Information System (CHRIS), including, but not limited to:
  - A listing of any and all known cultural resources have already been recorded on or adjacent to the APE, such as known archaeological sites;
  - Copies of any and all cultural resource records and study reports that may have been provided by the Information Center as part of the records search response;
  - Whether the records search indicates a low, moderate or high probability that unrecorded cultural resources are located in the APE; and
  - If a survey is recommended by the Information Center to determine whether previously unrecorded cultural resources are present.
2. The results of any archaeological inventory survey that was conducted, including:
  - Any report that may contain site forms, site significance, and suggested mitigation measures.

All information regarding site locations, Native American human remains, and associated funerary objects should be in a separate confidential addendum, and not be made available for public disclosure in accordance with Government Code Section 6254.10.
3. The result of the Sacred Lands File (SFL) check conducted through the Native American Heritage Commission was negative.
4. Any ethnographic studies conducted for any area including all or part of the potential APE; and
5. Any geotechnical reports regarding all or part of the potential APE.

Lead agencies should be aware that records maintained by the NAHC and CHRIS is not exhaustive, and a negative response to these searches does not preclude the existence of a tribal cultural resource. A tribe may be the only source of information regarding the existence of a tribal cultural resource.

This information will aid tribes in determining whether to request formal consultation. In the event, that they do, having the information beforehand will help to facilitate the consultation process.

If you receive notification of change of addresses and phone numbers from tribes, please notify the NAHC. With your assistance we can assure that our consultation list remains current.

If you have any questions, please contact me at my email address:

[Cody.Campagne@nahc.ca.gov](mailto:Cody.Campagne@nahc.ca.gov).

Sincerely,



Cody Campagne  
Cultural Resources Analyst

Attachment

**Native American Heritage Commission  
Tribal Consultation List  
San Mateo County  
2/20/2023**

***Amah Mutsun Tribal Band of  
Mission San Juan Bautista***

Irene Zwierlein, Chairperson  
3030 Soda Bay Road Costanoan  
Lakeport, CA, 95453  
Phone: (650) 851 - 7489  
Fax: (650) 332-1526  
amahmutsuntribal@gmail.com

***The Ohlone Indian Tribe***

Andrew Galvan, Chairperson  
P.O. Box 3388 Bay Miwok  
Fremont, CA, 94539 Ohlone  
Phone: (510) 882 - 0527 Patwin  
Fax: (510) 687-9393 Plains Miwok  
chochenyo@AOL.com

***Costanoan Rumsen Carmel  
Tribe***

Tony Cerda, Chairperson  
244 E. 1st Street Costanoan  
Pomona, CA, 91766  
Phone: (909) 629 - 6081  
Fax: (909) 524-8041  
rumsen@aol.com

***Wuksache Indian Tribe/Eshom  
Valley Band***

Kenneth Woodrow, Chairperson  
1179 Rock Haven Ct. Foothill Yokut  
Salinas, CA, 93906 Mono  
Phone: (831) 443 - 9702  
kwood8934@aol.com

***Indian Canyon Mutsun Band of  
Costanoan***

Kanyon Sayers-Roods, MLD  
Contact  
1615 Pearson Court Costanoan  
San Jose, CA, 95122  
Phone: (408) 673 - 0626  
kanyon@kanyonconsulting.com

***Indian Canyon Mutsun Band of  
Costanoan***

Ann Marie Sayers, Chairperson  
P.O. Box 28 Costanoan  
Hollister, CA, 95024  
Phone: (831) 637 - 4238  
ams@indiancanyon.org

***Muwekma Ohlone Indian Tribe  
of the SF Bay Area***

Monica Arellano, Vice  
Chairwoman  
20885 Redwood Road, Suite 232 Costanoan  
Castro Valley, CA, 94546  
Phone: (408) 205 - 9714  
monicavarellano@gmail.com

***Muwekma Ohlone Indian Tribe  
of the SF Bay Area***

Charlene Nijmeh, Chairperson  
20885 Redwood Road, Suite 232 Costanoan  
Castro Valley, CA, 94546  
Phone: (408) 464 - 2892  
cnijmeh@muwekma.org

This list is current only as of the date of this document and is based on the information available to the Commission on the date it was produced. Distribution of this list does not relieve any person of statutory responsibility as defined in Section 7050.5 of the Health and Safety Code, Section 5097.94 of the Public Resources Code and Section 5097.98 of the Public Resources Code.

This list is applicable only for consultation with Native American tribes under Government Code Sections 65352.3, 65352.4 et seq. and Public Resources Code Sections 21080.3.1 for the proposed Baylands Specific Plan Project, San Mateo County.



**City of Brisbane**  
50 Park Place  
Brisbane, CA 94005-1310  
(415) 508-2100  
(415) 467-4989 Fax

August 3, 2022

Muwekma Ohlone Indian Tribe of the SF Bay Area  
Attn: Monica Arellano, Vice Chairwoman  
20885 Redwood Road, Suite 232  
Castro Valley, CA 94546

Re: Assembly Bill 52 Consultation for the Proposed Brisbane Baylands and Sierra Point Towers Projects, San Mateo County, California

Dear Vice Chairwoman Arellano,

The City of Brisbane (City) has received applications for the proposed Brisbane Baylands Specific Plan and Sierra Point Towers Projects. Summaries of both projects are provided below. Additional information for each project can be found on the City's webpage, which can be accessed at: <https://www.brisbaneca.org/projects> and selecting either "Brisbane Baylands" or "Sierra Point Towers" in the list of projects. A map of the project sites is provided as an attachment.

- The Baylands Specific Plan would allow for development of 2,200 dwelling units, 6.5 million square feet of commercial office development with an additional 500,000 square feet of hotel use; acquisition of a water supply; extension of Geneva Avenue east from Bayshore Boulevard including a bridge over the Caltrain right-of-way; and associated onsite and offsite infrastructure. The Specific Plan provides for preservation of 141.6 acres of the site's 500.2 upland acreage in open space, including habitat preservation and enhancement as well as active and passive recreational areas. The balance of the Baylands consists of the 115.8-acre Brisbane Lagoon and an additional 26 acres of existing upland area that will be inundated by sea level rise through the year 2100.]
- The proposed Sierra Point Towers Project (located at 2000 Sierra Point Parkway and 8000 Marina Boulevard within the City of Brisbane) consists of the demolition of the existing parking garage and surface parking lots and construction of two new office/life sciences buildings, an amenity building, and a parking structure, totaling approximately 863,220 square feet of new building construction. The existing 427,283 square feet of office building space on site would be retained, for a total of approximately 1,290,503 square feet of office/life science uses, and a total of 3,858 parking spaces.

The City is the Lead Agency overseeing preparation of the environmental review documentation to be prepared pursuant to the California Environmental Quality Act (CEQA). Separate Environmental Impact Reports (EIRs) will be prepared for each project.



Assembly Bill 52 (AB 52), which became law January 1, 2015, requires that, as part of the CEQA review process, public agencies provide early notice of a project to California Native American Tribes, and if formally requested in writing by a Tribe, consult with Tribes in regards to the project. The Native American Heritage Commission (NAHC) has identified your Tribe as traditionally and culturally affiliated with the geographic area of the above projects and is entitled to consult with the City pursuant to AB 52. Via this letter, the City is hereby providing notice to all Tribes identified by the NAHC as eligible to consult with the City for both the Brisbane Baylands and Sierra Point Towers Projects.

The purpose of AB 52 and the consultation, if requested, is to identify and consider potential impacts to Tribal Cultural Resources (TCRs),<sup>1</sup> and take into account tribal cultural values in addition to scientific and archaeological values when determining possible impacts and mitigation. An impact to a TCR may result in a significant impact under CEQA which would require avoidance or minimization of the impact.

To request consultation under AB 52 for the Brisbane Baylands and/or Sierra Point Towers Project, please contact me, John Swiecki, Community Development Director, in writing within 30 days of receipt of this letter. Should a response from you not be received within this timeframe, the City may not be required to enter into consultation with you on this matter.

I can be reached by email at [johnswiecki@ci.brisbane.ca.us](mailto:johnswiecki@ci.brisbane.ca.us) or by mail at:

John Swiecki, Community Development Director  
City of Brisbane  
Community Development Department  
50 Park Place  
Brisbane, CA 94005

Thank you for your consideration of this request.

Kind Regards,

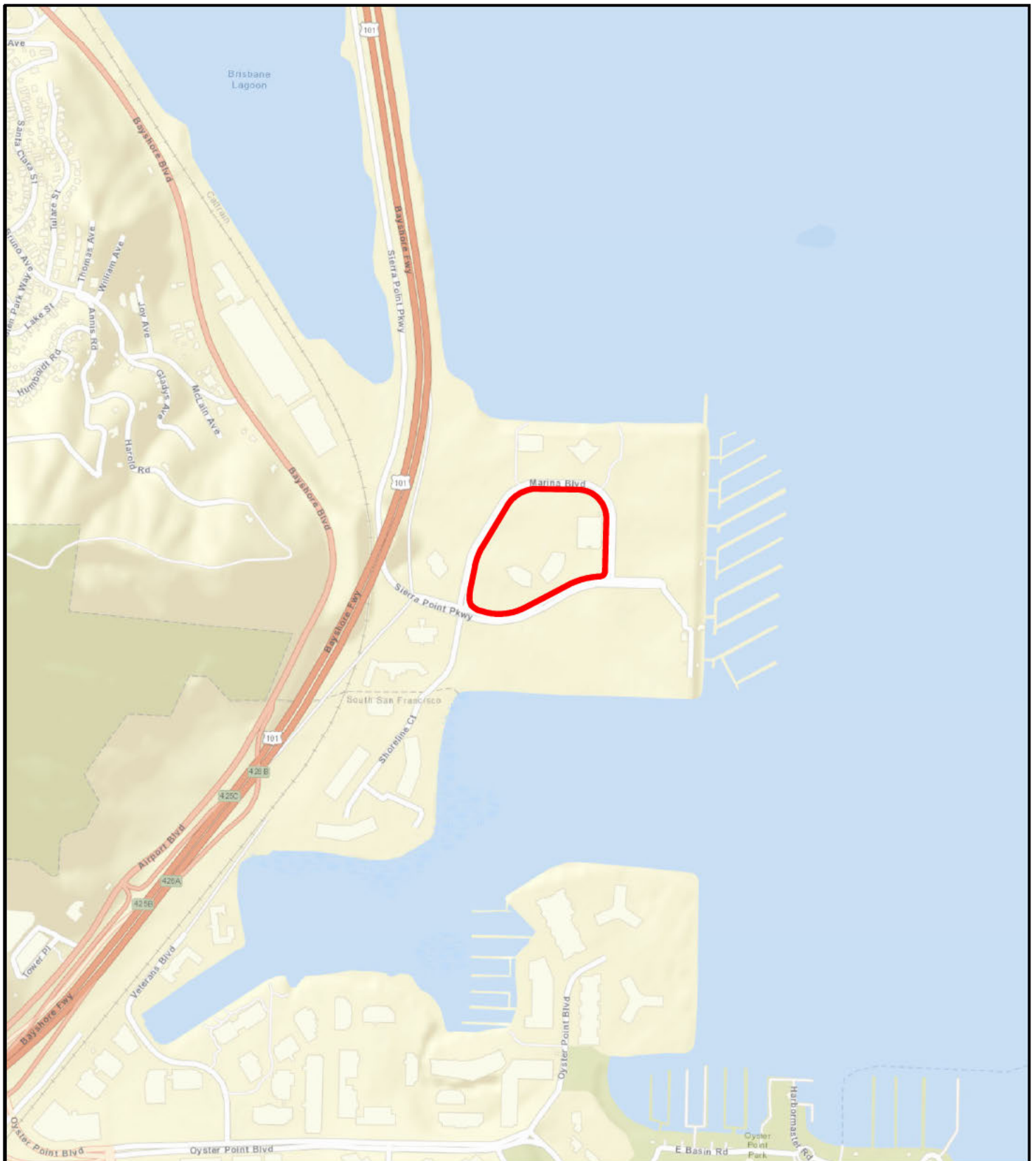


John Swiecki, Community Development Director

Attachment: Project Locations Map

---

<sup>1</sup> Public Resources Code (PRC) Section 21074(a) defines Tribal Cultural Resources as either of the following: (1) Sites, features, places, cultural landscapes, sacred places, and objects with cultural value to a California Native American tribe that are either: (A) included or determined to be eligible for inclusion in the California Register of Historical Resources; or (B) included in a local register of historical resources as defined in subdivision (k) of PRC Section 5020.1; or (2) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of PRC Section 5024.1.



LSA

LEGEND

  Project Location



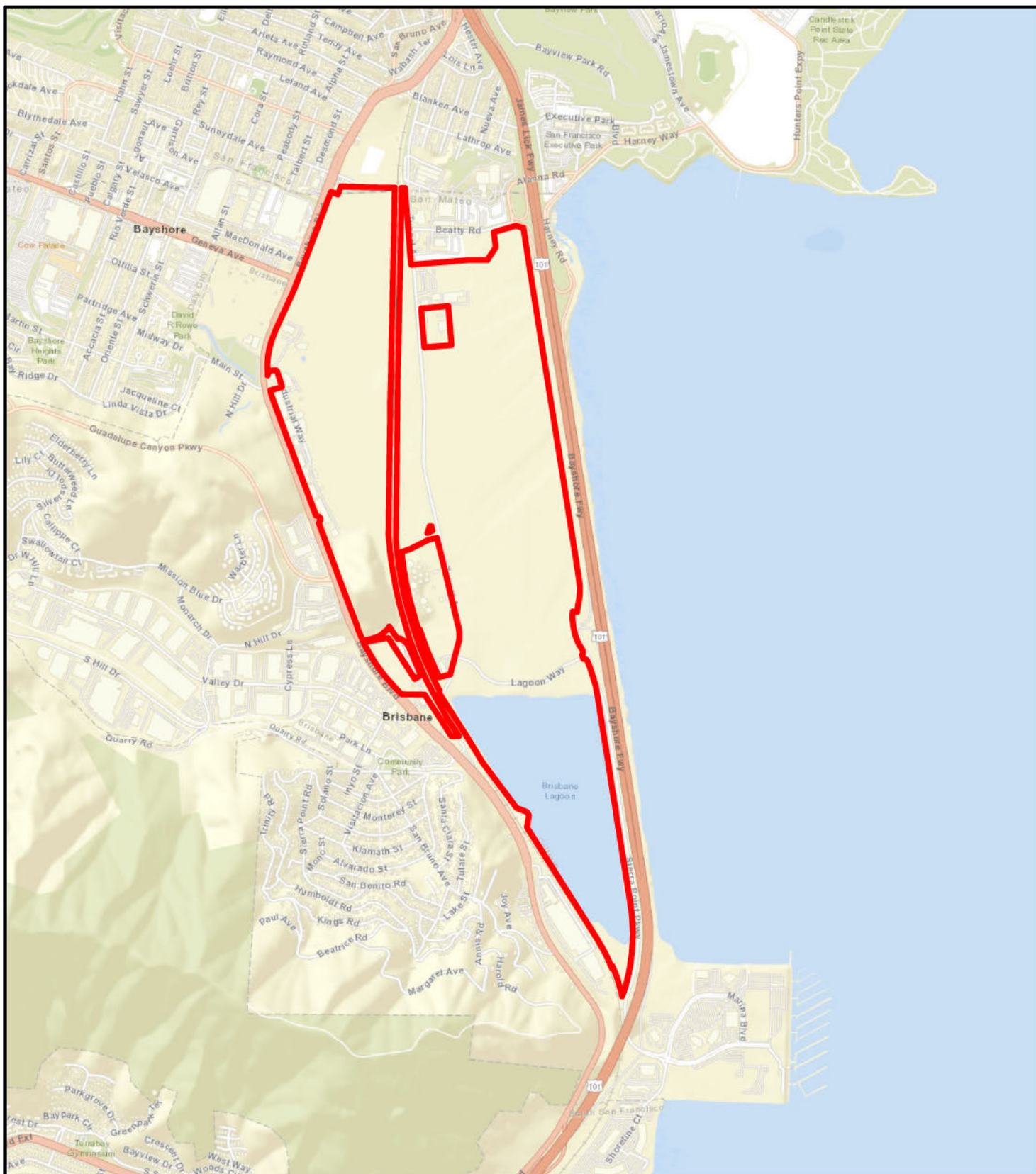
0 500 1000  
FEET

SOURCE: Esri World Street Map (2021)

I:\BRI2102\GIS\MXD\Project\_Location\_SierraPointTowers.mxd (6/29/2022)

FIGURE 1

Sierra Point Towers Project  
Project Location



**LSA**

**LEGEND**

Baylands Specific Plan Boundary



0 1000 2000  
FEET

SOURCE: Esri World Street Map (2021)

I:\BRI2102\GIS\MXD\Project\_Location\_BrisbaneBaylands.mxd (6/29/2022)

**FIGURE 2**

*Brisbane Baylands Project*  
**Project Location**





**City of Brisbane**  
50 Park Place  
Brisbane, CA 94005-1310  
(415) 508-2100  
(415) 467-4989 Fax

August 3, 2022

Costanoan Rumsen Carmel Tribe  
Attn: Tony Cerda, Chairperson  
244 E. 1st Street  
Pomona, CA, 91766

Re: Assembly Bill 52 Consultation for the Proposed Brisbane Baylands and Sierra Point Towers Projects, San Mateo County, California

Dear Chairman Cerda,

The City of Brisbane (City) has received applications for the proposed Brisbane Baylands Specific Plan and Sierra Point Towers Projects. Summaries of both projects are provided below. Additional information for each project can be found on the City's webpage, which can be accessed at: <https://www.brisbaneca.org/projects> and selecting either "Brisbane Baylands" or "Sierra Point Towers" in the list of projects. A map of the project sites is provided as an attachment.

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- The proposed Sierra Point Towers Project (located at 2000 Sierra Point Parkway and 8000 Marina Boulevard within the City of Brisbane) consists of the demolition of the existing parking garage and surface parking lots and construction of two new office/life sciences buildings, an amenity building, and a parking structure, totaling approximately 863,220 square feet of new building construction. The existing 427,283 square feet of office building space on site would be retained, for a total of approximately 1,290,503 square feet of office/life science uses, and a total of 3,858 parking spaces.

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Assembly Bill 52 (AB 52), which became law January 1, 2015, requires that, as part of the CEQA review process, public agencies provide early notice of a project to California Native American Tribes, and if formally requested in writing by a Tribe, consult with Tribes in regards to the project. The Native American Heritage Commission (NAHC) has identified your Tribe as traditionally and culturally affiliated with the geographic area of the above projects and is entitled to consult with the City pursuant to AB 52. Via this letter, the City is hereby providing notice to all Tribes identified by the NAHC as eligible to consult with the City for both the Brisbane Baylands and Sierra Point Towers Projects.

The purpose of AB 52 and the consultation, if requested, is to identify and consider potential impacts to Tribal Cultural Resources (TCRs),<sup>1</sup> and take into account tribal cultural values in addition to scientific and archaeological values when determining possible impacts and mitigation. An impact to a TCR may result in a significant impact under CEQA which would require avoidance or minimization of the impact.

To request consultation under AB 52 for the Brisbane Baylands and/or Sierra Point Towers Project, please contact me, John Swiecki, Community Development Director, in writing within 30 days of receipt of this letter. Should a response from you not be received within this timeframe, the City may not be required to enter into consultation with you on this matter.

I can be reached by email at [johnswiecki@ci.brisbane.ca.us](mailto:johnswiecki@ci.brisbane.ca.us) or by mail at:

John Swiecki, Community Development Director  
City of Brisbane  
Community Development Department  
50 Park Place  
Brisbane, CA 94005

Thank you for your consideration of this request.

Kind Regards,

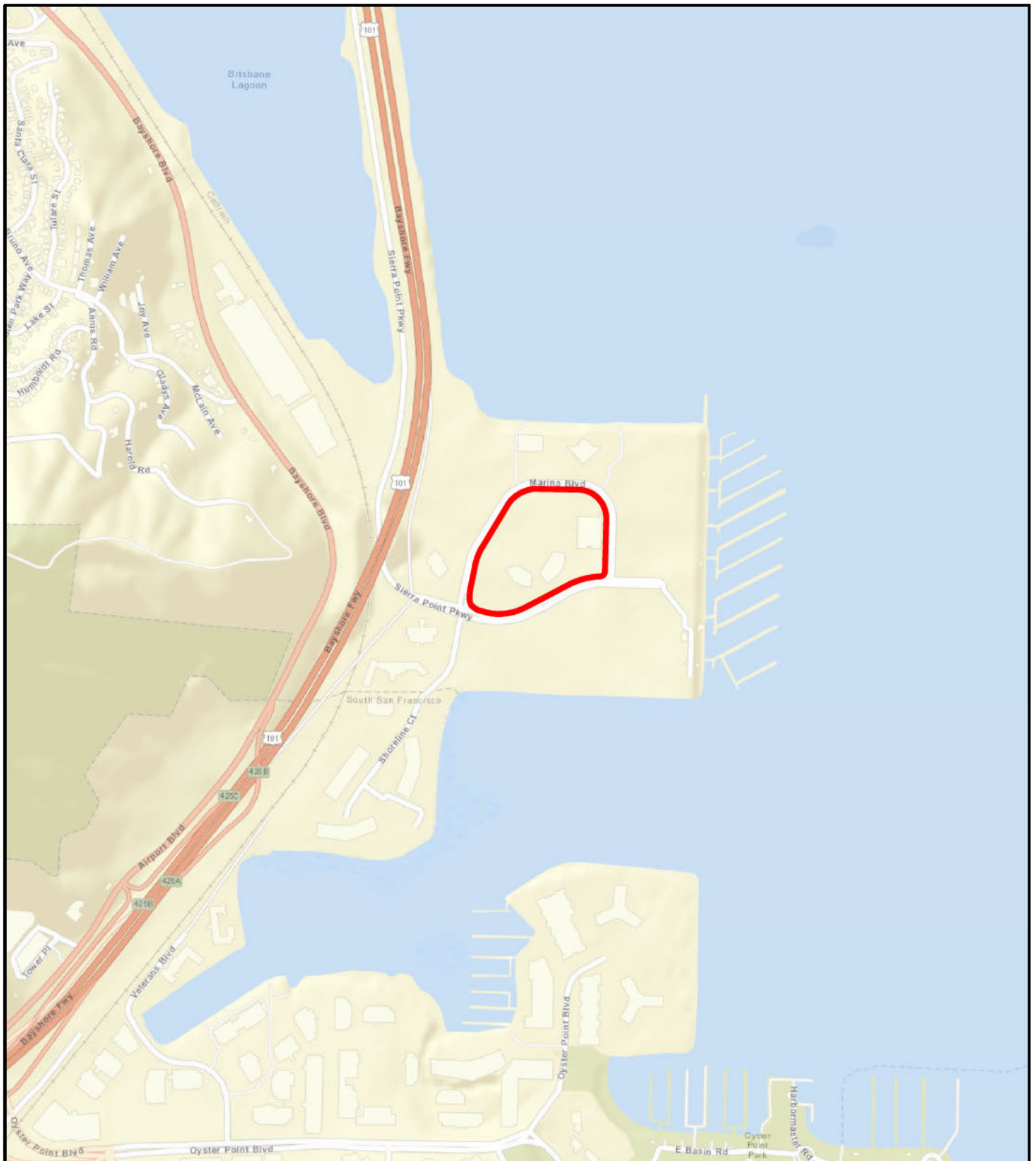


John Swiecki, Community Development Director

Attachment: Project Locations Map

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LSA

LEGEND

Project Location



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FEET

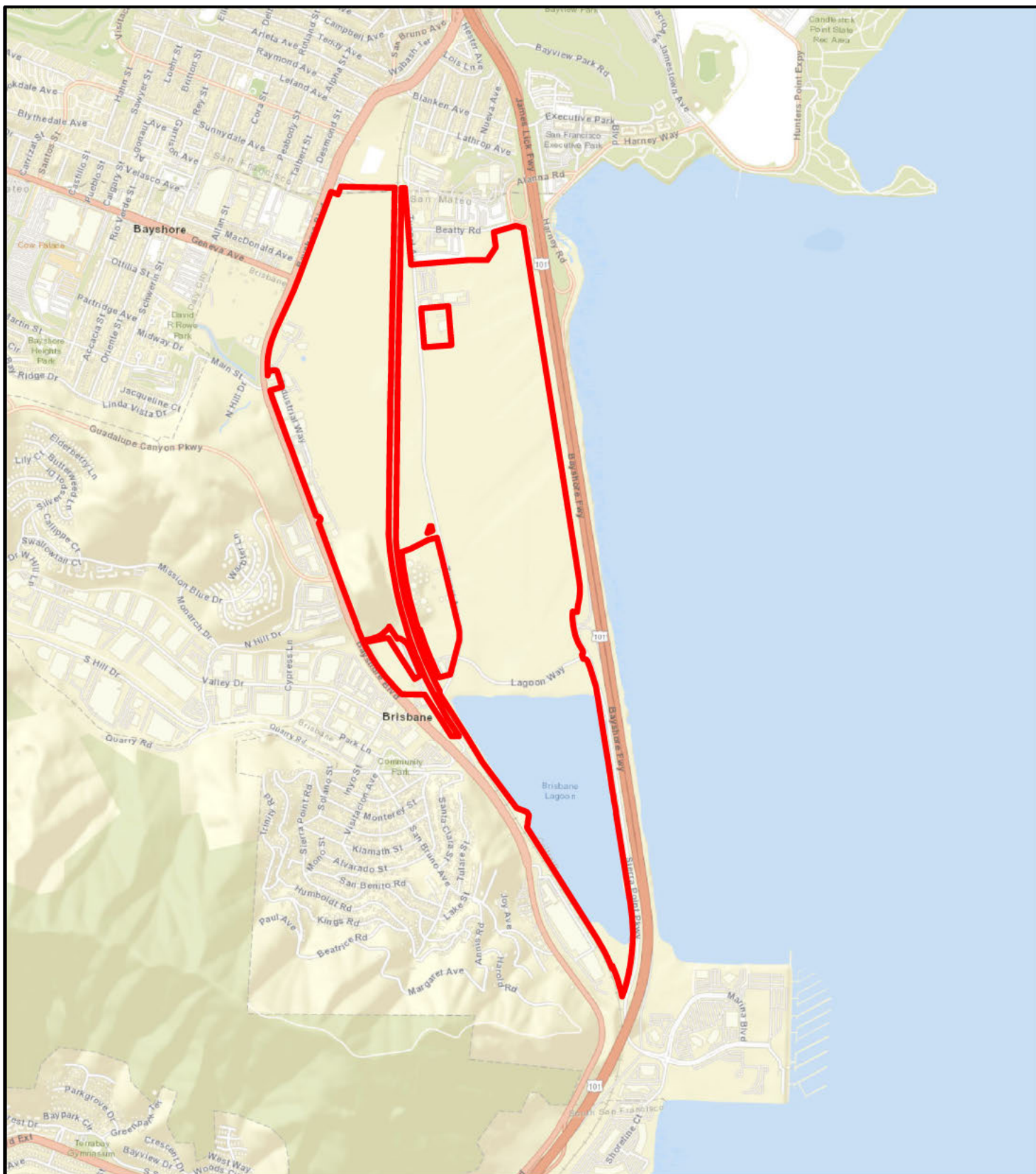
SOURCE: Esri World Street Map (2021)

I:\BRI2102\GIS\MXD\Project\_Location\_SierraPointTowers.mxd (6/29/2022)

FIGURE 1

Sierra Point Towers Project  
Project Location





**LSA**

**LEGEND**

Baylands Specific Plan Boundary



0 1000 2000  
FEET

SOURCE: Esri World Street Map (2021)

I:\BRI2102\GIS\MXD\Project\_Location\_BrisbaneBaylands.mxd (6/29/2022)

**FIGURE 2**

*Brisbane Baylands Project*  
**Project Location**



**City of Brisbane**  
50 Park Place  
Brisbane, CA 94005-1310  
(415) 508-2100  
(415) 467-4989 Fax

August 3, 2022

The Ohlone Indian Tribe  
Andrew Galvan  
P.O. Box 3388  
Fremont, CA 94539

Re: Assembly Bill 52 Consultation for the Proposed Brisbane Baylands and Sierra Point Towers Projects, San Mateo County, California

Dear Mr. Galvan,

The City of Brisbane (City) has received applications for the proposed Brisbane Baylands Specific Plan and Sierra Point Towers Projects. Summaries of both projects are provided below. Additional information for each project can be found on the City's webpage, which can be accessed at: <https://www.brisbaneca.org/projects> and selecting either "Brisbane Baylands" or "Sierra Point Towers" in the list of projects. A map of the project sites is provided as an attachment.

- The Baylands Specific Plan would allow for development of 2,200 dwelling units, 6.5 million square feet of commercial office development with an additional 500,000 square feet of hotel use; acquisition of a water supply; extension of Geneva Avenue east from Bayshore Boulevard including a bridge over the Caltrain right-of-way; and associated onsite and offsite infrastructure. The Specific Plan provides for preservation of 141.6 acres of the site's 500.2 upland acreage in open space, including habitat preservation and enhancement as well as active and passive recreational areas. The balance of the Baylands consists of the 115.8-acre Brisbane Lagoon and an additional 26 acres of existing upland area that will be inundated by sea level rise through the year 2100.]
- The proposed Sierra Point Towers Project (located at 2000 Sierra Point Parkway and 8000 Marina Boulevard within the City of Brisbane) consists of the demolition of the existing parking garage and surface parking lots and construction of two new office/life sciences buildings, an amenity building, and a parking structure, totaling approximately 863,220 square feet of new building construction. The existing 427,283 square feet of office building space on site would be retained, for a total of approximately 1,290,503 square feet of office/life science uses, and a total of 3,858 parking spaces.

The City is the Lead Agency overseeing preparation of the environmental review documentation to be prepared pursuant to the California Environmental Quality Act (CEQA). Separate Environmental Impact Reports (EIRs) will be prepared for each project.

Assembly Bill 52 (AB 52), which became law January 1, 2015, requires that, as part of the CEQA review process, public agencies provide early notice of a project to California Native American Tribes, and if formally requested in writing by a Tribe, consult with Tribes in regards to the project. The Native American Heritage Commission (NAHC) has identified your Tribe as traditionally and culturally affiliated with the geographic area of the above projects and is entitled to consult with the City pursuant to AB 52. Via this letter, the City is hereby providing notice to all Tribes identified by the NAHC as eligible to consult with the City for both the Brisbane Baylands and Sierra Point Towers Projects.

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To request consultation under AB 52 for the Brisbane Baylands and/or Sierra Point Towers Project, please contact me, John Swiecki, Community Development Director, in writing within 30 days of receipt of this letter. Should a response from you not be received within this timeframe, the City may not be required to enter into consultation with you on this matter.

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John Swiecki, Community Development Director  
City of Brisbane  
Community Development Department  
50 Park Place  
Brisbane, CA 94005

Thank you for your consideration of this request.

Kind Regards,



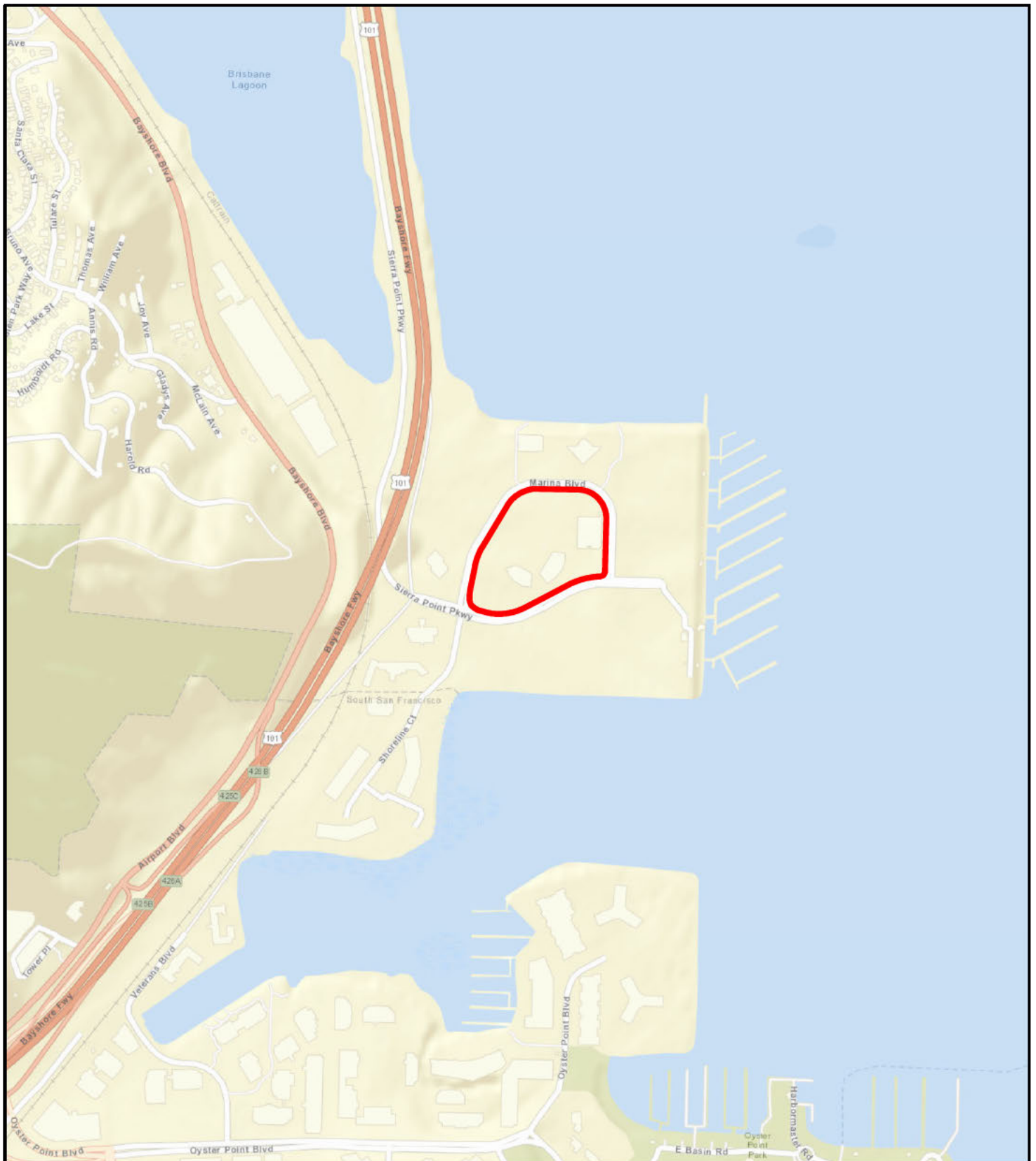
John Swiecki, Community Development Director

Attachment: Project Locations Map

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LSA

LEGEND

  Project Location



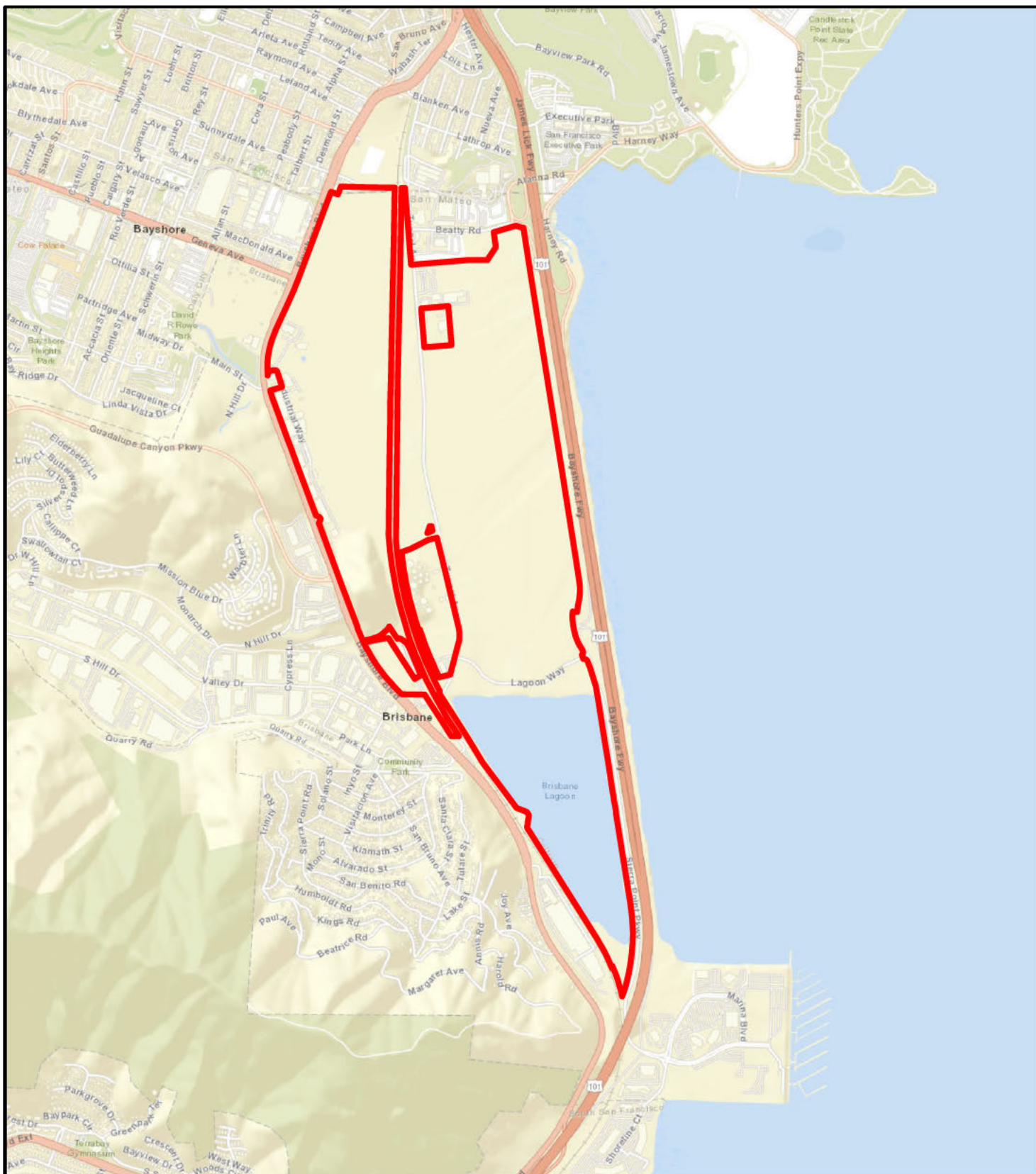
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FEET

SOURCE: Esri World Street Map (2021)

I:\BRI2102\GIS\MXD\Project\_Location\_SierraPointTowers.mxd (6/29/2022)

FIGURE 1

Sierra Point Towers Project  
Project Location



**LSA**

**LEGEND**

Baylands Specific Plan Boundary



0 1000 2000  
FEET

SOURCE: Esri World Street Map (2021)

I:\BRI2102\GIS\MXD\Project\_Location\_BrisbaneBaylands.mxd (6/29/2022)

**FIGURE 2**

*Brisbane Baylands Project*  
**Project Location**



**City of Brisbane**  
50 Park Place  
Brisbane, CA 94005-1310  
(415) 508-2100  
(415) 467-4989 Fax

August 3, 2022

Muwekma Ohlone Indian Tribe of the SF Bay Area  
Charlene Nijmeh, Chairperson  
20885 Redwood Road, Suite 232  
Castro Valley, CA, 94546

Re: Assembly Bill 52 Consultation for the Proposed Brisbane Baylands and Sierra Point Towers Projects, San Mateo County, California

Dear Chairwoman Nijmeh,

The City of Brisbane (City) has received applications for the proposed Brisbane Baylands Specific Plan and Sierra Point Towers Projects. Summaries of both projects are provided below. Additional information for each project can be found on the City's webpage, which can be accessed at: <https://www.brisbaneca.org/projects> and selecting either "Brisbane Baylands" or "Sierra Point Towers" in the list of projects. A map of the project sites is provided as an attachment.

- The Baylands Specific Plan would allow for development of 2,200 dwelling units, 6.5 million square feet of commercial office development with an additional 500,000 square feet of hotel use; acquisition of a water supply; extension of Geneva Avenue east from Bayshore Boulevard including a bridge over the Caltrain right-of-way; and associated onsite and offsite infrastructure. The Specific Plan provides for preservation of 141.6 acres of the site's 500.2 upland acreage in open space, including habitat preservation and enhancement as well as active and passive recreational areas. The balance of the Baylands consists of the 115.8-acre Brisbane Lagoon and an additional 26 acres of existing upland area that will be inundated by sea level rise through the year 2100.]
- The proposed Sierra Point Towers Project (located at 2000 Sierra Point Parkway and 8000 Marina Boulevard within the City of Brisbane) consists of the demolition of the existing parking garage and surface parking lots and construction of two new office/life sciences buildings, an amenity building, and a parking structure, totaling approximately 863,220 square feet of new building construction. The existing 427,283 square feet of office building space on site would be retained, for a total of approximately 1,290,503 square feet of office/life science uses, and a total of 3,858 parking spaces.

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To request consultation under AB 52 for the Brisbane Baylands and/or Sierra Point Towers Project, please contact me, John Swiecki, Community Development Director, in writing within 30 days of receipt of this letter. Should a response from you not be received within this timeframe, the City may not be required to enter into consultation with you on this matter.

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John Swiecki, Community Development Director  
City of Brisbane  
Community Development Department  
50 Park Place  
Brisbane, CA 94005

Thank you for your consideration of this request.

Kind Regards,

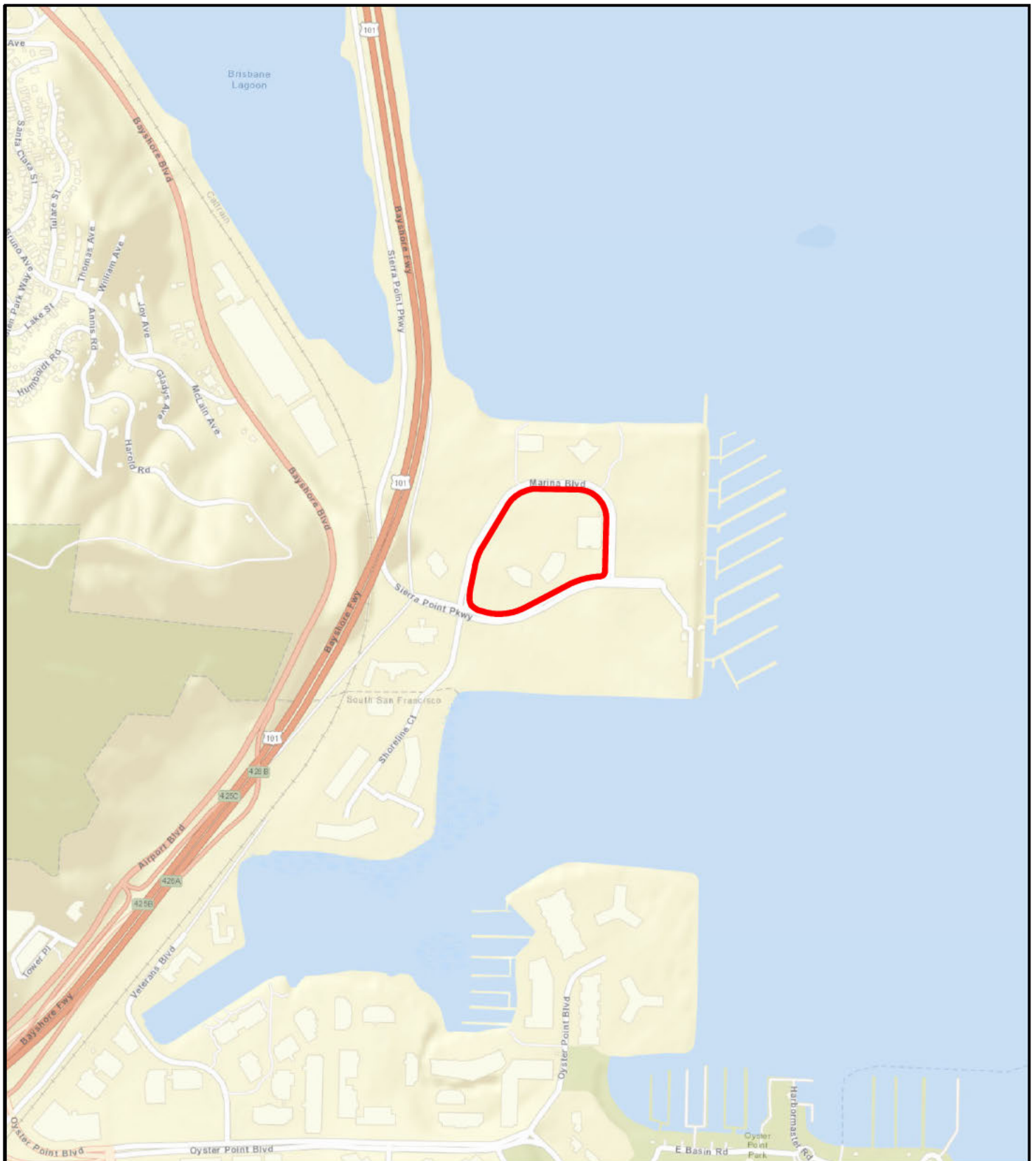


John Swiecki, Community Development Director

Attachment: Project Locations Map

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LSA

# LEGEND

Project Location



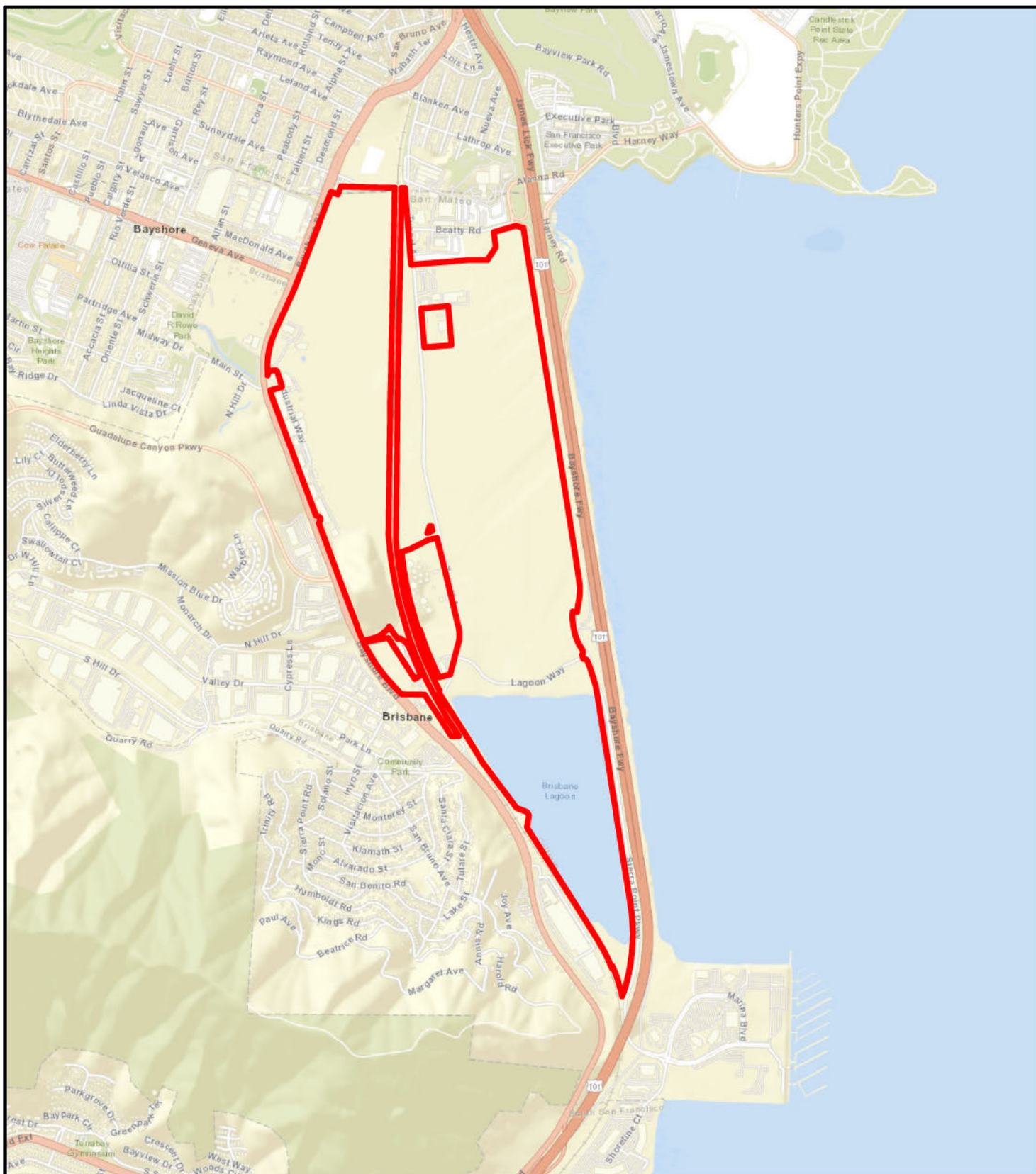
0 500 1000  
FEET

SOURCE: Esri World Street Map (2021)

I:\BRI2102\GIS\MXD\Project\_Location\_SierraPointTowers.mxd (6/29/2022)

FIGURE 1

Sierra Point Towers Project  
Project Location



**LSA**

**LEGEND**

Baylands Specific Plan Boundary



0 1000 2000  
FEET

SOURCE: Esri World Street Map (2021)

I:\BRI2102\GIS\MXD\Project\_Location\_BrisbaneBaylands.mxd (6/29/2022)

**FIGURE 2**

*Brisbane Baylands Project*  
**Project Location**





**City of Brisbane**  
50 Park Place  
Brisbane, CA 94005-1310  
(415) 508-2100  
(415) 467-4989 Fax

August 3, 2022

Indian Canyon Mutsun Band of Costanoan  
Ann Marie Sayers, Chairperson  
P.O. Box 28  
Hollister, CA 95024

Re: Assembly Bill 52 Consultation for the Proposed Brisbane Baylands and Sierra Point Towers Projects, San Mateo County, California

Dear Chairwoman Sayers,

The City of Brisbane (City) has received applications for the proposed Brisbane Baylands Specific Plan and Sierra Point Towers Projects. Summaries of both projects are provided below. Additional information for each project can be found on the City's webpage, which can be accessed at: <https://www.brisbaneca.org/projects> and selecting either "Brisbane Baylands" or "Sierra Point Towers" in the list of projects. A map of the project sites is provided as an attachment.

- The Baylands Specific Plan would allow for development of 2,200 dwelling units, 6.5 million square feet of commercial office development with an additional 500,000 square feet of hotel use; acquisition of a water supply; extension of Geneva Avenue east from Bayshore Boulevard including a bridge over the Caltrain right-of-way; and associated onsite and offsite infrastructure. The Specific Plan provides for preservation of 141.6 acres of the site's 500.2 upland acreage in open space, including habitat preservation and enhancement as well as active and passive recreational areas. The balance of the Baylands consists of the 115.8-acre Brisbane Lagoon and an additional 26 acres of existing upland area that will be inundated by sea level rise through the year 2100.]
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To request consultation under AB 52 for the Brisbane Baylands and/or Sierra Point Towers Project, please contact me, John Swiecki, Community Development Director, in writing within 30 days of receipt of this letter. Should a response from you not be received within this timeframe, the City may not be required to enter into consultation with you on this matter.

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John Swiecki, Community Development Director  
City of Brisbane  
Community Development Department  
50 Park Place  
Brisbane, CA 94005

Thank you for your consideration of this request.

Kind Regards,

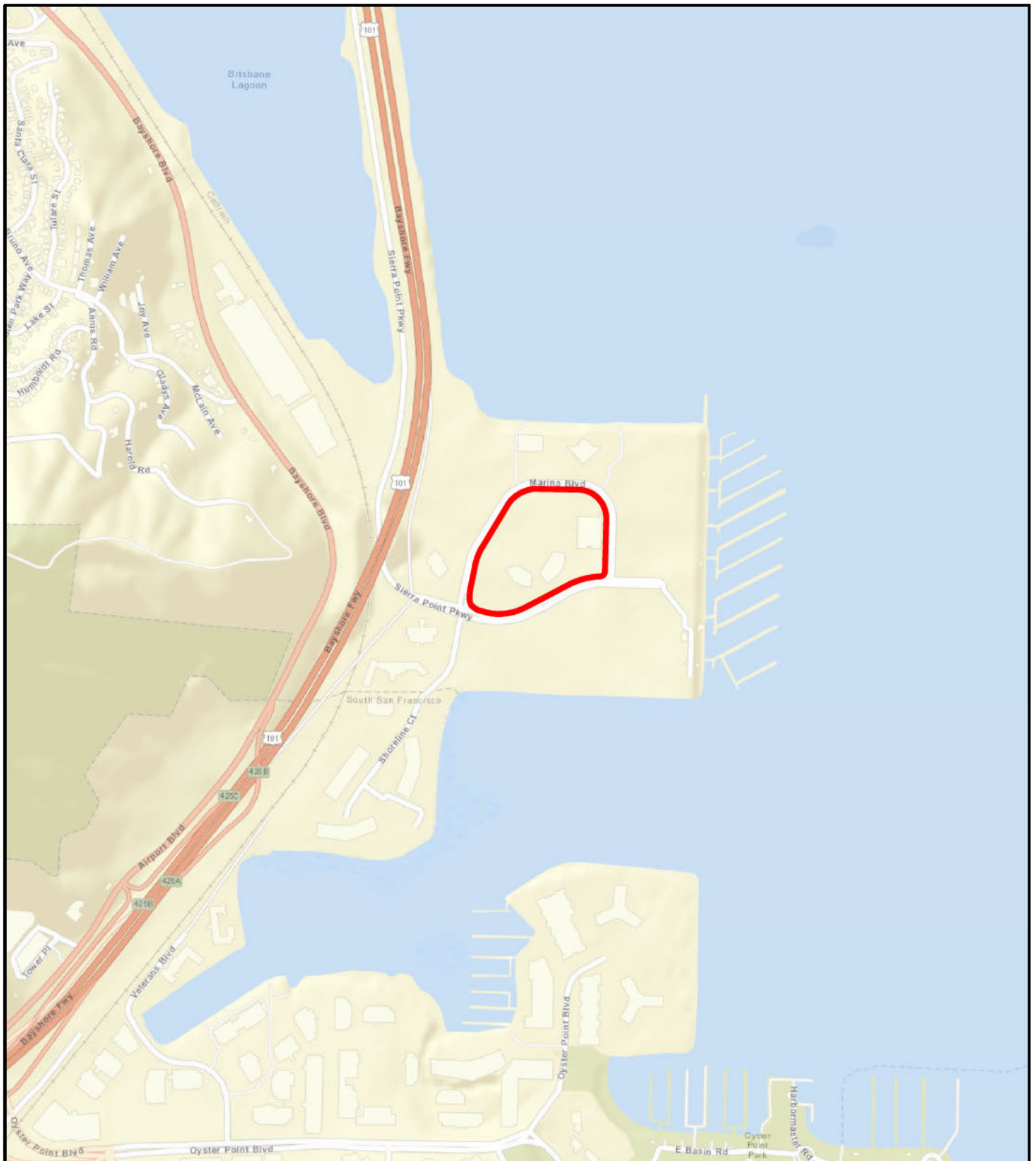


John Swiecki, Community Development Director

Attachment: Project Locations Map

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LSA

LEGEND

  Project Location



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FEET

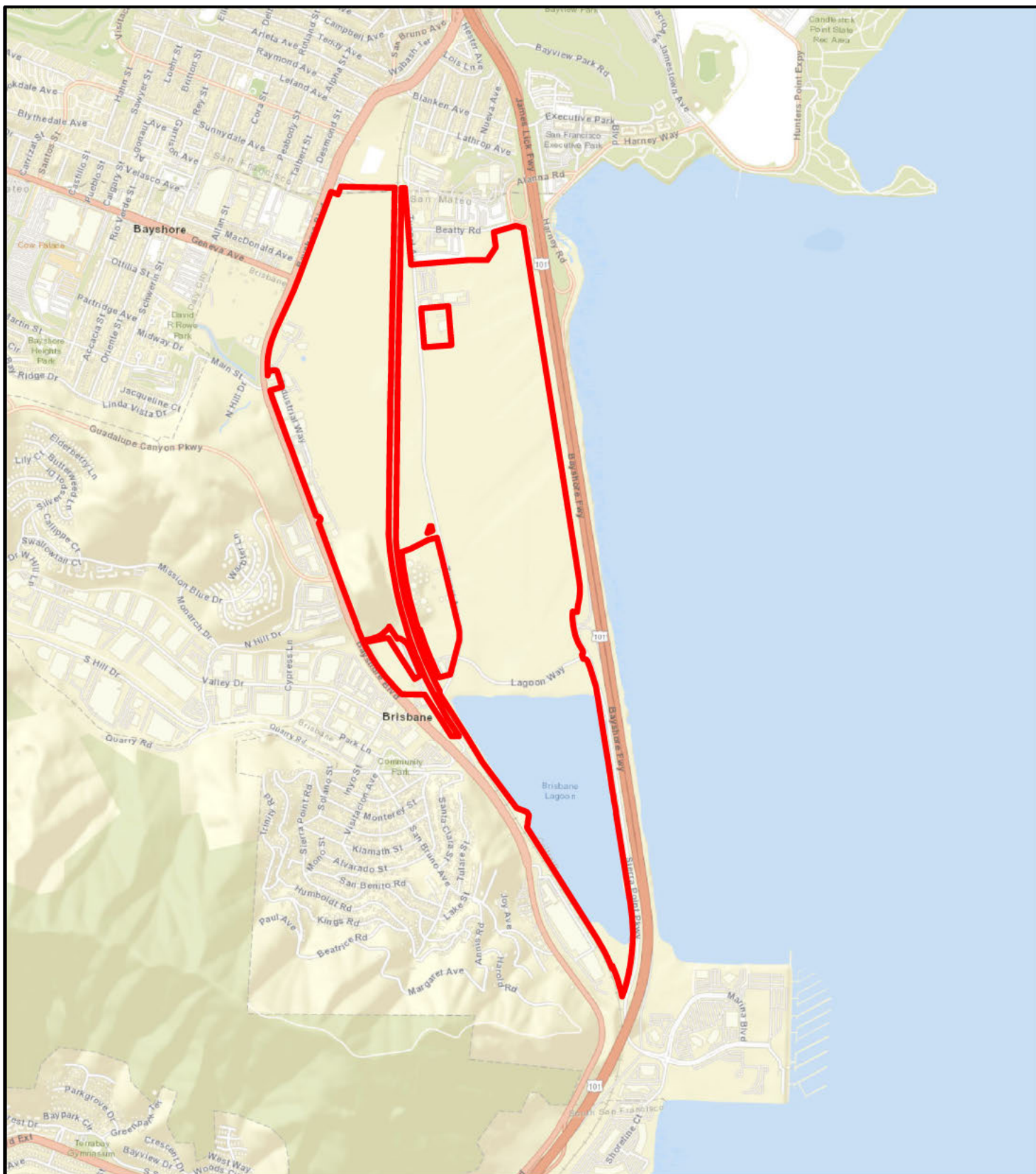
SOURCE: Esri World Street Map (2021)

I:\BRI2102\GIS\MXD\Project\_Location\_SierraPointTowers.mxd (6/29/2022)

FIGURE 1

Sierra Point Towers Project  
Project Location





**LSA**

**LEGEND**

Baylands Specific Plan Boundary



0 1000 2000  
FEET

SOURCE: Esri World Street Map (2021)

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**FIGURE 2**

*Brisbane Baylands Project*  
**Project Location**



**City of Brisbane**  
50 Park Place  
Brisbane, CA 94005-1310  
(415) 508-2100  
(415) 467-4989 Fax

August 3, 2022

Indian Canyon Mutsun Band of Costanoan  
Kanyon Sayers-Roods, MLD Contact  
1615 Pearson Court  
San Jose, CA 95122

Re: Assembly Bill 52 Consultation for the Proposed Brisbane Baylands and Sierra Point Towers Projects, San Mateo County, California

Dear Ms. Sayers-Roods,

The City of Brisbane (City) has received applications for the proposed Brisbane Baylands Specific Plan and Sierra Point Towers Projects. Summaries of both projects are provided below. Additional information for each project can be found on the City's webpage, which can be accessed at: <https://www.brisbaneca.org/projects> and selecting either "Brisbane Baylands" or "Sierra Point Towers" in the list of projects. A map of the project sites is provided as an attachment.

- The Baylands Specific Plan would allow for development of 2,200 dwelling units, 6.5 million square feet of commercial office development with an additional 500,000 square feet of hotel use; acquisition of a water supply; extension of Geneva Avenue east from Bayshore Boulevard including a bridge over the Caltrain right-of-way; and associated onsite and offsite infrastructure. The Specific Plan provides for preservation of 141.6 acres of the site's 500.2 upland acreage in open space, including habitat preservation and enhancement as well as active and passive recreational areas. The balance of the Baylands consists of the 115.8-acre Brisbane Lagoon and an additional 26 acres of existing upland area that will be inundated by sea level rise through the year 2100.]
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John Swiecki, Community Development Director  
City of Brisbane  
Community Development Department  
50 Park Place  
Brisbane, CA 94005

Thank you for your consideration of this request.

Kind Regards,



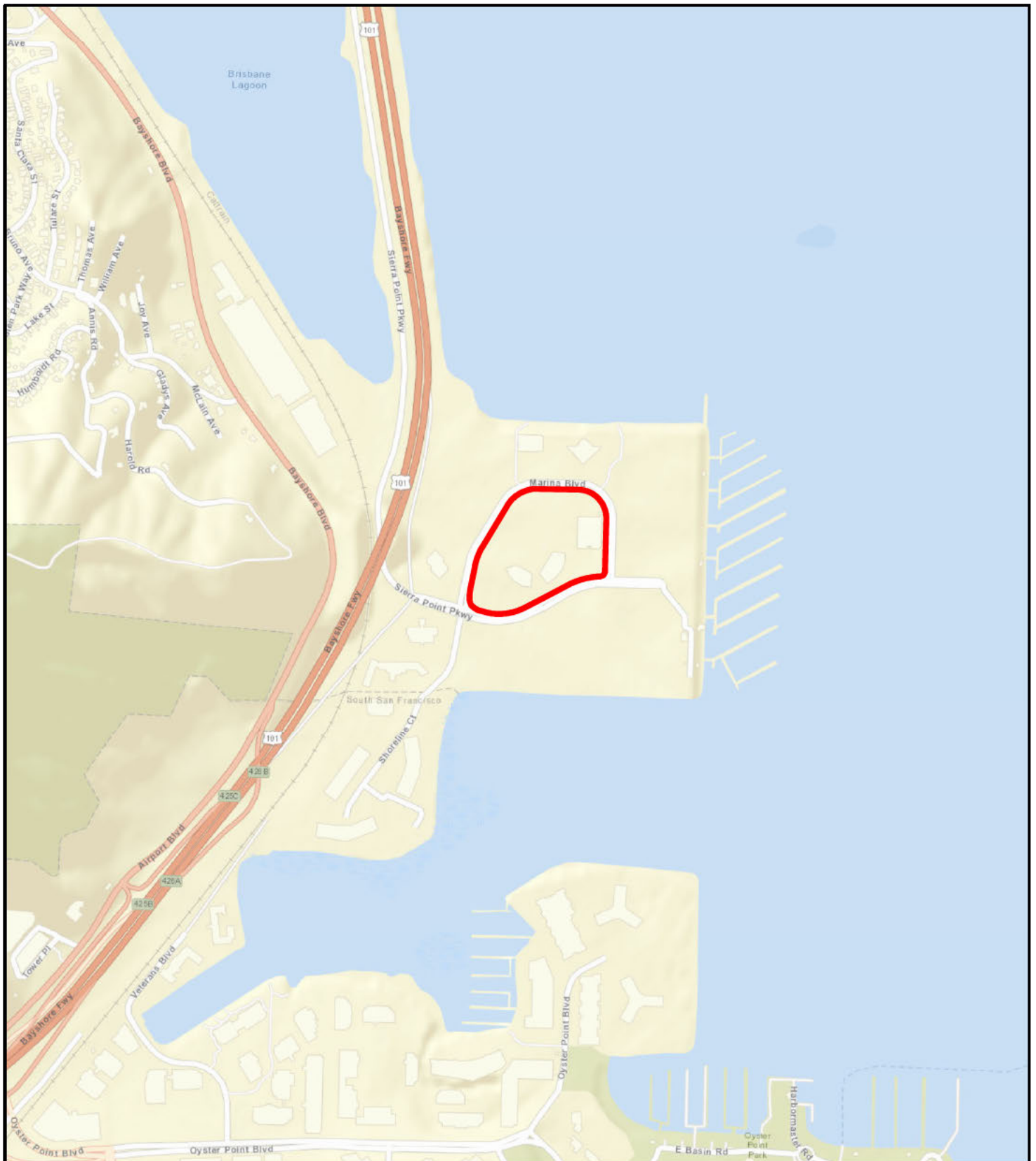
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Attachment: Project Locations Map

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LSA

#### LEGEND

Project Location



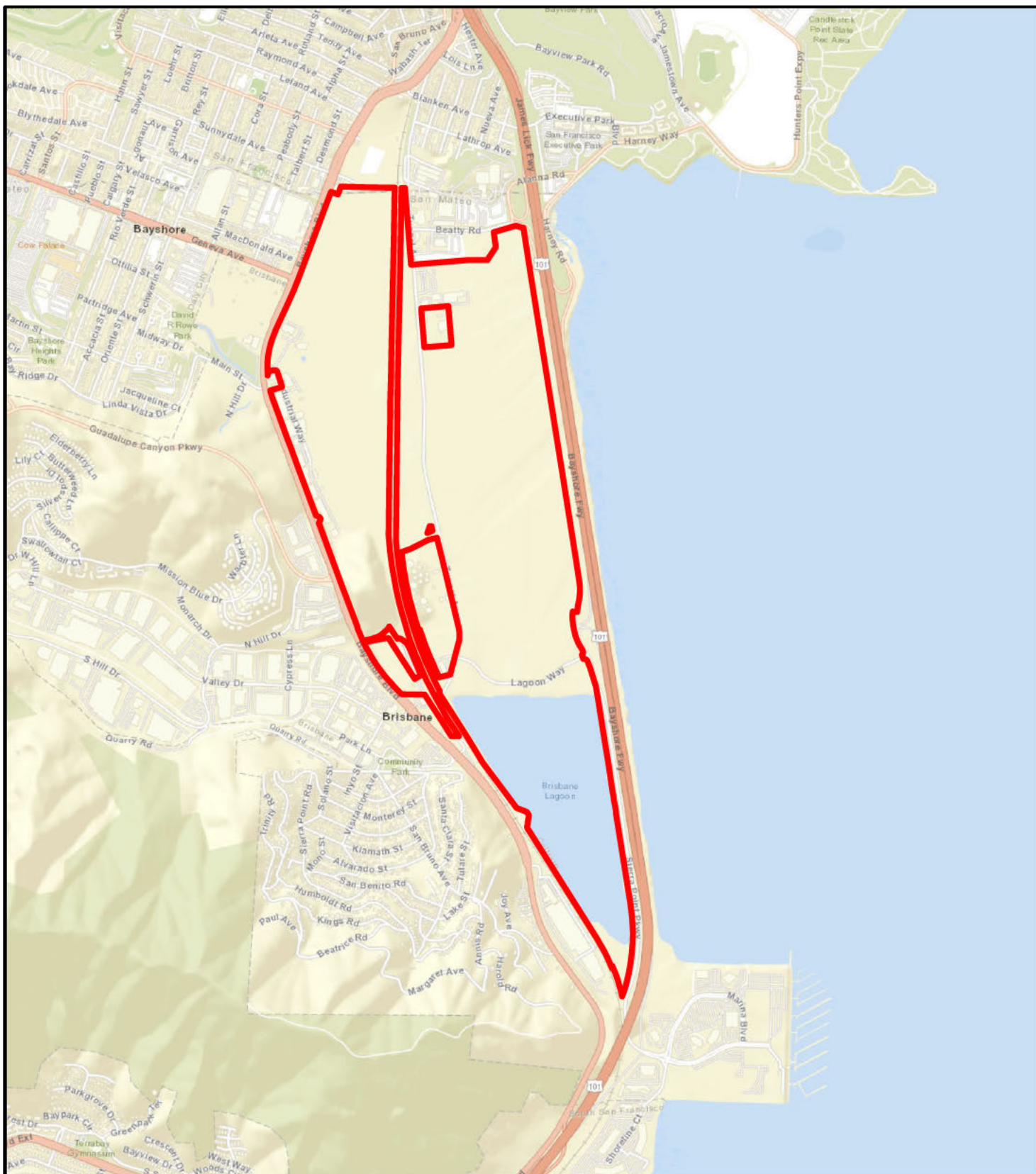
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SOURCE: Esri World Street Map (2021)

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FIGURE 1

Sierra Point Towers Project  
Project Location



**LSA**

**LEGEND**

Baylands Specific Plan Boundary



0 1000 2000  
FEET

SOURCE: Esri World Street Map (2021)

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**FIGURE 2**

*Brisbane Baylands Project*  
**Project Location**



**City of Brisbane**  
50 Park Place  
Brisbane, CA 94005-1310  
(415) 508-2100  
(415) 467-4989 Fax

August 3, 2022

Wuksache Indian Tribe/Eshom Valley Band  
Kenneth Woodrow, Chairperson  
1179 Rock Haven Ct.  
Salinas, CA, 93906

Re: Assembly Bill 52 Consultation for the Proposed Brisbane Baylands and Sierra Point Towers Projects, San Mateo County, California

Dear Chairman Woodrow,

The City of Brisbane (City) has received applications for the proposed Brisbane Baylands Specific Plan and Sierra Point Towers Projects. Summaries of both projects are provided below. Additional information for each project can be found on the City's webpage, which can be accessed at: <https://www.brisbaneca.org/projects> and selecting either "Brisbane Baylands" or "Sierra Point Towers" in the list of projects. A map of the project sites is provided as an attachment.

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City of Brisbane  
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Brisbane, CA 94005

Thank you for your consideration of this request.

Kind Regards,

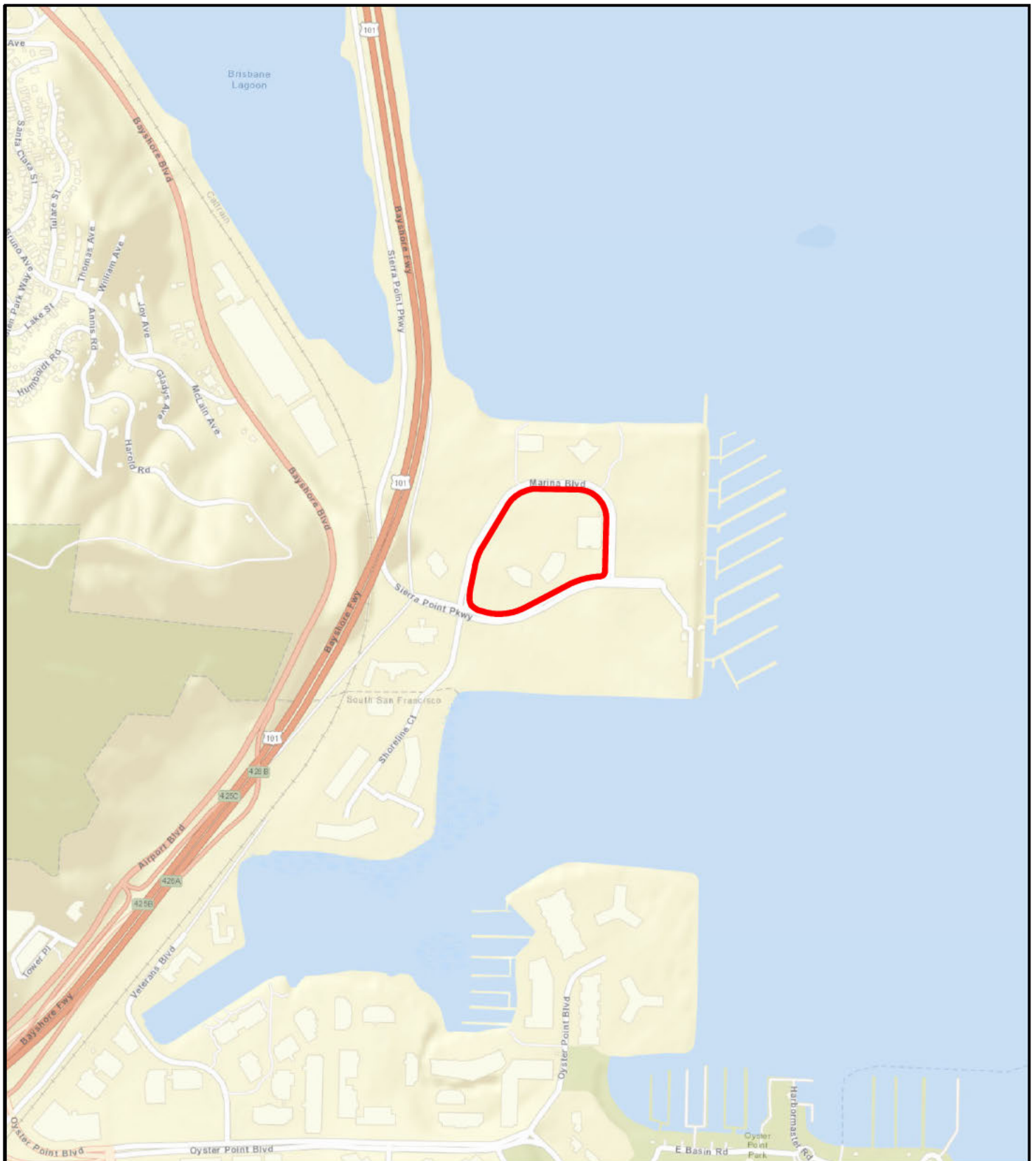


John Swiecki, Community Development Director

Attachment: Project Locations Map

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<sup>1</sup> Public Resources Code (PRC) Section 21074(a) defines Tribal Cultural Resources as either of the following: (1) Sites, features, places, cultural landscapes, sacred places, and objects with cultural value to a California Native American tribe that are either: (A) included or determined to be eligible for inclusion in the California Register of Historical Resources; or (B) included in a local register of historical resources as defined in subdivision (k) of PRC Section 5020.1; or (2) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of PRC Section 5024.1.



LSA

#### LEGEND

Project Location



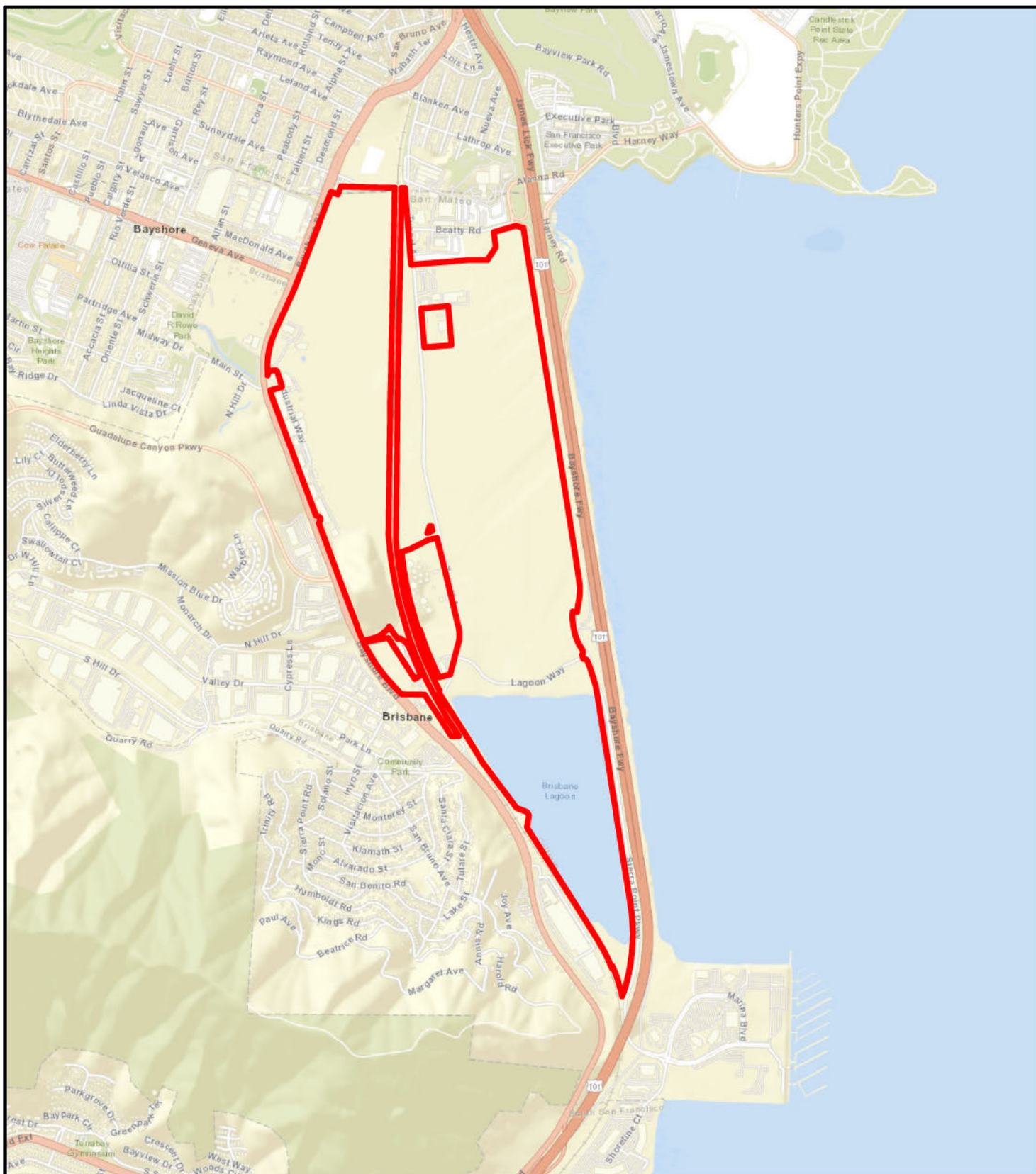
0 500 1000  
FEET

SOURCE: Esri World Street Map (2021)

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FIGURE 1

Sierra Point Towers Project  
Project Location



**LSA**

**LEGEND**

Baylands Specific Plan Boundary



0 1000 2000  
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SOURCE: Esri World Street Map (2021)

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**FIGURE 2**

*Brisbane Baylands Project*  
**Project Location**





**City of Brisbane**  
50 Park Place  
Brisbane, CA 94005-1310  
(415) 508-2100  
(415) 467-4989 Fax

August 3, 2022

Amah Mutsun Tribal Band of Mission San Juan Bautista  
Attn: Irene Zwierlein, Chairperson  
3030 Soda Bay Road  
Lakeport, CA, 95453

Re: Assembly Bill 52 Consultation for the Proposed Brisbane Baylands and Sierra Point Towers Projects, San Mateo County, California

Dear Chairwoman Zwierlein,

The City of Brisbane (City) has received applications for the proposed Brisbane Baylands Specific Plan and Sierra Point Towers Projects. Summaries of both projects are provided below. Additional information for each project can be found on the City's webpage, which can be accessed at: <https://www.brisbaneca.org/projects> and selecting either "Brisbane Baylands" or "Sierra Point Towers" in the list of projects. A map of the project sites is provided as an attachment.

- The Baylands Specific Plan would allow for development of 2,200 dwelling units, 6.5 million square feet of commercial office development with an additional 500,000 square feet of hotel use; acquisition of a water supply; extension of Geneva Avenue east from Bayshore Boulevard including a bridge over the Caltrain right-of-way; and associated onsite and offsite infrastructure. The Specific Plan provides for preservation of 141.6 acres of the site's 500.2 upland acreage in open space, including habitat preservation and enhancement as well as active and passive recreational areas. The balance of the Baylands consists of the 115.8-acre Brisbane Lagoon and an additional 26 acres of existing upland area that will be inundated by sea level rise through the year 2100.]
- The proposed Sierra Point Towers Project (located at 2000 Sierra Point Parkway and 8000 Marina Boulevard within the City of Brisbane) consists of the demolition of the existing parking garage and surface parking lots and construction of two new office/life sciences buildings, an amenity building, and a parking structure, totaling approximately 863,220 square feet of new building construction. The existing 427,283 square feet of office building space on site would be retained, for a total of approximately 1,290,503 square feet of office/life science uses, and a total of 3,858 parking spaces.

The City is the Lead Agency overseeing preparation of the environmental review documentation to be prepared pursuant to the California Environmental Quality Act (CEQA). Separate Environmental Impact Reports (EIRs) will be prepared for each project.

Assembly Bill 52 (AB 52), which became law January 1, 2015, requires that, as part of the CEQA review process, public agencies provide early notice of a project to California Native American Tribes, and if formally requested in writing by a Tribe, consult with Tribes in regards to the project. The Native American Heritage Commission (NAHC) has identified your Tribe as traditionally and culturally affiliated with the geographic area of the above projects and is entitled to consult with the City pursuant to AB 52. Via this letter, the City is hereby providing notice to all Tribes identified by the NAHC as eligible to consult with the City for both the Brisbane Baylands and Sierra Point Towers Projects.

The purpose of AB 52 and the consultation, if requested, is to identify and consider potential impacts to Tribal Cultural Resources (TCRs),<sup>1</sup> and take into account tribal cultural values in addition to scientific and archaeological values when determining possible impacts and mitigation. An impact to a TCR may result in a significant impact under CEQA which would require avoidance or minimization of the impact.

To request consultation under AB 52 for the Brisbane Baylands and/or Sierra Point Towers Project, please contact me, John Swiecki, Community Development Director, in writing within 30 days of receipt of this letter. Should a response from you not be received within this timeframe, the City may not be required to enter into consultation with you on this matter.

I can be reached by email at [johnswiecki@ci.brisbane.ca.us](mailto:johnswiecki@ci.brisbane.ca.us) or by mail at:

John Swiecki, Community Development Director  
City of Brisbane  
Community Development Department  
50 Park Place  
Brisbane, CA 94005

Thank you for your consideration of this request.

Kind Regards,

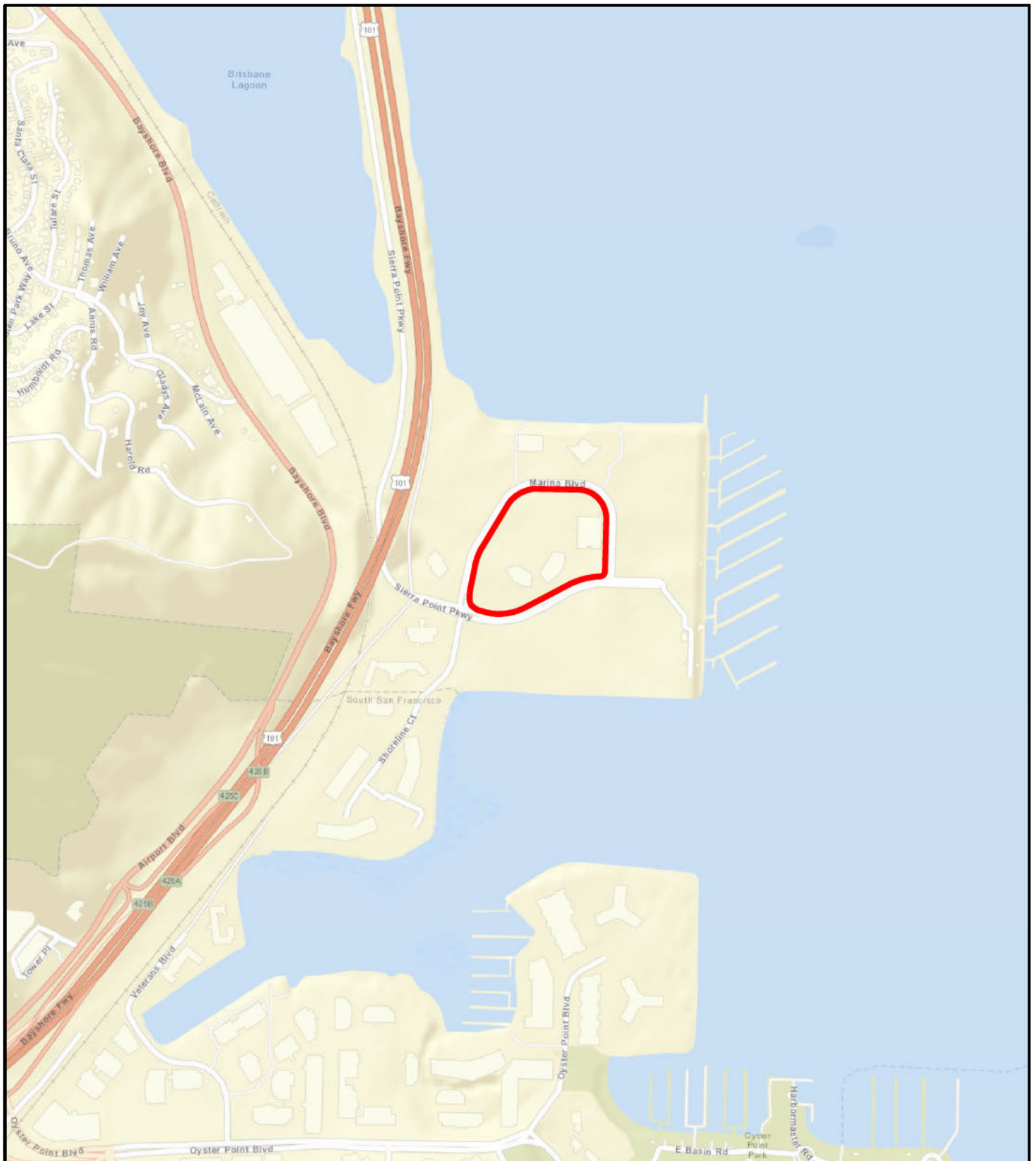


John Swiecki, Community Development Director

Attachment: Project Locations Map

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LSA

LEGEND

  Project Location



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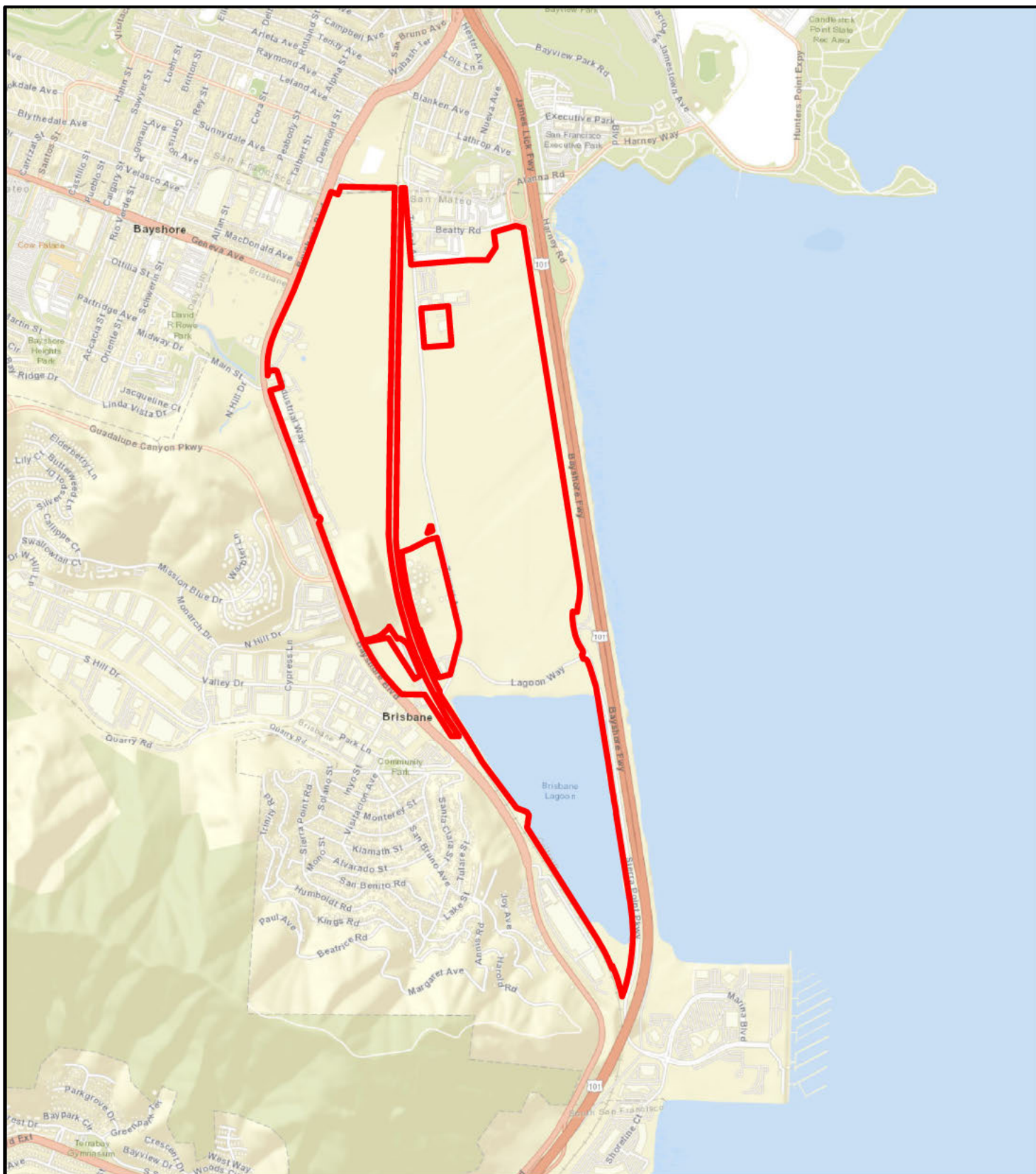
SOURCE: Esri World Street Map (2021)

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FIGURE 1

Sierra Point Towers Project  
Project Location





**LSA**

**LEGEND**

Baylands Specific Plan Boundary



0 1000 2000  
FEET

SOURCE: Esri World Street Map (2021)

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**FIGURE 2**

*Brisbane Baylands Project*  
**Project Location**



# Appendix D

## Photographs







**Photo 3. Overview of northwestern portion of Project Area with Brisbane in background, view SW.**



**Photo 4. Overview of SPRR Roundhouse, Boiler Shop and Tank, view S.**





**Photo 5. Overview of eastern portion of Project Area between the Caltrain and Tunnel Drive, view S.**



**Photo 6. Overview of southwestern portion of Project Area with P-41-002720 in background, view W.**



# Appendix E

## **SPRR Roundhouse 2023**

### **Photographs**





SOURCE: Esri, 2022; ESA, 2023;

Bayland Specific Plan Cultural Resources Technical Report

Photo Key Map







Photo 1 SPRR Roundhouse facade overview, facing east (panorama)



Photo 2 SPRR Roundhouse facade overview, facing east

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SOURCE: Cox Black & White, 2023

Baylands Specific Plan Cultural Resources Technical Report

## Appendix E

### Photographs 1 and 2



Photo 3 SPRR Roundhouse facade overview, facing southeast



Photo 4 SPRR Roundhouse facade overview, facing south

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SOURCE: Cox Black & White, 2023

Baylands Specific Plan Cultural Resources Technical Report

## Appendix E

### Photographs 3 and 4





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SOURCE: Cox Black & White, 2023

Baylands Specific Plan Cultural Resources Technical Report

## Appendix E

### Photographs 5 and 6





Photo 7 View from turntable, facing north into SPRR Roundhouse



Photo 8 SPRR Roundhouse facade overview, facing north

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SOURCE: Cox Black & White, 2023

Baylands Specific Plan Cultural Resources Technical Report

**Appendix E**  
Photographs 7 and 8





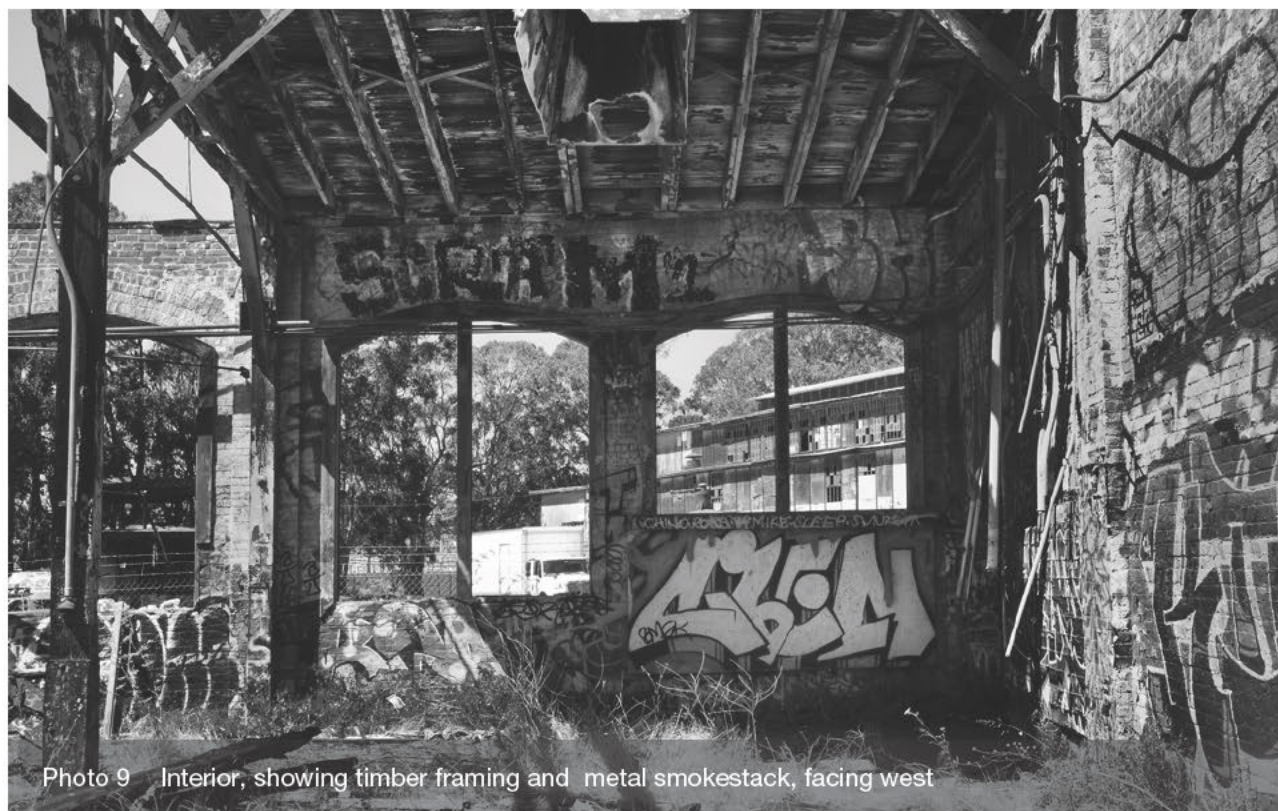


Photo 9 Interior, showing timber framing and metal smokestack, facing west



Photo 10 Interior, showing fenestration pattern, facing northwest

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SOURCE: Cox Black & White, 2023

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## Appendix E

### Photographs 9 and 10





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SOURCE: Cox Black & White, 2023

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## Appendix E

### Photographs 11 and 12





Photo 13 Wood Timber Framing, facing north



Photo 14 SPRR Roundhouse, facing southwest

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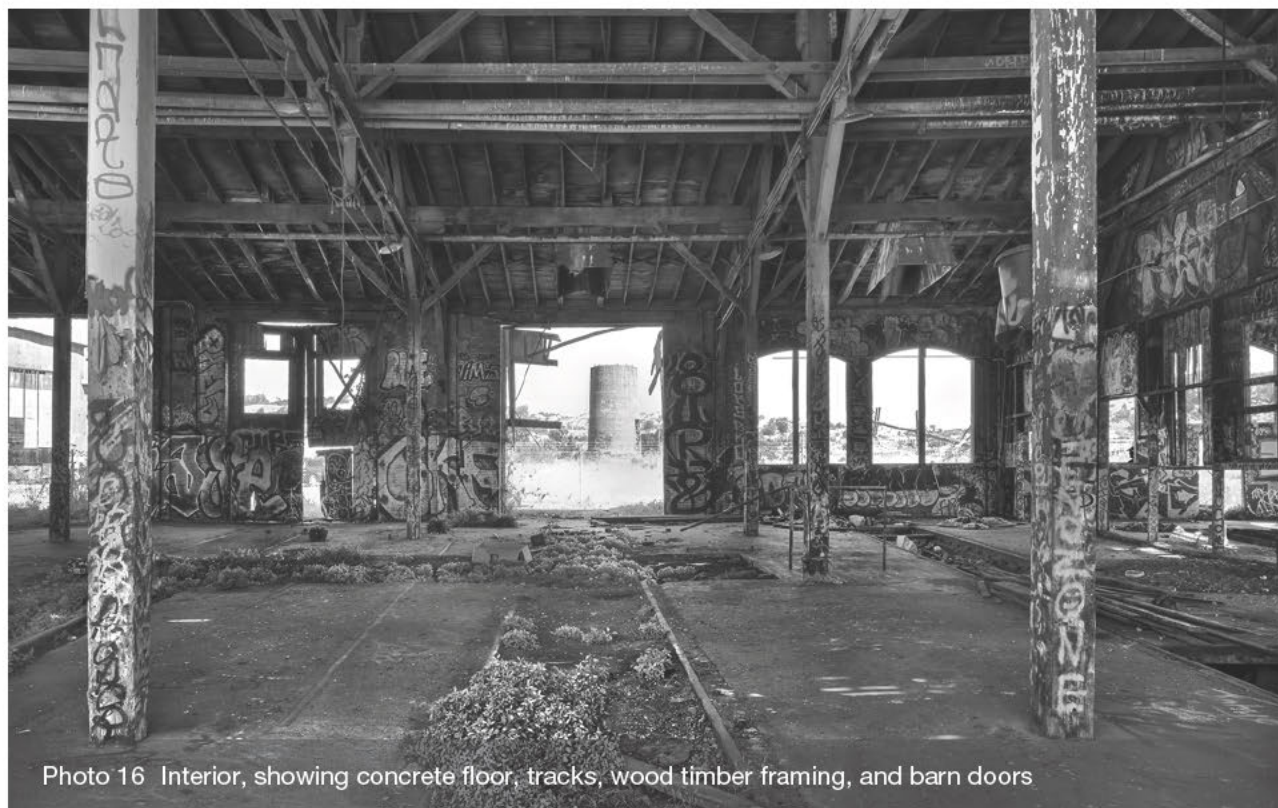
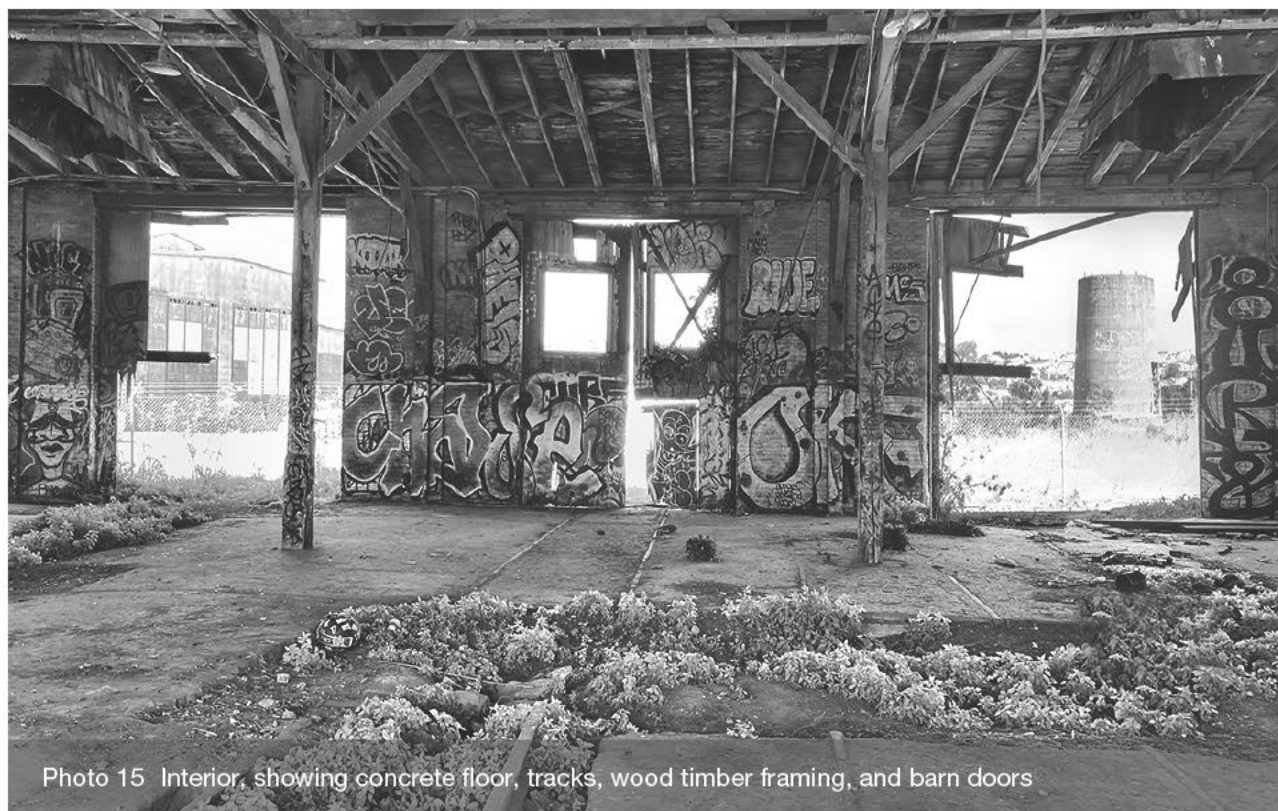
SOURCE: Cox Black & White, 2023

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## Appendix E

### Photographs 13 and 14





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SOURCE: Cox Black & White, 2023

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## Appendix E

### Photographs 15 and 16





Photo 17 Cast Iron Stall Posts with lintels/stall numbers



Photo 18 Interior, showing concrete floor, wood timber framing, and drop pits between 36 and 38

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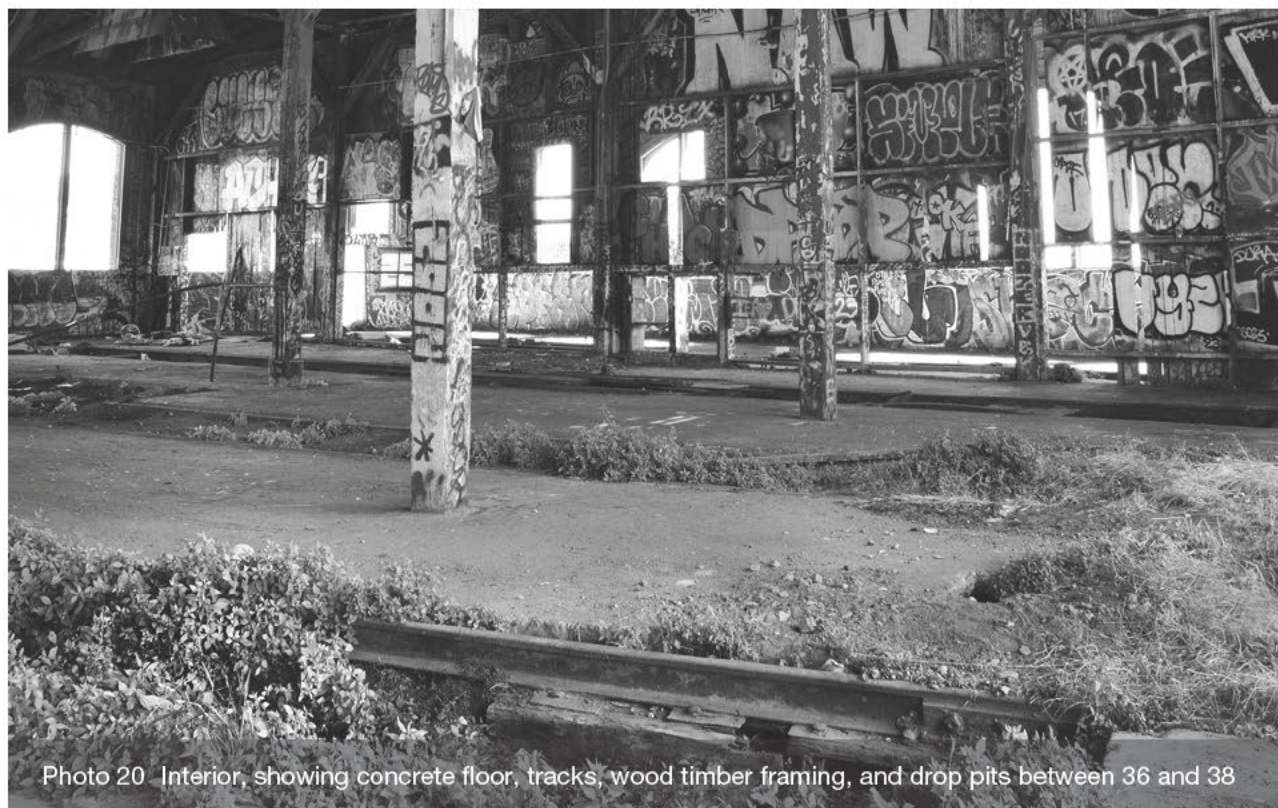
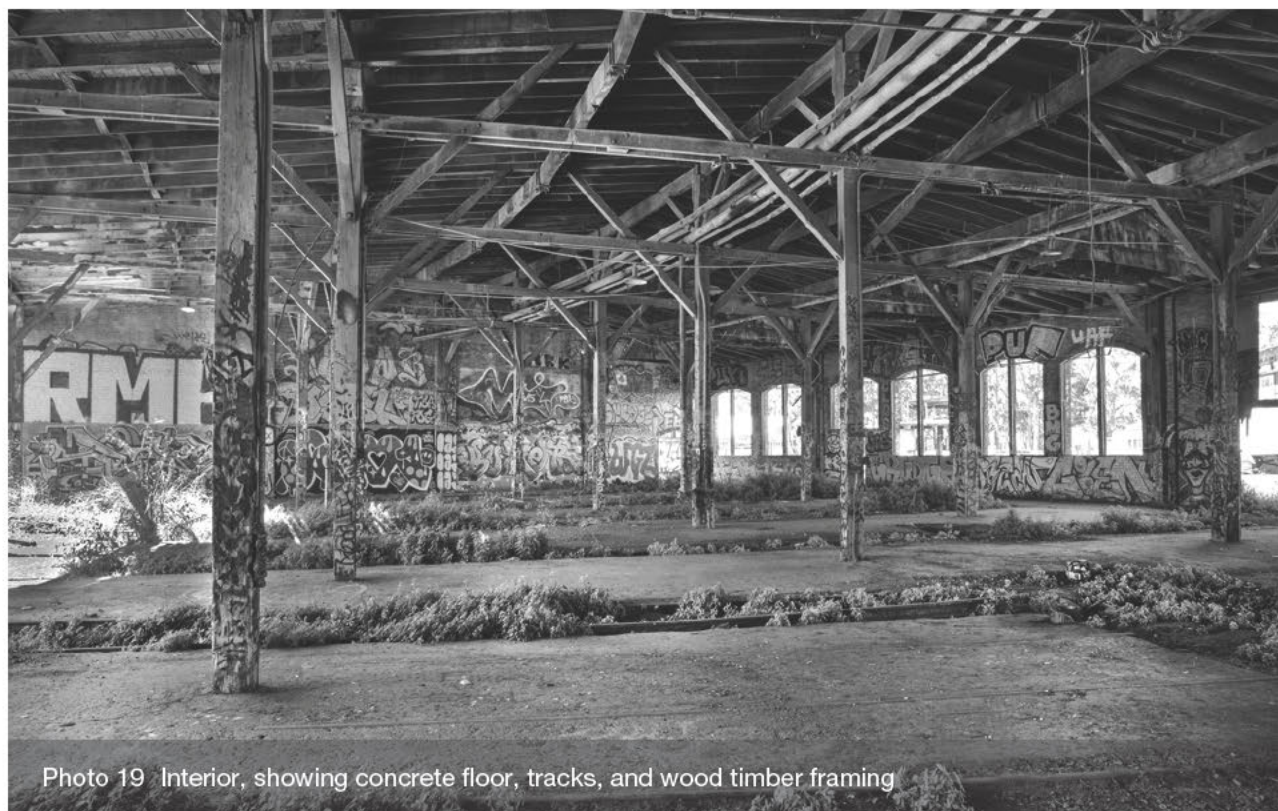
SOURCE: Cox Black & White, 2023

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## Appendix E

### Photographs 17 and 18





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SOURCE: Cox Black & White, 2023

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## Appendix E

### Photographs 19 and 20





Photo 21 Interior, showing drop pits between 36 and 38, and wood partition wall between 39 and 40



Photo 22 Interior, showing wood partition wall between 39 and 40

SOURCE: Cox Black & White, 2023

Baylands Specific Plan Cultural Resources Technical Report